



# TOWN OF FORT MILL TRAIL MASTER PLAN

June 2023



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## EXECUTIVE SUMMARY

Fort Mill is well-positioned to improve its trail network as it continues to grow. The Town has experienced significant growth in recent years due to its family-focused amenities and proximity to employment centers in Charlotte and Rock Hill. To ensure that Fort Mill develops responsibly and sustainably and meets the recreational needs of residents, Town leadership has prioritized trail network expansion through current and recent planning efforts.

The *Fort Mill Trail Master Plan* is a community-led initiative that proposes a comprehensive trail network based on input from residents, visitors, key area stakeholders, Town staff, and the Parks + Recreation Subcommittee. The plan builds on proposed trail facilities developed through the 2009 *Carolina Thread Trail Master Plan* for York County communities and the 2040 *Comprehensive Plan* (2020). This plan will also guide future development and ongoing maintenance of trails and sidepaths to increase active transportation and outdoor enjoyment opportunities. Together, the proposed and existing trail network will connect neighborhoods, commercial corridors, schools, parks, and other popular destinations within and outside of the Town limits.

### Key Recommendations

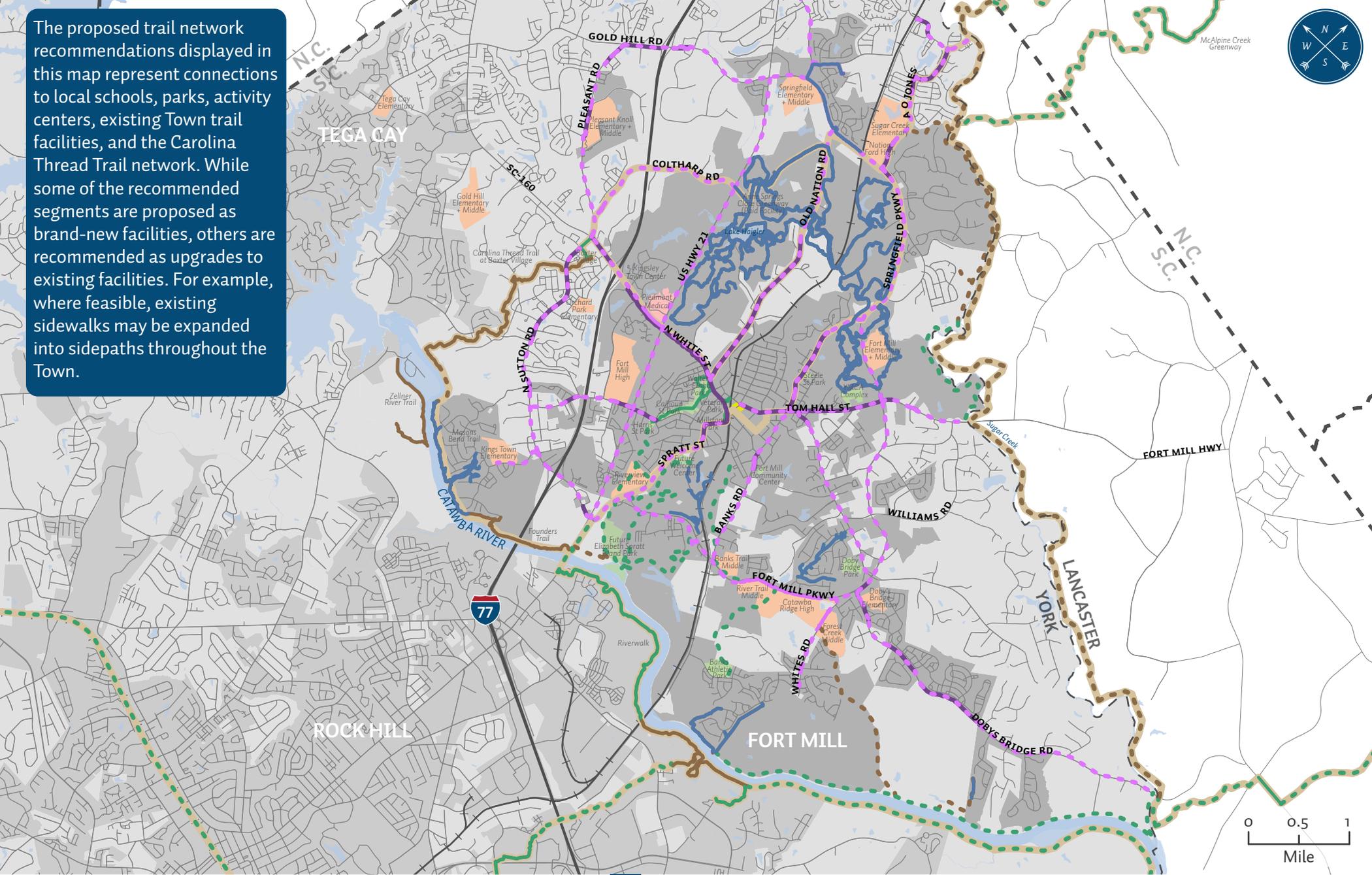
The key recommendations in this Plan are categorized into paved trails, natural surface trails, and sidepaths. Several of the proposed projects fill critical gaps in the existing trail network between key destinations identified by both the Steering Committee and the public (i.e., paved trail connection between Riverview Elementary and Harris Street Park). Some of the recommended projects are also part of Carolina Thread Trail planned network (i.e., natural surface trail connector between Masons Bend Trail and the Baxter Village Trail). In addition to network recommendations, this plan also recommends programs and policies to improve the overall trail network.

The Town also identified several priority projects within the recommended network to streamline implementation efforts. Priority projects recommended in this plan were prioritized based on a set of four criteria including community needs (based on public input), connections to schools, connections to parks, and connections to major activity centers.



**FORT MILL  
IS TRAIL-FRIENDLY!**

The proposed trail network recommendations displayed in this map represent connections to local schools, parks, activity centers, existing Town trail facilities, and the Carolina Thread Trail network. While some of the recommended segments are proposed as brand-new facilities, others are recommended as upgrades to existing facilities. For example, where feasible, existing sidewalks may be expanded into sidepaths throughout the Town.



# FORT MILL TRAIL MASTER PLAN

## Proposed Trail Network + Potential Carolina Thread Trail (CTT) Network

### LEGEND

- Road
  - ⊕ Railroad
  - Private Facility
  - Existing Sidewalk
  - Potential CTT Corridor
- MULTI-USE PATH/TRAIL STATUS**
- Complete Paved Trail
  - ⋯ Proposed Paved Trail
  - Complete Natural Surface Trail
  - ⋯ Proposed Natural Surface Trail
  - Complete Sidepath
  - ⋯ Proposed Sidepath
- Fort Mill School
  - Fort Mill Park
  - Hospital
  - Downtown/Main St
  - Water
  - Fort Mill
  - Other Jurisdiction
  - York County
  - County Boundary

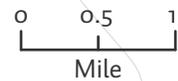




Photo: @jessicawilliams  
Illustration: @mariela  
and 'multiple Blessings'

**LOVE**  
*where you*  
**LIVE**



 **ACADEMY ST**  
DOWNTOWN FORT MILL



# 01

# INTRODUCTION

## OVERVIEW

The goal of the *Fort Mill Trail Master Plan* is to enable the Town of Fort Mill to understand the extent of multi-use path (i.e., paved trails and sidepaths) and trail needs, from development through maintenance, and the resources required to meet those needs. This plan proposes several new trail facilities, as well as implementation strategies and steps for maintenance regimen upon completion. The plan will direct development and ongoing maintenance of trails and sidepaths to increase active transportation and outdoor enjoyment opportunities, connecting neighborhoods, commercial corridors, schools, and other popular destinations within and outside of the Town limits. To ensure that Fort Mill grows responsibly and sustainably, residents and Town leadership have prioritized the expansion of the trail network through recent planning efforts.

The following sections are included within Chapter 1:

- ▶ Vision + Goals
- ▶ Study Area Profile
  - » Community Demographics
- ▶ Greenway + Trail Benefits

## Process + Schedule

The recommended trail network for the *Town of Fort Mill Trail Master Plan* was developed and evaluated using an approach with considerations of the built, natural, social, and economic environments. These environments are explored further in Chapter 2: Existing Conditions.

The *Fort Mill Trail Master Plan* began in August 2022 and concluded in August 2023. The study process was divided into the following four phases: Project Visioning, Existing Conditions, Plan Recommendations, and Implementation + Final Plan. Key components for each phase are listed within the study process graphic below. Engagement efforts were integrated throughout the study process and included several meetings with a Steering Committee, one meeting with the Parks and Recreation Subcommittee, and one meeting with the general public. In addition to these meetings, a community survey was launched in November 2022 to obtain feedback on trail priorities for the public.

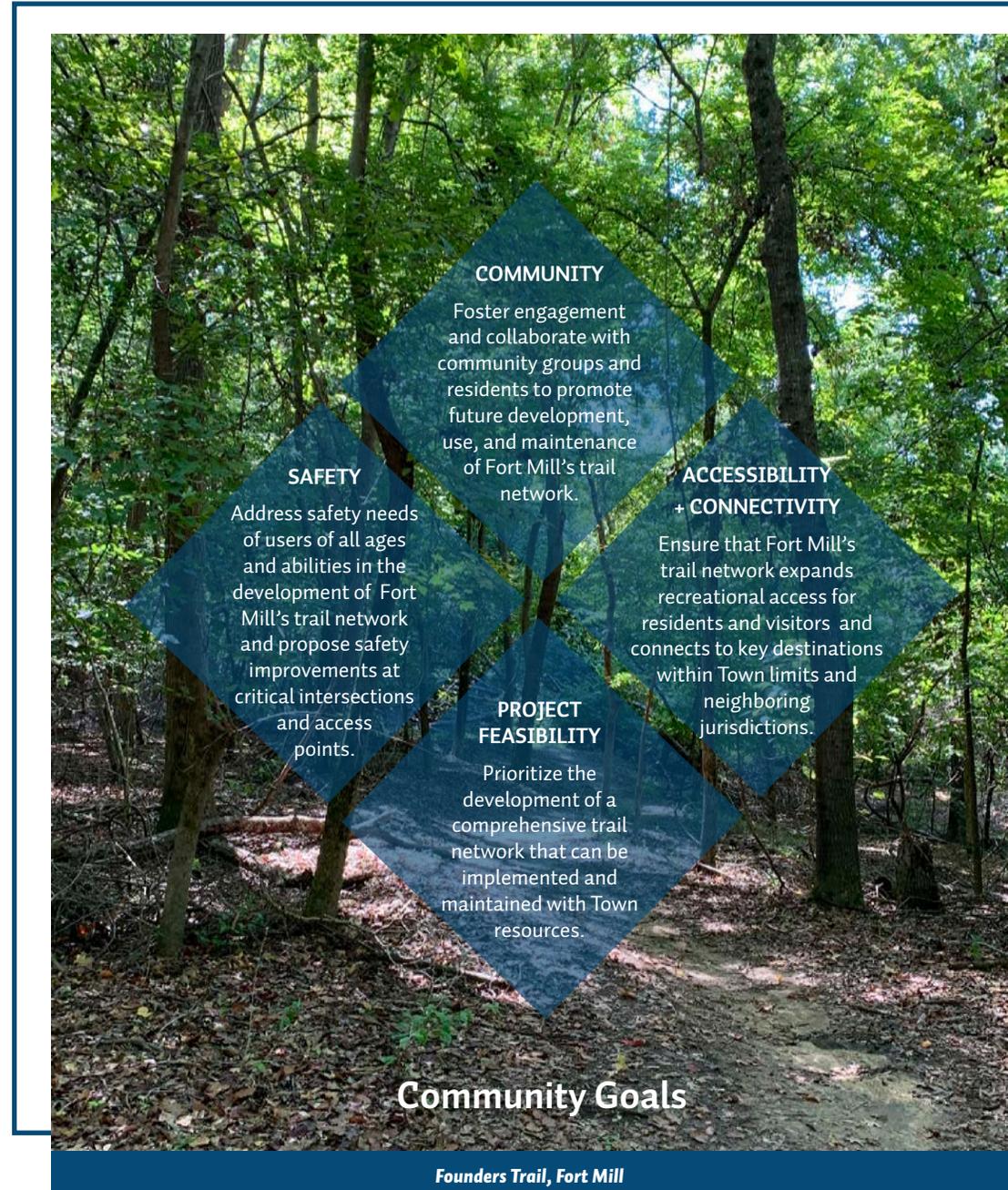
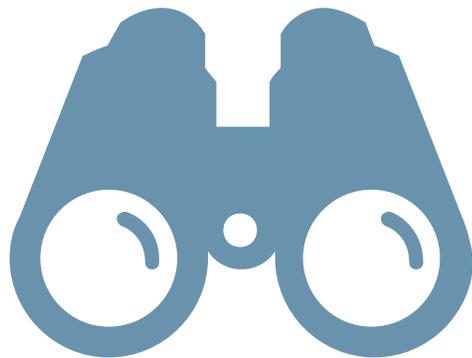


## VISION + GOALS

The Steering Committee for the *Fort Mill Trail Master Plan* formed the vision and goals for the plan through a visioning exercise held during the first Steering Committee Meeting. The goals of the *Fort Mill Trail Master Plan* focus on themes of accessibility, connectivity, safety, project feasibility, and community and served as a guide throughout the planning process. Community members also echoed these goals during outreach events. The goals in this plan reflect the proposed outcomes and recommendations of the plan. The community vision for the plan is provided below and the goals are described in the graphic to the right.

### Community Vision

“The Town of Fort Mill will be safe and accessible for community members of all ages and abilities to bike, walk, and hike along a comprehensive trail network that connects key destinations such as neighborhoods, parks, schools, commercial centers, and neighboring communities.”



## STUDY AREA PROFILE

The Town of Fort Mill is in York County, South Carolina, just north of the Catawba River. Trails have always been important to the community since the 1700's when the Catawba Nation constructed trails to connect to nearby villages for trading purposes. The Town was officially established in 1873 and its name originated from a fort constructed by the British, along with Webb's Mill. According to the 2020 Census, the Town supports slightly over 24,500 people in its corporate limits. The Town is home to several schools, parks, and trail systems including the Anne Springs Close Greenway, the Carolina Thread Trail, and the Fort Mill Gateway Trail among others.



### Anne Springs Close Greenway

Fort Mill's proximity to the Catawba River primes it for hiking trails and greenways that residents may enjoy all year round. The Anne Springs Close Greenway, located north of Downtown, provides some of the longest trail and greenway facilities in the Town. This 2,100-acre natural preserve offers several outdoor activities for locals to explore. All facilities are private and require memberships for use.



### Carolina Thread Trail

The Carolina Thread Trail acts a key conservation leader in York County and has worked to conserve significant natural areas throughout the County through trail construction. One popular trail near the Town of Fort Mill is the Baxter Village Trail. This 3.3-mile trail offers various surface types including natural surface, boardwalk, sidewalk, and paved paths. While on the trail, users may pass through wooded areas and through residential streets within Baxter Village.



*Credit: Carolina Thread Trail*

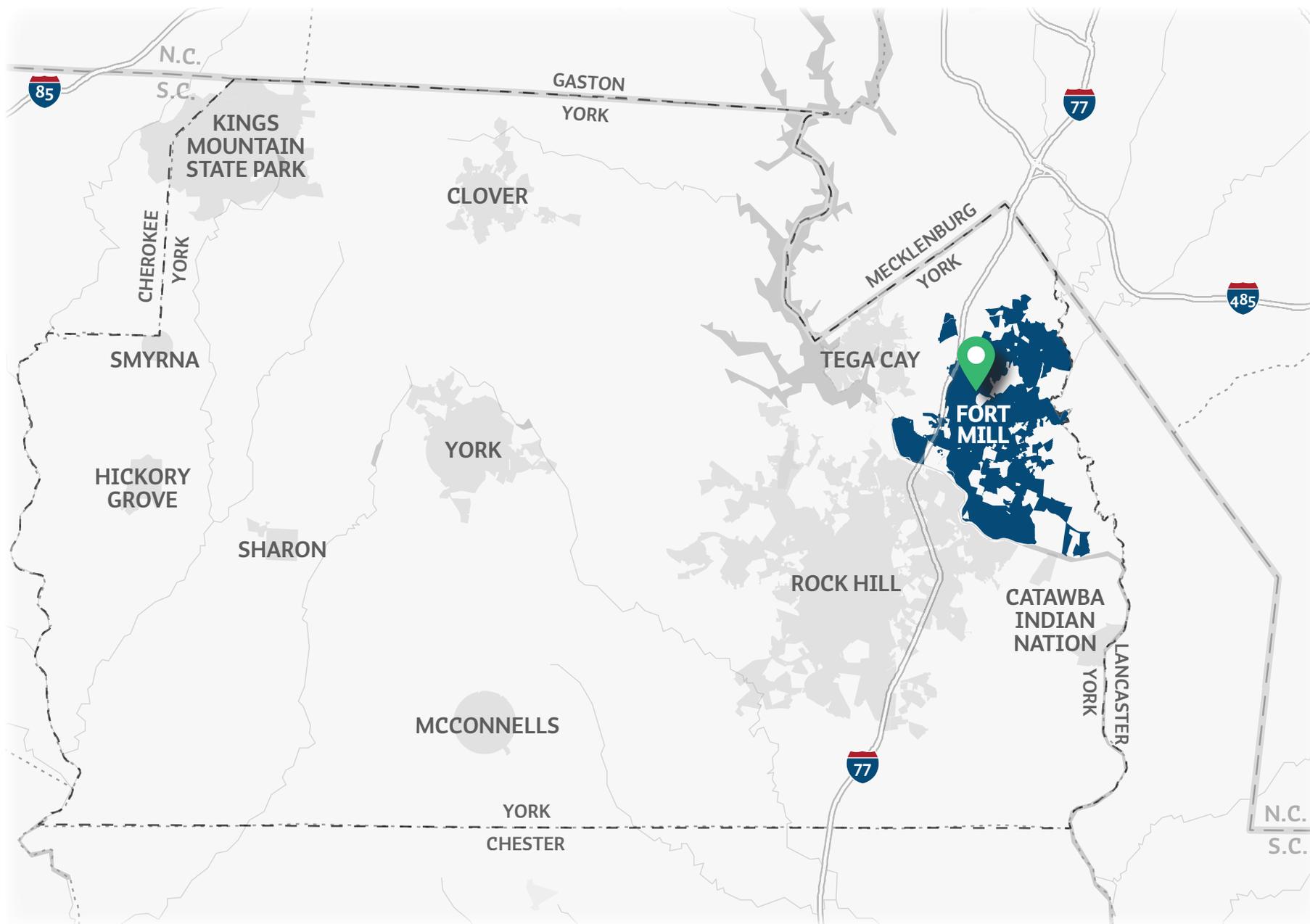


### Fort Mill Gateway Trail

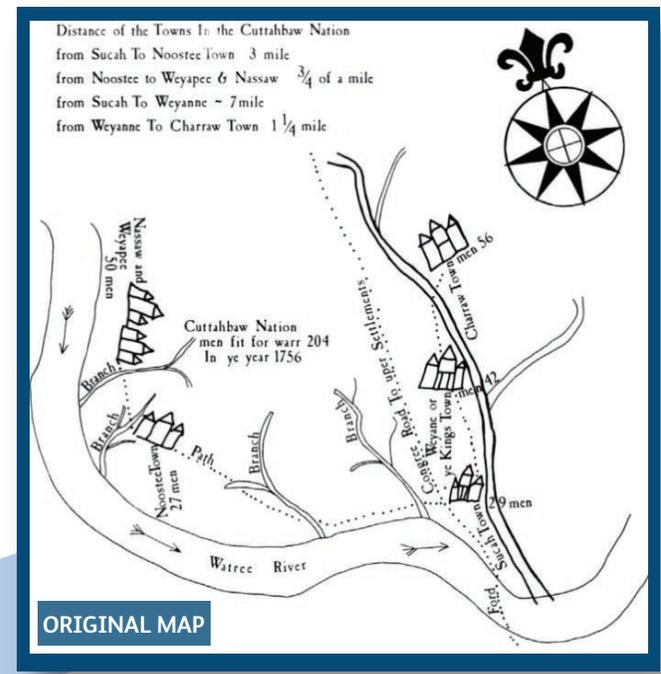
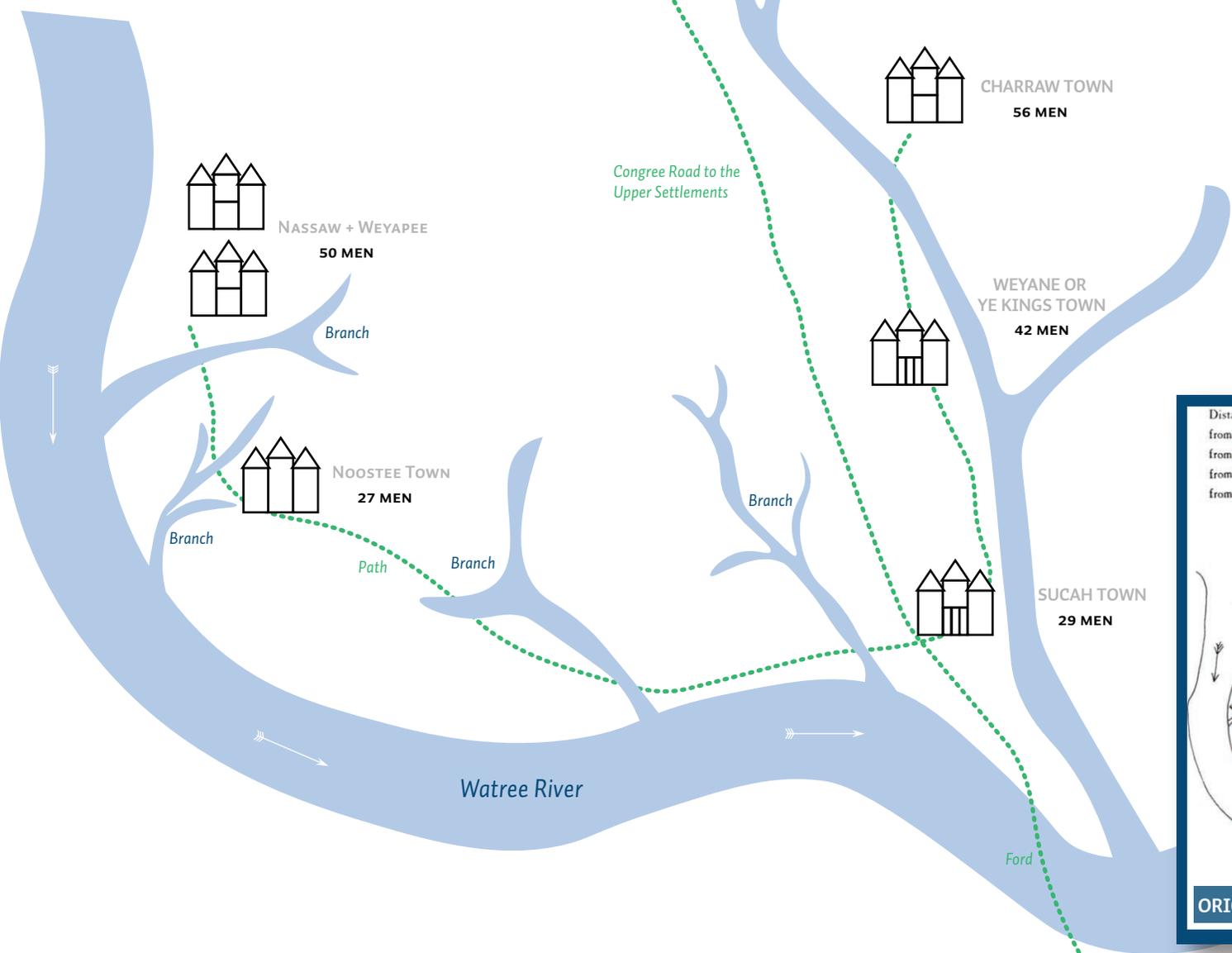
The Fort Mill Gateway Trail is a sidewalk that runs adjacent to Steele Creek Road and N White Street in Fort Mill. This sidewalk meanders through a peach orchard and provides connections between businesses at Kingsley Park Drive and Walter Y. Elisha Park. It is approximately two miles long and is part of the South Carolina Trails network. Both biking and walking are permitted uses along this sidewalk.



*Credit: Google Maps*



# Cattahbaw Nation men fit for warr 204 In ye year 1756



## CATAWBA NATION TOWN TRAILS

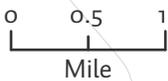
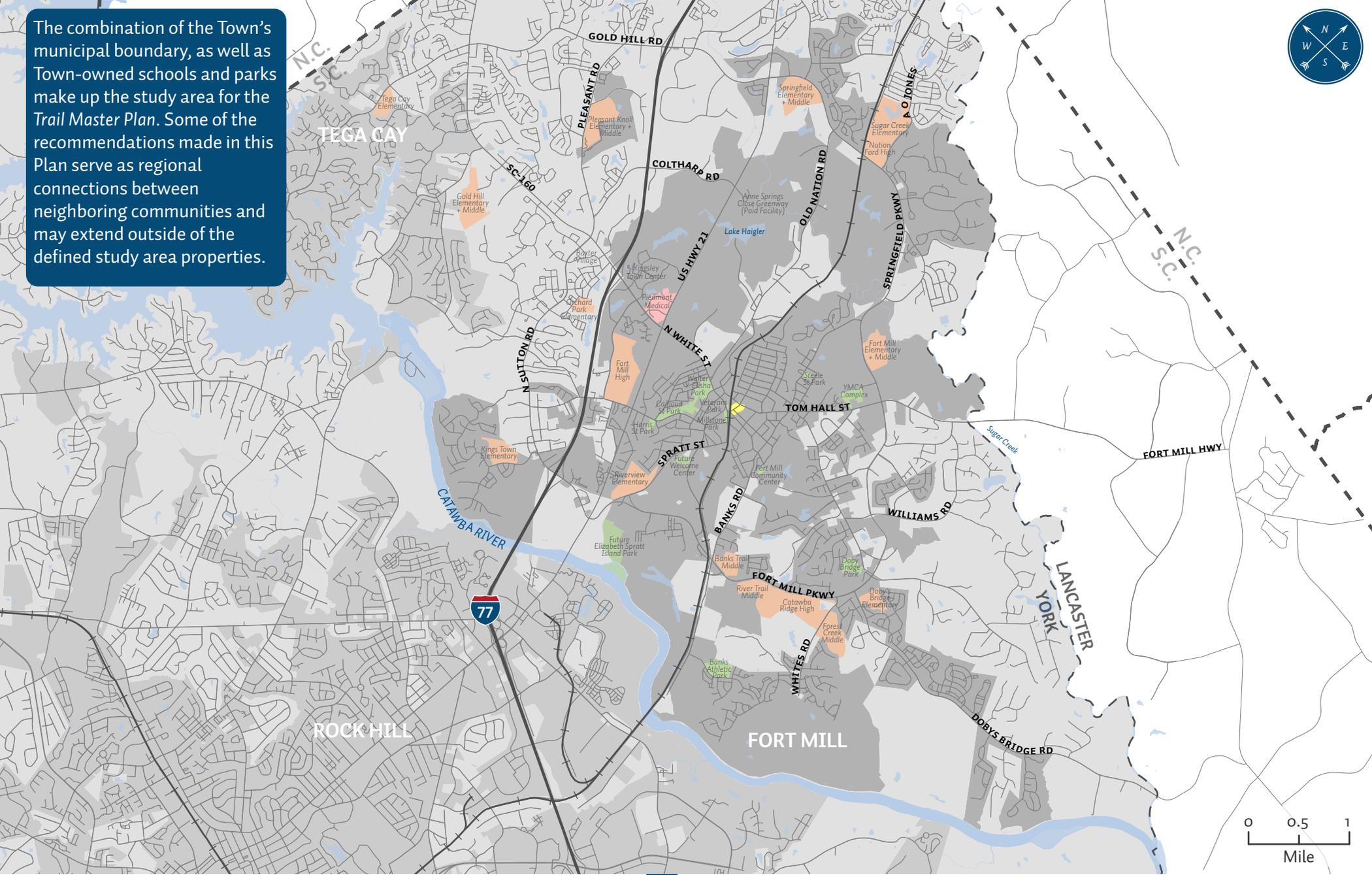
John Evans 1756 Map

Source: M. Wayne Archie, Avocational Archaeologist - Fort Mill History Museum Research Team

### MAP DEPICTING THE CATAWBA TOWNS AT NATION FORD

“The Catawba Nation has been an integral part of Fort Mill’s history. In the 1700’s there were six Catawba Towns or villages located in the present township of Fort Mill. These towns were located on the northeast side of the Catawba River along the Great Trading Path near the Nation Ford. All six towns were located within a two-mile radius of each other. These towns were connected by trails as indicated on the John Evans 1756 map. This map indicates the total number of men fit for war in each town, which totaled 204 men.”

The combination of the Town's municipal boundary, as well as Town-owned schools and parks make up the study area for the Trail Master Plan. Some of the recommendations made in this Plan serve as regional connections between neighboring communities and may extend outside of the defined study area properties.



# FORT MILL TRAIL MASTER PLAN

## Study Area

### LEGEND

- Road
- ++ Railroad
- Fort Mill School
- Fort Mill Park
- Hospital
- Downtown/Main St
- Water
- Fort Mill
- Other Jurisdiction
- York County
- County Boundary

## Planning Level Considerations: Community Demographics

Analyzing demographic trends are essential to planning the study area's trail network. This analysis helps to inform the public engagement approach and to ensure proposed recommendations meet the diverse needs of people residing in the study area. Demographic data was pulled from the 2020 American Community Survey (ACS) 5-year estimates (2016-2020) and was accessed through the United States Census Bureau.

This section includes an analysis of the following:

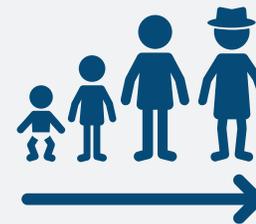
- Population
- Age
- Education
- Race + Ethnicity
- Poverty
- Commute
- Vehicle availability
- Income
- Poverty
- Language
- Employment density

### POPULATION

According to recent ACS data, the study area includes an estimated total population of 19.9k people. Between 2019 and 2020 the Town's population grew from 17,692 to 19,920, a 12.6% increase.



**19.9k**  
residents lived in the  
Town of Fort Mill in  
2020

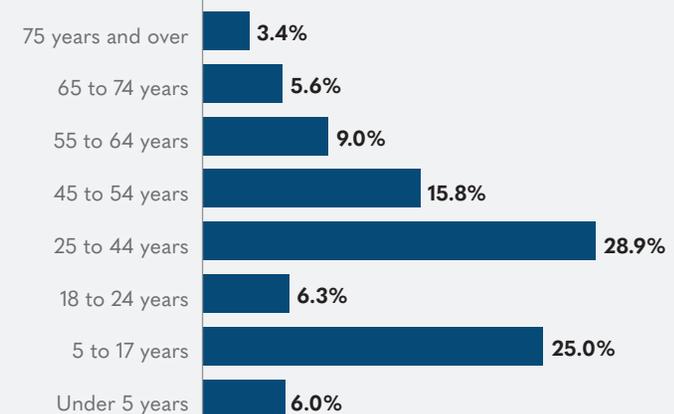


**66.2%**  
of the study area's  
population is  
*44 years or younger.*

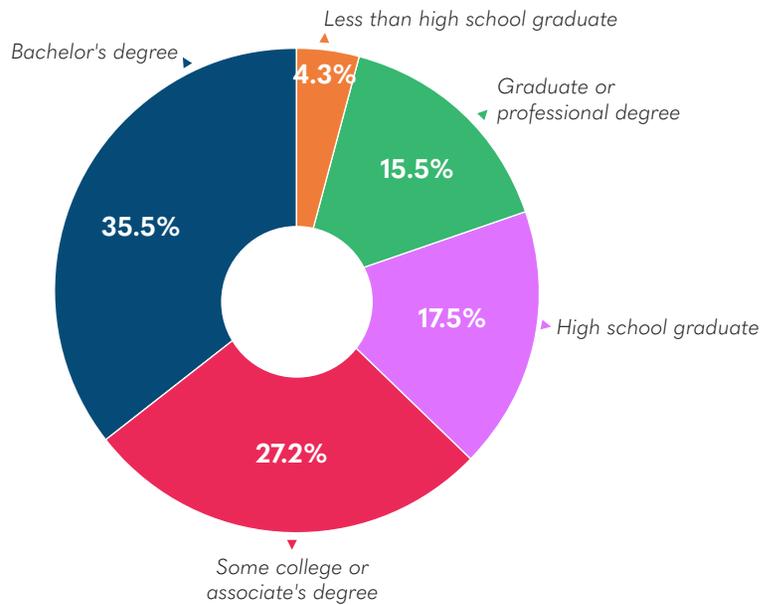
### AGE

Approximately twenty-nine percent (28.9%) of the Town's population falls between 25 to 44 years of age. The median age within the Town was 35.8. This is consistent with the median age in South Carolina which is 39.7. The second largest age range in the Town is the 5- to 17-year-old category at 25.0 percent.

### AGE GROUPS



### EDUCATIONAL ATTAINMENT



**35.5%**  
of the study area's population obtained a 4-year college degree

### EDUCATION

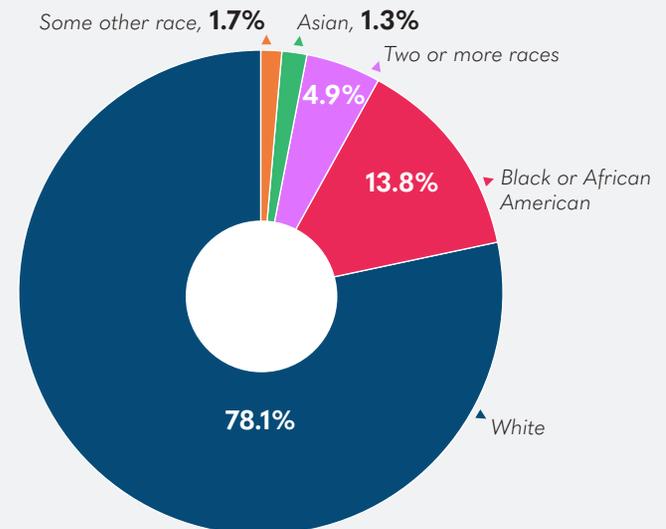
Less than 5 percent (4.3%) of Fort Mill's population did not complete high school. Twenty-seven percent (27.2%) of the population obtained an associate degree or attended some college. Thirty-five percent of the population obtained a 4-year college degree. This is higher than South Carolina's average which was 29.0 percent.

### RACE + ETHNICITY

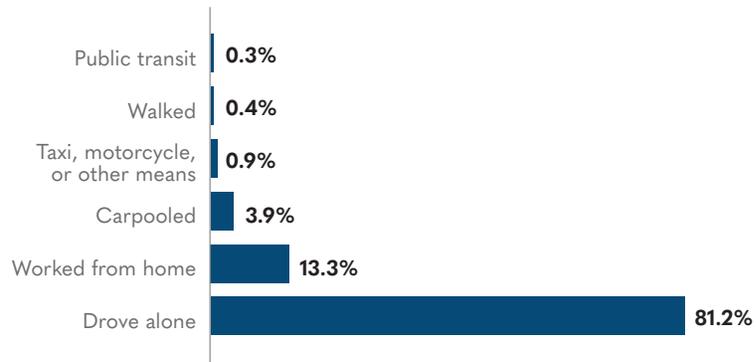
The largest racial groups in Fort Mill are White (Non-Hispanic) (78.1%), Black or African American (13.8%), and Asian (1.3%). Residents of mixed and other races comprise 6.6% of the population.

In the study area, six percent of residents identify as Hispanic or Latino origin which is consistent with that of South Carolina, in which 5.8 percent of the statewide population identifies as "Hispanic or Latino".

### RACIAL COMPOSITION



## MODE OF COMMUTE



**81.2%**  
of Fort Mill's residents  
*drive alone*  
in a car to reach work

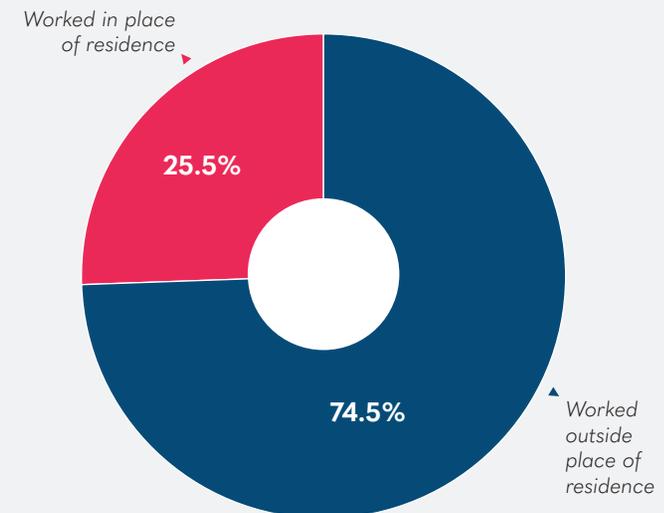
## COMMUTE MODE

Residents in the study area predominantly commute by single-occupancy vehicle, with 81.2 percent of workers driving alone to work. This statistic was followed by those who worked at home (13.3%) and those who carpoled to work (3.9%).

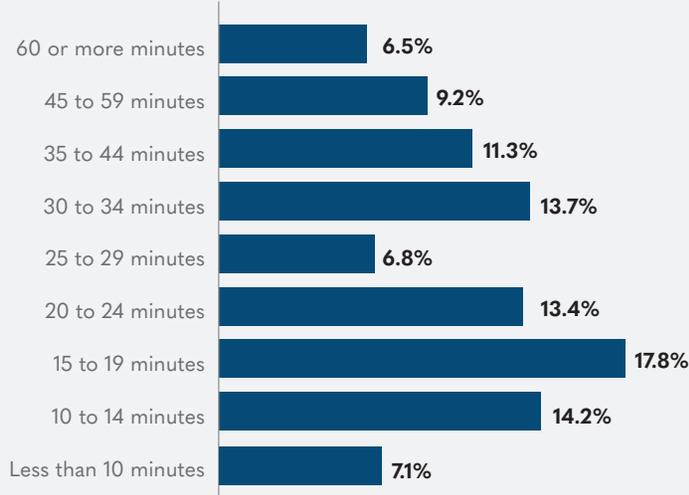
## LOCATION OF WORK

Out of 9,866 workers 16 years and older within the Town of Fort Mill, almost 75 percent (74.5%) worked outside their place of residence in the Town. The remaining 25.5 percent worked within the Town of Fort Mill. The employment density for workers in the Town is displayed on page 19.

## LOCATION OF WORK



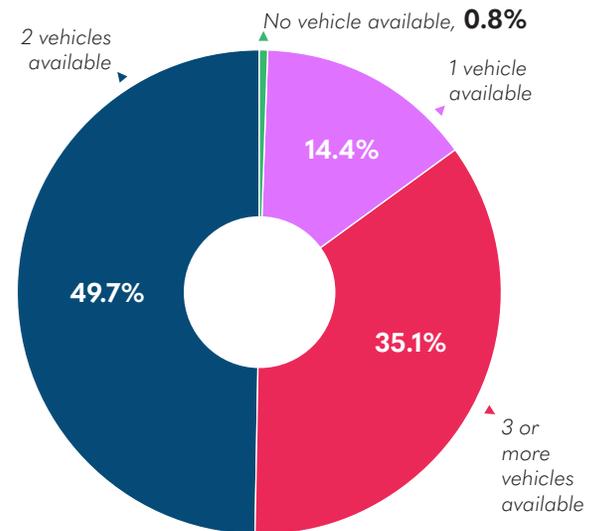
### COMMUTE TIME RANGES



### COMMUTE TIME

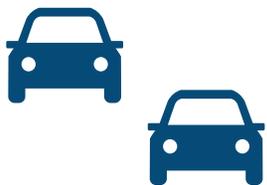
Using averages, employees in Fort Mill have a shorter commute time (26.6 minutes) than the average US worker (26.9 minutes). Roughly 7 percent of the Town spends 60 or more minutes commuting to work, while 0.98% of the workforce in the Town have "super commutes" in excess of 90 minutes.

### ACCESS TO VEHICLES



### VEHICLE ACCESS

Car ownership in Fort Mill is approximately the same as the national average, with an average of 2 cars per household. This is followed by households with access to three or more vehicles at 35.1%. Less than one percent (0.8%) of households do have access to a vehicle.



**49.7%**  
of Fort Mill's households  
have access to  
*2 or more vehicles*

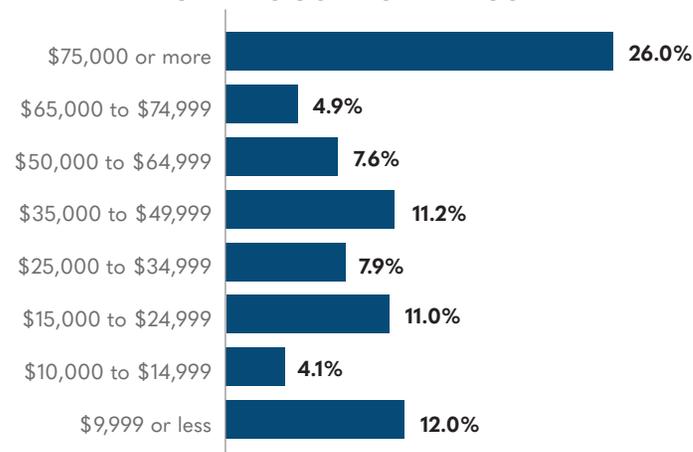
### INCOME

Between 2016 and 2020, the median household income in South Carolina was \$54,864. For the same time period, the Town of Fort Mill's median household income was almost double at \$101,699. Although 26.0 percent of the study area's population makes \$75,000 or more a year, 12.0% of the population make \$9,999 or less.

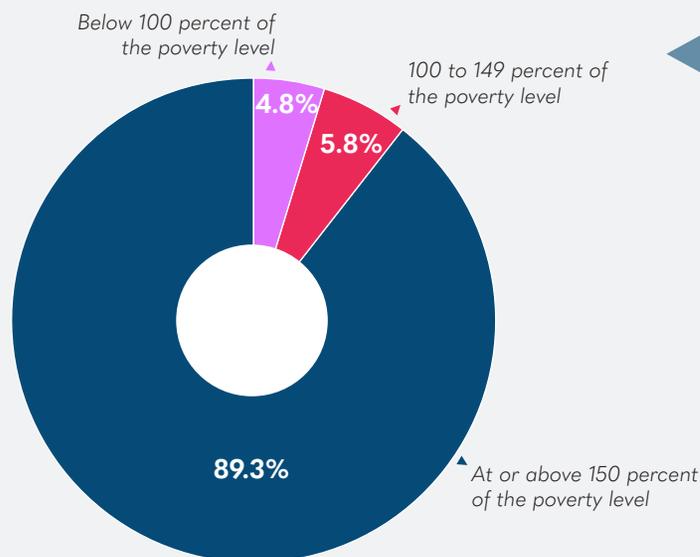


**26.0%**  
of Fort Mill's  
population makes  
*\$75,000 or more*  
(annually)

### ANNUAL HOUSEHOLD INCOME



### RESIDENTS LIVING BELOW POVERTY LEVEL (LAST 12 MONTHS)



### RESIDENTS LIVING BELOW POVERTY LEVEL

Approximately 5 percent (4.8%) of Fort Mill's residents live below the poverty line. This is lower than the state's average of 14.7 percent.

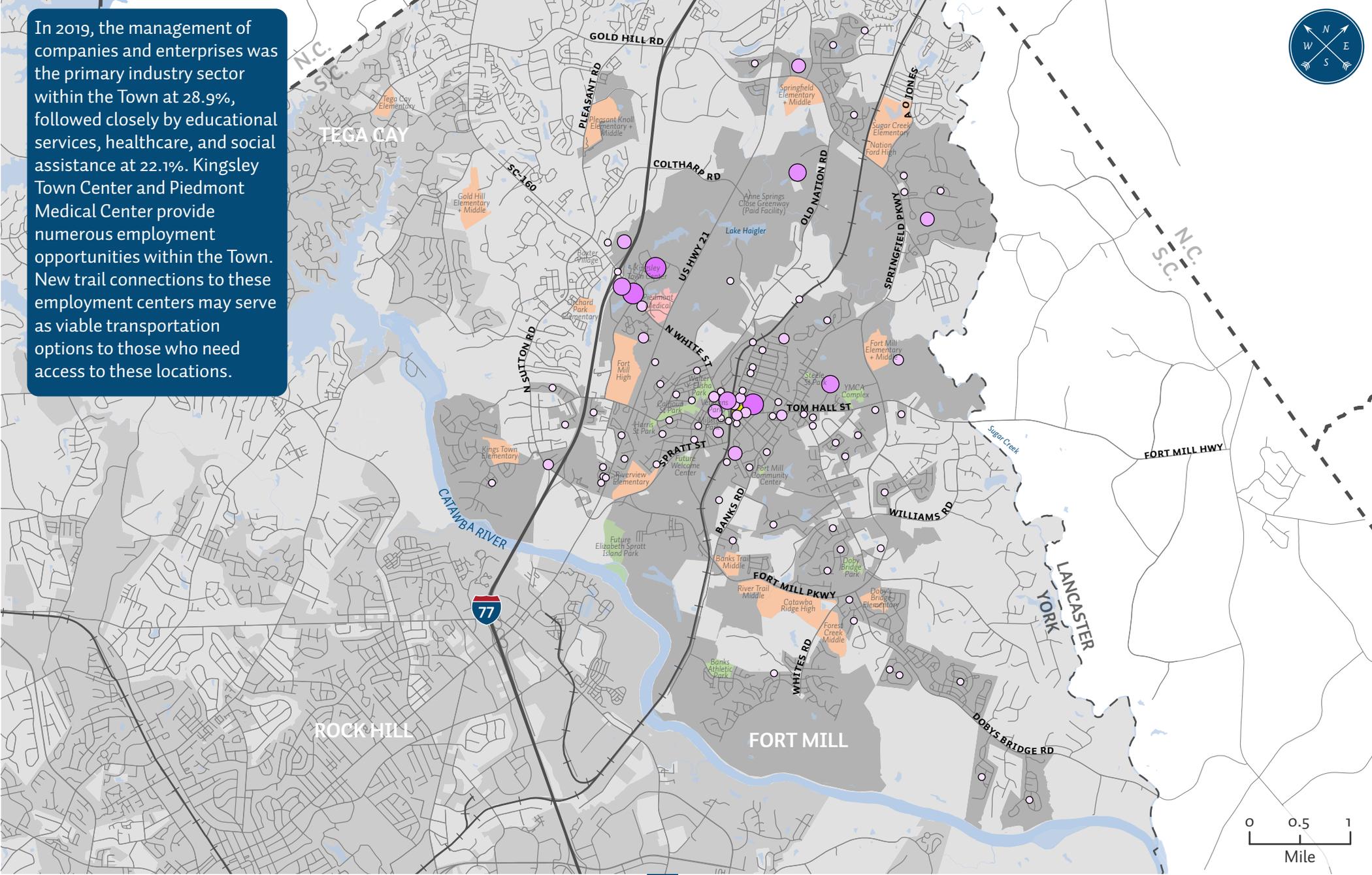
### LANGUAGE

A little over 7 percent of the population in South Carolina speaks a language other than English at home. By comparison, 6.1% of Fort Mill's population speaks a language other than English. These values do not consider the potential multi-lingual nature of households, but only the primary self-reported language spoken by all members of the household.



**6.1%**  
of Fort Mill's  
population speaks  
*a language other*  
*than English*

In 2019, the management of companies and enterprises was the primary industry sector within the Town at 28.9%, followed closely by educational services, healthcare, and social assistance at 22.1%. Kingsley Town Center and Piedmont Medical Center provide numerous employment opportunities within the Town. New trail connections to these employment centers may serve as viable transportation options to those who need access to these locations.



# FORT MILL TRAIL MASTER PLAN

## Employment Density (2019 - All Jobs)

LEGEND

- 1 - 50 Jobs
- 51 - 150 Jobs
- 151 - 300 Jobs
- 301 - 600 Jobs
- 601 - 2,698 Jobs

- Road
- Railroad
- Fort Mill School
- Fort Mill Park
- Hospital
- Downtown/Main St
- Water
- Fort Mill
- Other Jurisdiction
- York County
- County Boundary

## GREENWAY + TRAIL BENEFITS

Individual trail segments are often viewed only as recreational amenities; however, through strategic network expansions they can provide a variety of benefits to the communities and surrounding regions they serve. Additional benefits from trails and potential impacts of trail development or similar facilities like greenways and multi-use paths include the following:

- ▶ Improving bicycle and pedestrian transportation **mobility**
- ▶ Improving **health and well-being** through active living
- ▶ Protecting and enhancing the **environment**
- ▶ Improving **equity** through a fair distribution of facilities
- ▶ Creating value and generating a strong **economy**
- ▶ Promoting unity and a sense of **community**

These benefits, among others, can help justify trail investments within communities as well as help determine whether these projects are important enough to be prioritized for funding. Each benefit is outlined in this section with case study examples throughout the country and within the state of South Carolina.



Riverwalk, Rock Hill

## Mobility

Trails can be used for both recreational and transportation-related purposes. Trails serve as off-road linkages to neighborhoods, as well as centers for shopping, entertainment, education, entertainment, and employment. Off-road and separated facilities like trails, greenways, and multi-use paths provide users with a safe and enjoyable way to travel. Oftentimes, they are used for short trips, especially those trips less than one mile.

More than 45 percent of all driving trips in the United States are under 3 miles, and 60 percent of trips are less than 5 miles. These trips, which could be taken by bike or on foot in 20 to 30 minutes, represent opportunities for mode shifts to biking and walking in communities across the United States. There is a great opportunity to increase mode shares of active transportation by investing in trails. Communities that are increasing their active transportation mode shares invest in well-connected, multi-modal networks that allow people of all ages and abilities to bike and walk to their desired destinations.

Reducing distances between key destinations is an effective way to attract residents and visitors to bicycle and walk to locations that are closer to home. Combining this practice with the improvement of existing trails, greenway, and parking makes the use of the facilities more predictable and comfortable for users. Improvements to such facilities may include crosswalk upgrades at intersections, upgraded transit shelter amenities that tie into nearby trail systems, filling in trial network gaps with sidewalks or multi-use paths, and constructing paths to schools for children to safely access (USDOT).

The Carolina Thread Trail is a regional example of a trail system that enhances mobility options within communities. This trail helps to connect neighborhoods and adjoining communities, connects children with nature, and promotes regional thinking among its communities. It also strategically designs its trails by incorporating public art, helping its users reconnect and recognize the local history of the land it traverses.



American Tobacco Trail, Durham

## Health + Well-being

According to the USDOT, approximately one in four adults in the United States report that they do not engage in any physical activity outside of their jobs. The US Department of Health and Human Services (DHHS) estimates that inactivity contributes to \$117 billion in annual healthcare costs nationwide and causes nearly 10 percent of premature mortality. To improve and support physical and mental health, DHHS recommends adults get at least 150 minutes of moderate-intensity exercise, like brisk walking or bicycling, per week. When local municipalities invest in active transportation facilities like trails and multi-use paths, they invest in the overall health of their communities. Trails provide a safe and comfortable way for people to engage in regular physical activity. Providing walkable and/or bikeable access to key destinations such as schools, shopping areas, jobs, neighborhoods, and recreation facilities promotes active lifestyles, provides opportunities for residents to meet health goals, and reduces dependence on automobiles.

Recent 2020 data from the Centers for Medicare and Medicaid Services (CMS) Office of the Actuary reported that the per capita personal health care spending in South Carolina was \$8,766 in 2020. This is a 17.7 percent increase from 2015 (\$7,446). The South Carolina Department of Health and Environmental Control found that almost 76 percent of York County residents reported that they spent leisure time physical activity in the past 30 days between 2016 and 2018 (ranked 4th in the state). Implementation of the proposed projects in this plan may help increase the percentage of leisure time physical activity that York County residents report in the future.



## Environment

Trails and greenways minimize the impact on biodiverse lands across their landscapes. Greenways and trails can play an important role in improving water quality and mitigating flooding. Facilities may be used to preserve open space that provides natural buffer zones to protect streams, rivers, and waterbodies from pollution run-off. According to the Federal Emergency Management Agency (FEMA), flooding causes over \$1 billion in property damages every year. A benefits study written by the Rails to Trails Conservancy on trails and greenways states that these facilities can serve as natural floodplains by absorbing excess water; restoring developed floodplains to their natural state can mitigate flooding within communities. Additionally, many greenways and trails are located in forested or vegetated landscapes, and they can serve as protective buffers for wildlife habitat areas. These buffers are critical to supporting wildlife populations that require safe routes for migration.

Statewide trail planning efforts in South Carolina also consider the natural environment. For example, the South Carolina Department of Parks, Recreation and Tourism (SCPRT) operates the central trails program in South Carolina (including the State Trails Program) and led the 2022 *State Trails Plan*. One goal from the plan was to focus on sustainability to guide the process. The plan also noted that trail resources would focus on promoting trails that are sustainable over the long term. According to the plan, “sustainability means that the trails are designed, constructed, and maintained in such a manner that the environment is respected, long term maintenance is kept to a minimum and that associated costs are within the reach of the land manager.” Utilizing the latest best management practices, concepts, materials, and maintenance techniques were also listed as priorities.



## Equity

The costs of owning, maintaining, fueling, insuring, and registering a car are great and can be a tremendous burden on many households. A bicycle, on the other hand, is a relatively inexpensive investment, and walking is free. For those who cannot afford a private vehicle, trails offer a convenient and economical way to get around the city and access shops, schools, and jobs. Trails can also help provide additional safe mobility options for those with disabilities, and connect communities underserved by pedestrian and bicycle infrastructure.

For some communities, access to outdoor recreation has not always been easy to obtain and must be reached by car. Access to outdoor recreation for parks and recreation opportunities are historically less abundant in low-income neighborhoods where minorities reside. Providing equitable access to all users within a community is fundamental to reducing instances of inequality. Historically, disadvantaged communities have been overlooked or ignored when it comes to planning and involving them engagement efforts for future project connections. A study conducted by Headwater Economics found that these disparities in access to trails and parks have significant health, social, and economic implications, further exacerbating existing environmental justice concerns in these communities.



## Economic Activity

Outdoor recreation opportunities such as trails and greenways contribute to positive economic impacts to the communities they are constructed in. For instance, property values of homes in many communities have been shown to increase by as much as 6.5% when access to trails is included. Additionally, in the real estate market, close trail access can be an attractive amenity that increases interest in a property. Many developers also include trails as recreational amenities in new developments. In Headwater Economics' review on the business impact of trails, the study noted that the economic impact from trails is highest when a trail is connected to local businesses that cater to trail user needs, including restaurants, grocery stores, camping, hotels, and gear stores. Since lodging typically accounts for the greatest expense during a trip, the study also found that a trail's economic impact is greatly increased when it attracts more overnight users.

The Swamp Rabbit Trail is a regional example of a newly constructed multi-use path that is fostering economic development. Located in Greenville, South Carolina, the Swamp Rabbit is a 13.5-mile walking and bike trail that connects the City of Travelers Rest to Greenville. Greenville County Recreation estimates the trail's economic impact on the county is \$7 million per year. The direct link between Travelers Rest and Greenville allows multiple jurisdictions to capitalize on the trail. Since its development, several shops and restaurants have started catering to cyclists and walkers in the area. In addition, a bike-friendly bed and breakfast, the Swamp Rabbit Inn and Properties, accommodates trail users wishing to stay and explore the area surrounding the trail corridor. Newly constructed outdoor recreation opportunities, such as the Swamp Rabbit Trail, have successfully attracted new residents, developments, businesses, and contribute to culture and a sense of pride for the communities that envision and build them.



*Credit: Kris Decker - Firewater Photography*

## Community

Trails help foster a sense of place and community by connecting people and key destinations. People often feel more welcomed and engaged when connections linked trails provide opportunities for neighbors to gather, bind, and strengthen relationships. Trails can help bridge the gap between cultures, support alternative modes of travel, as well as serve as neutral gathering places and recreational areas for communities. The integration and promotion of trails within communities helps regions associate trails with specific places. In these cases, trails may become a symbol of the community, and in return ignites community pride.

The Anne Spring Close Greenway serves as a cherished asset within the Town. This facility provides numerous benefits to the Town, through outdoor activities, educational programming, gathering spaces, as well as a unique eatery along the greenway. The greenway serves as a key destination within the Town that celebrates the community's local culture and Catawba Indian history through interpretive signage. Providing similar signage or public art representative of a culture may help strengthen residents' cultural awareness.

In addition to celebrating the history and culture of a community, trails can also serve as the safest place within a community. A study on crime rates along the American Tobacco Trail in Durham, North Carolina, found that less than a half percent of area crimes occurred on the trail. With regular use, proper design (i.e., featuring lighting and clear sightlines users), and continued maintenance, users can remain safe and secure while on trails within their communities. Safety is even listed as one core value of the Anne Springs Close Greenway. Set hours, dedicated trail staff, guest services, trail status updates, and more help promote a safe environment for visitors utilizing the greenway.



**Resources:**

- ▶ Carolina Thread Trail – Trail Benefits: Connecting Communities: <https://www.carolinathreadtrail.org/trail-benefits/>
- ▶ Center for Disease Control and Prevention – Transportation Health Impact Assessment Toolkit (2011): [https://www.cdc.gov/healthyplaces/transportation/promote\\_strategy.htm](https://www.cdc.gov/healthyplaces/transportation/promote_strategy.htm)
- ▶ Headwater Economics – Measuring Trail Benefits: Business Impacts (2016): <https://headwaterseconomics.org/wp-content/uploads/trails-library-business-impacts-overview.pdf>
- ▶ Headwater Economics – Measuring Trail Benefits: Equitable Access (2018): <https://headwaterseconomics.org/wp-content/uploads/trails-library-access-overview.pdf>
- ▶ Institute for Transportation Research and Education – Evaluating the Economic Contribution of Shared Use Paths in NC (2018): <https://itre.ncsu.edu/focus/bike-ped/sup-economic-impacts/>
- ▶ National Association of City Transportation Officials – Equitable Bike Share Means Building Better Places for People to Ride (2016): [https://nacto.org/wp-content/uploads/2016/07/NACTO\\_Equitable\\_Bikeshare\\_Means\\_Bike\\_Lanes.pdf](https://nacto.org/wp-content/uploads/2016/07/NACTO_Equitable_Bikeshare_Means_Bike_Lanes.pdf)
- ▶ National Recreation and Parks Association – Trail Safety: The Perception and Reality: <https://www.nrpa.org/parks-recreation-magazine/2014/july/trail-safety-the-perception-and-reality/>
- ▶ Outdoor Industry Association – The Outdoor Recreation Economy (2017): [https://outdoorindustry.org/wp-content/uploads/2017/04/OIA\\_RecEconomy\\_FINAL\\_Single.pdf](https://outdoorindustry.org/wp-content/uploads/2017/04/OIA_RecEconomy_FINAL_Single.pdf)
- ▶ Rails to Trails Conservancy – Benefits of Trails and Greenways: <https://www.railstotrails.org/resourcehandler.ashx?id=2988>
- ▶ Reed, Julian A. - Greenville Health System Swamp Rabbit Trail Impact Study (2015): <http://greenvillerec.com/wp-content/uploads/2014/12/SRT-Impact-Study-Year-3-Final.pdf>
- ▶ Rock Hill/York County Convention & Visitor’s Bureau – Historic Paradise Community: <https://www.visitorkyorkcounty.com/listing/historic-paradise-community/3484/>
- ▶ South Carolina Department of Health and Environmental Control – South Carolina County Health Profiles (2016-2018): <https://gis.dhec.sc.gov/chp/>
- ▶ South Carolina Department of Parks, Recreation and Tourism – South Carolina State Trail Plan: [https://scprt.widen.net/s/gwczbr9v1k/statetrailsplanbook\\_2002](https://scprt.widen.net/s/gwczbr9v1k/statetrailsplanbook_2002)
- ▶ South Carolina Pedestrian and Bicycle Safety Action Plan (2022): <https://www.scdot.org/projects/pdf/SC%20Pedestrian%20and%20Bicycle%20Safety%20Action%20Plan.pdf>
- ▶ United States Department of Health and Human Services (2018). Physical Activity Guidelines for Americans, 2nd Edition. Accessed at: <https://health.gov/our-work/nutrition-physical-activity/physical-activity-guidelines/current-guidelines>



**KINGS TOWN**  
ELEMENTARY SCHOOL



# 02

## EXISTING CONDITIONS

### OVERVIEW

The *Fort Mill Trail Master Plan* surveyed existing conditions, reviewed relevant planning efforts and policies, and conducted an Opportunities and Constraints Analysis to gain a better understanding of the needs for the study area. The findings gathered during this analysis were used to inform policy, program, and network recommendations made in the *Trail Master Plan*.

Continue reading to learn more about the following sections that are included in this chapter:

- ▶ Existing Facilities
- ▶ Previous Planning Efforts
- ▶ Policy Review
- ▶ Opportunities + Constraints

## EXISTING FACILITIES

Following data collection, the project team conducted a one-day site tour (September 13, 2022) with Town staff to gather additional data for the *Trail Master Plan*. The team documented current conditions for bicyclists and pedestrians within the local transportation system, including existing and off-street trail networks and facilities, as well as the overall transportation network. Additional documentation was recorded on existing facilities along roadway corridors and at intersections, as well as information on roadway widths, shoulder treatments, lane markings, and posted speed limits. During the site visit, the team also made note of opportunities and constraints and observed maintenance issues and safety concerns at intersections or crossings. This information informed the Opportunities and Constraints Analysis, which is included at the end of this chapter. Photos from the site visit are provided within the Opportunities and Constraints Analysis with accompanying notes and captions.

This section contains annotated maps that cover the following topics within each consideration focus area:



### Human Environment Considerations

---

- Key Destinations
- Traffic Volume
- Speed Limit
- Utilities
- Transit
- Existing + Future Land Uses
- SCDOT Programmed Projects + Planned Roadways
- C-Fund + Critical Needs Program Projects
- Pennies for Progress Projects
- Bicycle + Pedestrian Crashes
- Existing Active Transportation Facilities
- Private Trails
- Planned Trail Segments
- Planned Parks



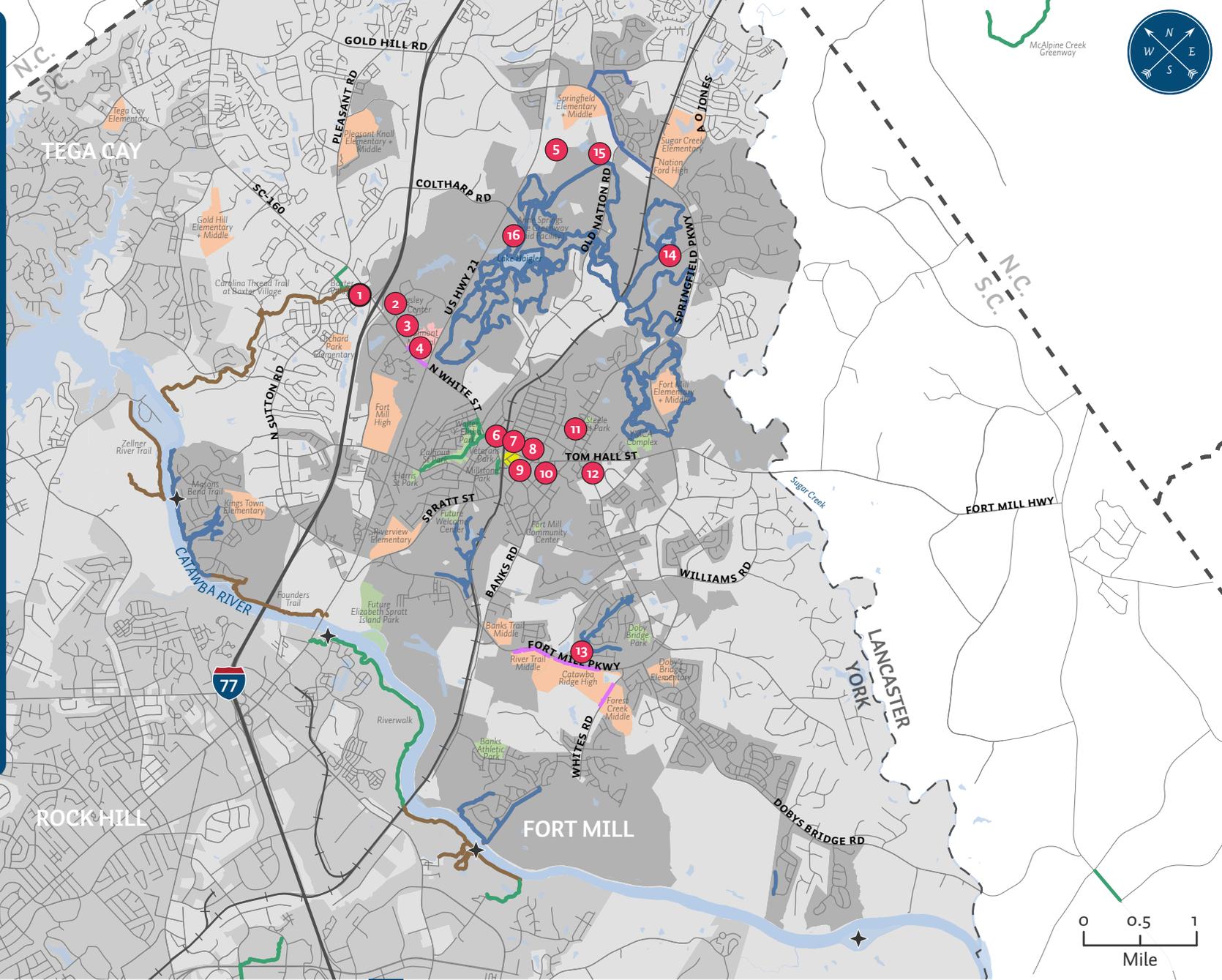
### Natural Environment Considerations

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- Endangered Species + Habitat Planning Areas
- Flooding
- Wetlands

The Steering Committee identified 16 key destinations that the public may want and/or need to access via multi-use paths and trails within the Town. Key destinations are listed below. Fort Mill's schools, parks, trails, and blueway access points are also included as destinations.

1. Fort Mill Library
2. Kingsley Shopping Center
3. Piedmont Medical Center
4. Historic Peach Stand
5. Comporium Amphitheater
6. Farmer's Market
7. Downtown Fort Mill + Historic District
8. Fort Mill Fire Department
9. Police Station
10. Fort Mill Town Hall
11. Historic Paradise Community
12. Walmart
13. Fort Mill Fire Station #2
14. Greenway Adventure Center
15. Dairy Barn
16. Lake Haigler

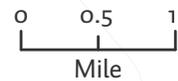


# FORT MILL TRAIL MASTER PLAN

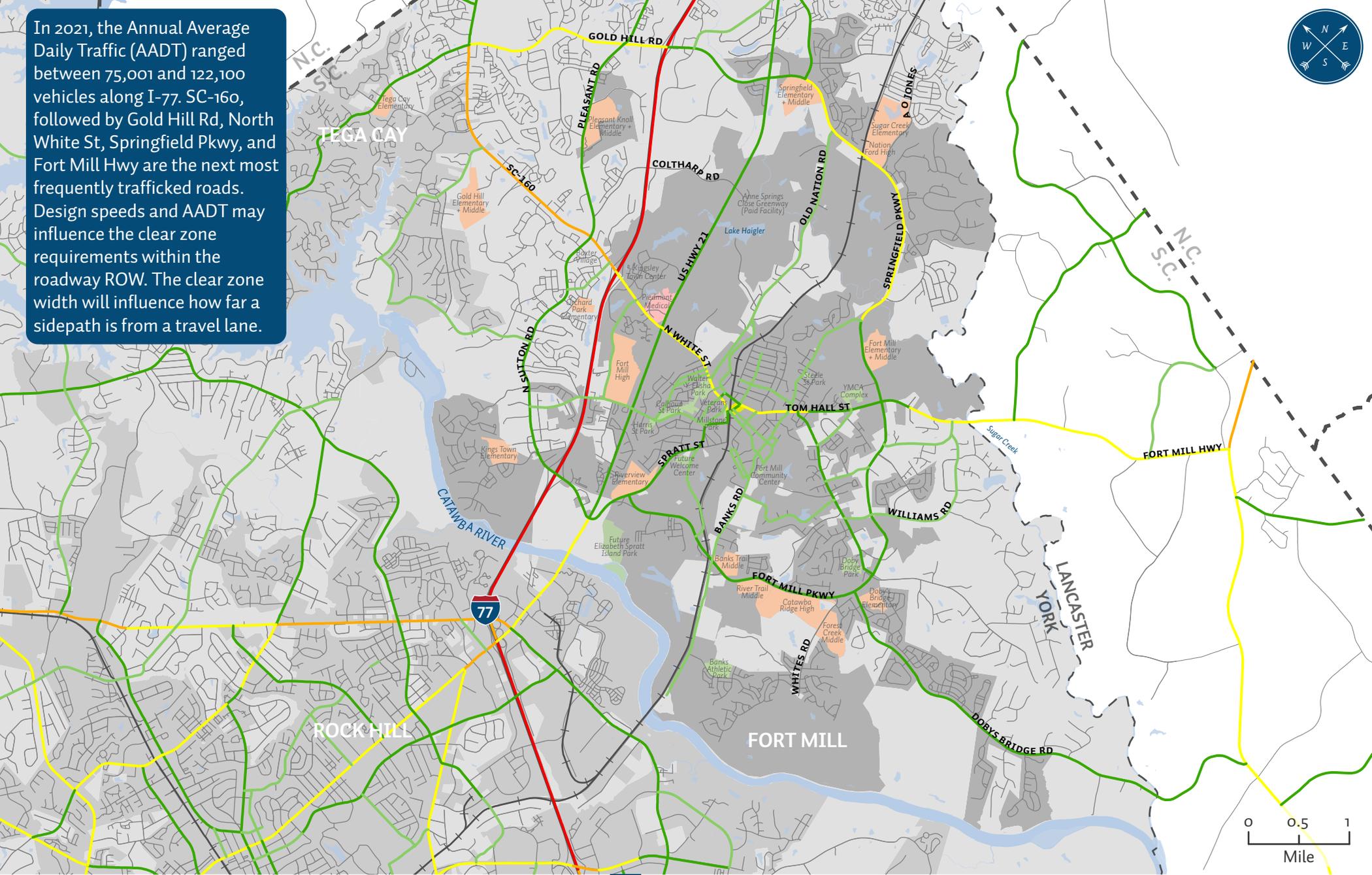
## Key Destinations

**LEGEND**

- Key Destination
- ◆ Blueway Access
- Road
- ⚓ Railroad
- Private Facility
- MULTI-USE PATH/TRAIL STATUS**
- Complete Paved Trail
- Complete Natural Surface Trail
- Complete Sidepath
- Fort Mill School
- Fort Mill Park
- Hospital
- Downtown/Main St
- Water
- Fort Mill
- Other Jurisdiction
- York County
- County Boundary



In 2021, the Annual Average Daily Traffic (AADT) ranged between 75,001 and 122,100 vehicles along I-77. SC-160, followed by Gold Hill Rd, North White St, Springfield Pkwy, and Fort Mill Hwy are the next most frequently trafficked roads. Design speeds and AADT may influence the clear zone requirements within the roadway ROW. The clear zone width will influence how far a sidepath is from a travel lane.



# FORT MILL TRAIL MASTER PLAN

## Traffic Volumes (AADT)

### LEGEND

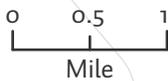
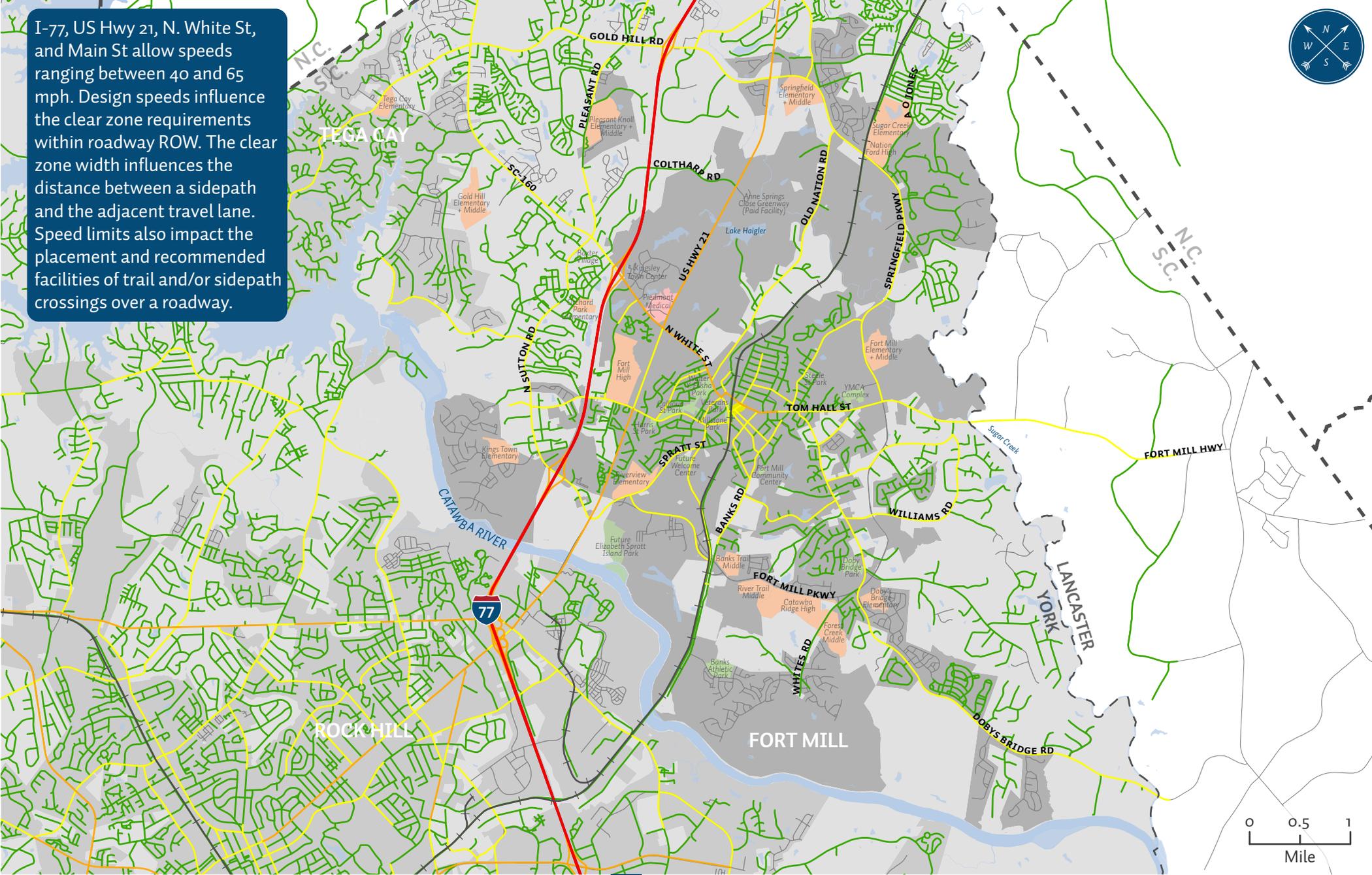
- AADT 2021**
- 125 - 5,500
  - 5,501 - 15,000
  - 15,001 - 35,000
  - 35,001 - 75,000
  - 75,001 - 122,100

- Road
- ++ Railroad

- Fort Mill School
- Fort Mill Park
- Hospital
- Downtown/Main St
- Water
- Fort Mill
- Other Jurisdiction
- York County
- County Boundary



I-77, US Hwy 21, N. White St, and Main St allow speeds ranging between 40 and 65 mph. Design speeds influence the clear zone requirements within roadway ROW. The clear zone width influences the distance between a sidepath and the adjacent travel lane. Speed limits also impact the placement and recommended facilities of trail and/or sidepath crossings over a roadway.



# FORT MILL TRAIL MASTER PLAN

## Roadway Speed Limits

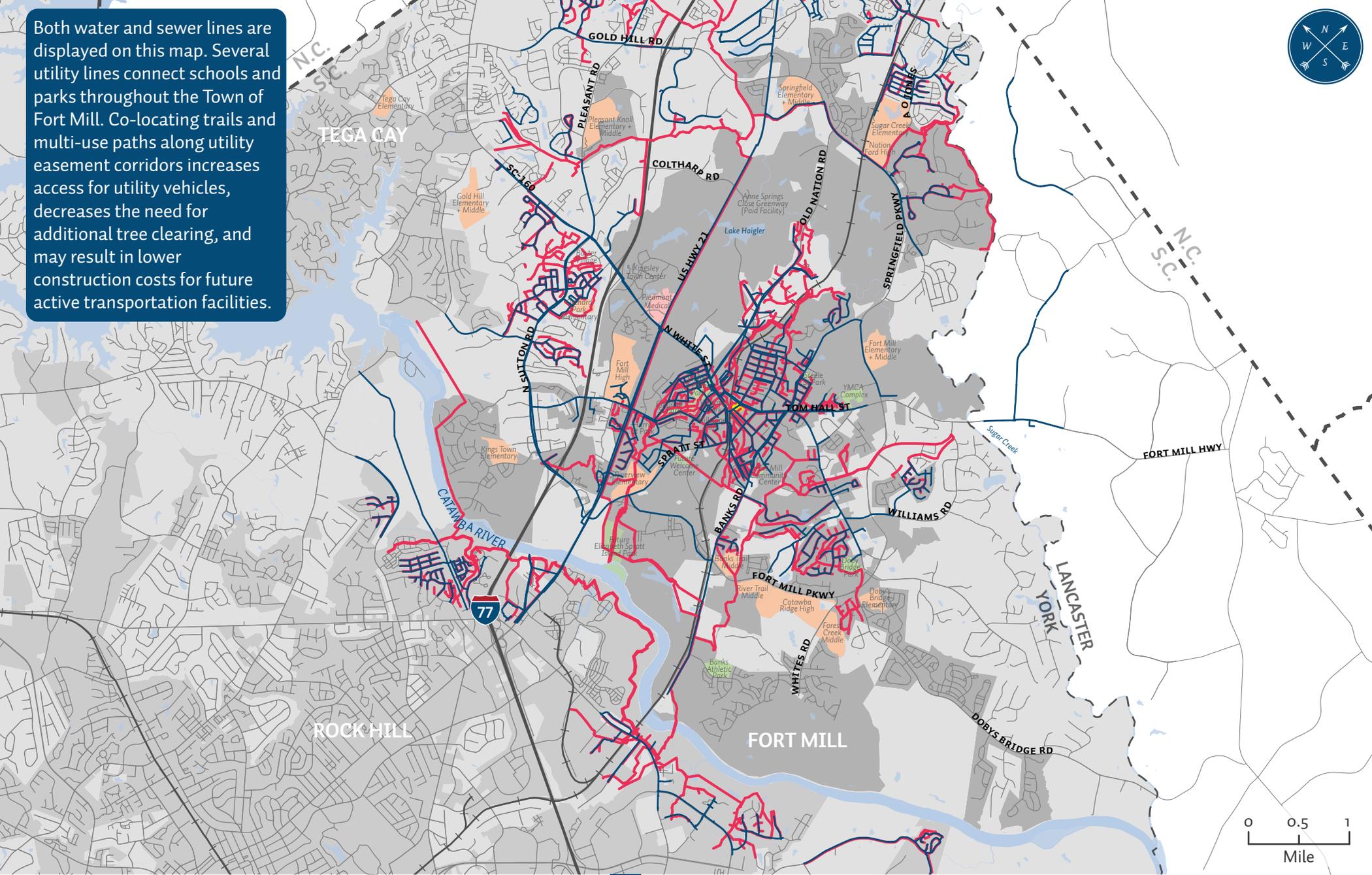
### LEGEND

- SPEED LIMIT (MPH)**
- 20 - 25 mph
  - 30 - 35 mph
  - 40 - 45 mph
  - 50 - 65 mph

- Road
- Railroad

- Fort Mill School
- Fort Mill Park
- Hospital
- Downtown/Main St
- Water
- Fort Mill
- Other Jurisdiction
- York County
- County Boundary

Both water and sewer lines are displayed on this map. Several utility lines connect schools and parks throughout the Town of Fort Mill. Co-locating trails and multi-use paths along utility easement corridors increases access for utility vehicles, decreases the need for additional tree clearing, and may result in lower construction costs for future active transportation facilities.



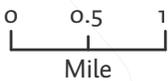
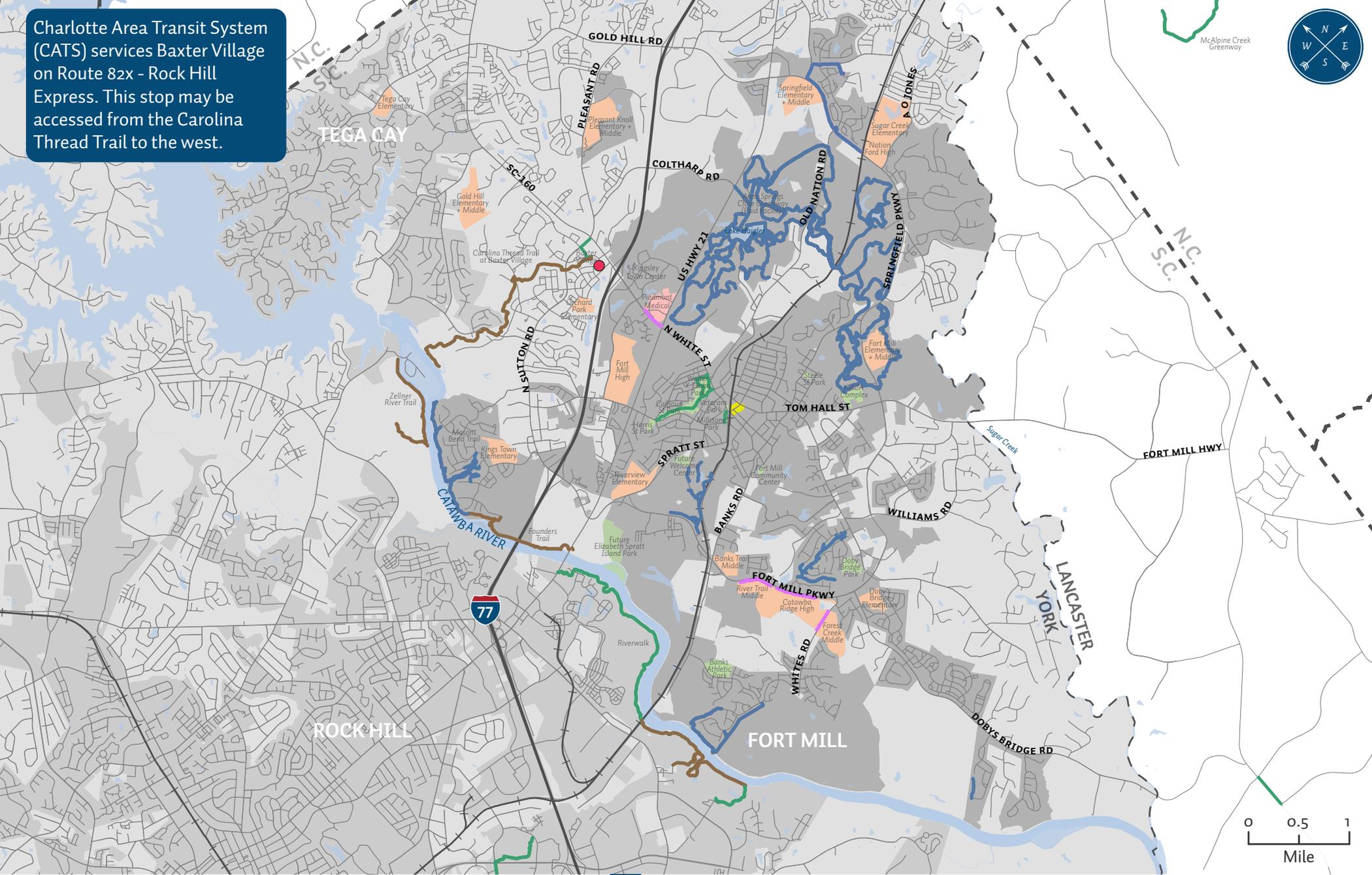
# FORT MILL TRAIL MASTER PLAN

## Utilities

### LEGEND

- Water Line
- Sewer Line
- Road
- Railroad
- Fort Mill School
- Fort Mill Park
- Hospital
- Downtown/Main St
- Water
- Fort Mill
- Other Jurisdiction
- York County
- County Boundary

Charlotte Area Transit System (CATS) services Baxter Village on Route 82x - Rock Hill Express. This stop may be accessed from the Carolina Thread Trail to the west.



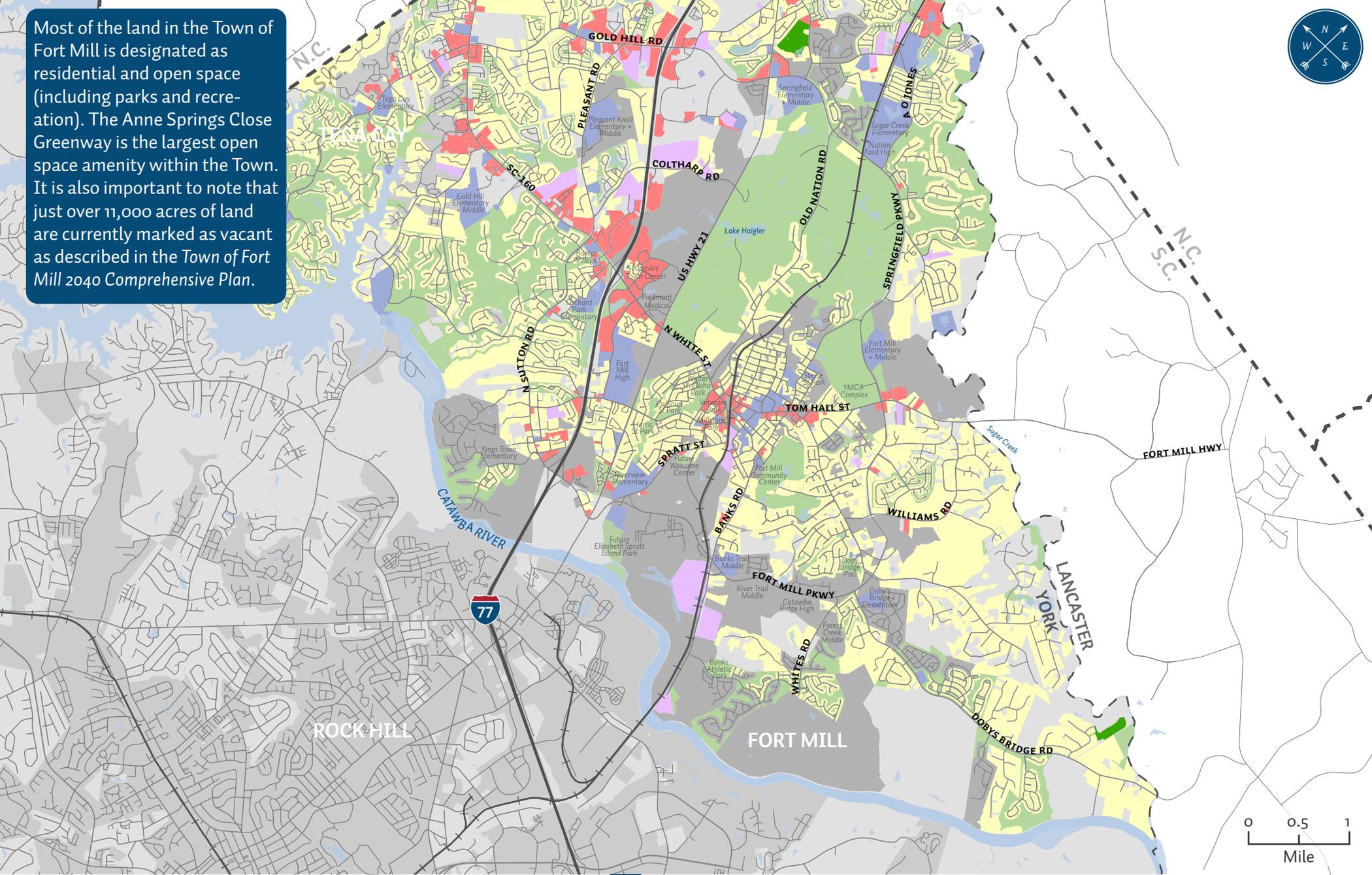
# FORT MILL TRAIL MASTER PLAN

## Transit Stop

LEGEND

- Transit Stop
  - Road
  - Railroad
  - Private Facility
- MULTI-USE PATH/TRAIL STATUS**
- Complete Paved Trail
  - Complete Natural Surface Trail
  - Complete Sidepath
- Fort Mill School
  - Fort Mill Park
  - Hospital
  - Downtown/Main St
  - Water
  - Fort Mill
  - Other Jurisdiction
  - York County
  - County Boundary

Most of the land in the Town of Fort Mill is designated as residential and open space (including parks and recreation). The Anne Springs Close Greenway is the largest open space amenity within the Town. It is also important to note that just over 11,000 acres of land are currently marked as vacant as described in the Town of Fort Mill 2040 Comprehensive Plan.



# FORT MILL TRAIL MASTER PLAN

## Existing Land Use

### LEGEND

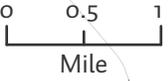
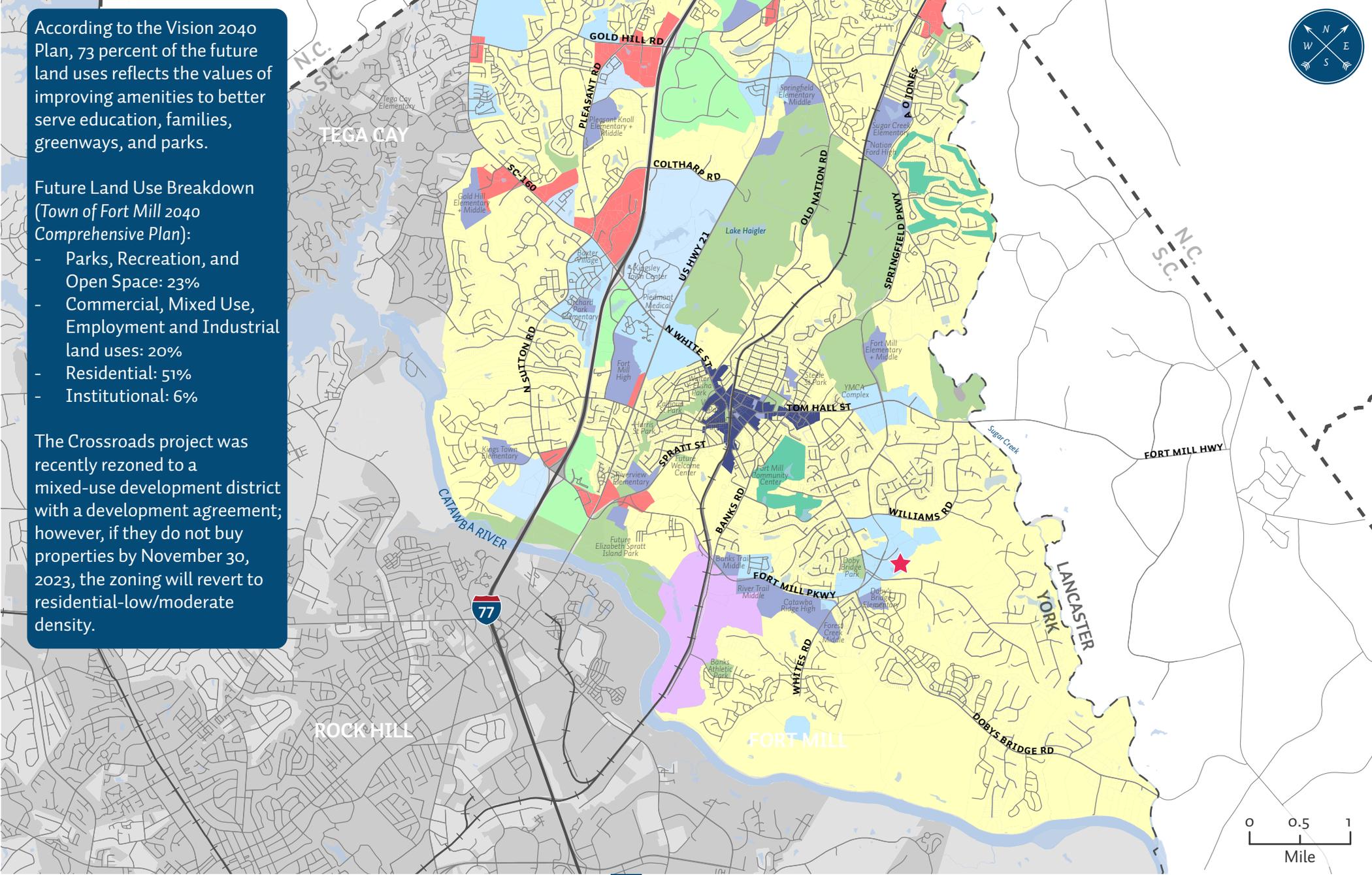
- Road
- ++ Railroad
- LAND USES**
- Agriculture
- Commercial
- Industrial
- Institutional
- Parks, Recreation, + Open Space
- Residential
- Water
- Fort Mill
- Other Jurisdiction
- York County
- County Boundary

According to the Vision 2040 Plan, 73 percent of the future land uses reflects the values of improving amenities to better serve education, families, greenways, and parks.

**Future Land Use Breakdown (Town of Fort Mill 2040 Comprehensive Plan):**

- Parks, Recreation, and Open Space: 23%
- Commercial, Mixed Use, Employment and Industrial land uses: 20%
- Residential: 51%
- Institutional: 6%

The Crossroads project was recently rezoned to a mixed-use development district with a development agreement; however, if they do not buy properties by November 30, 2023, the zoning will revert to residential-low/moderate density.



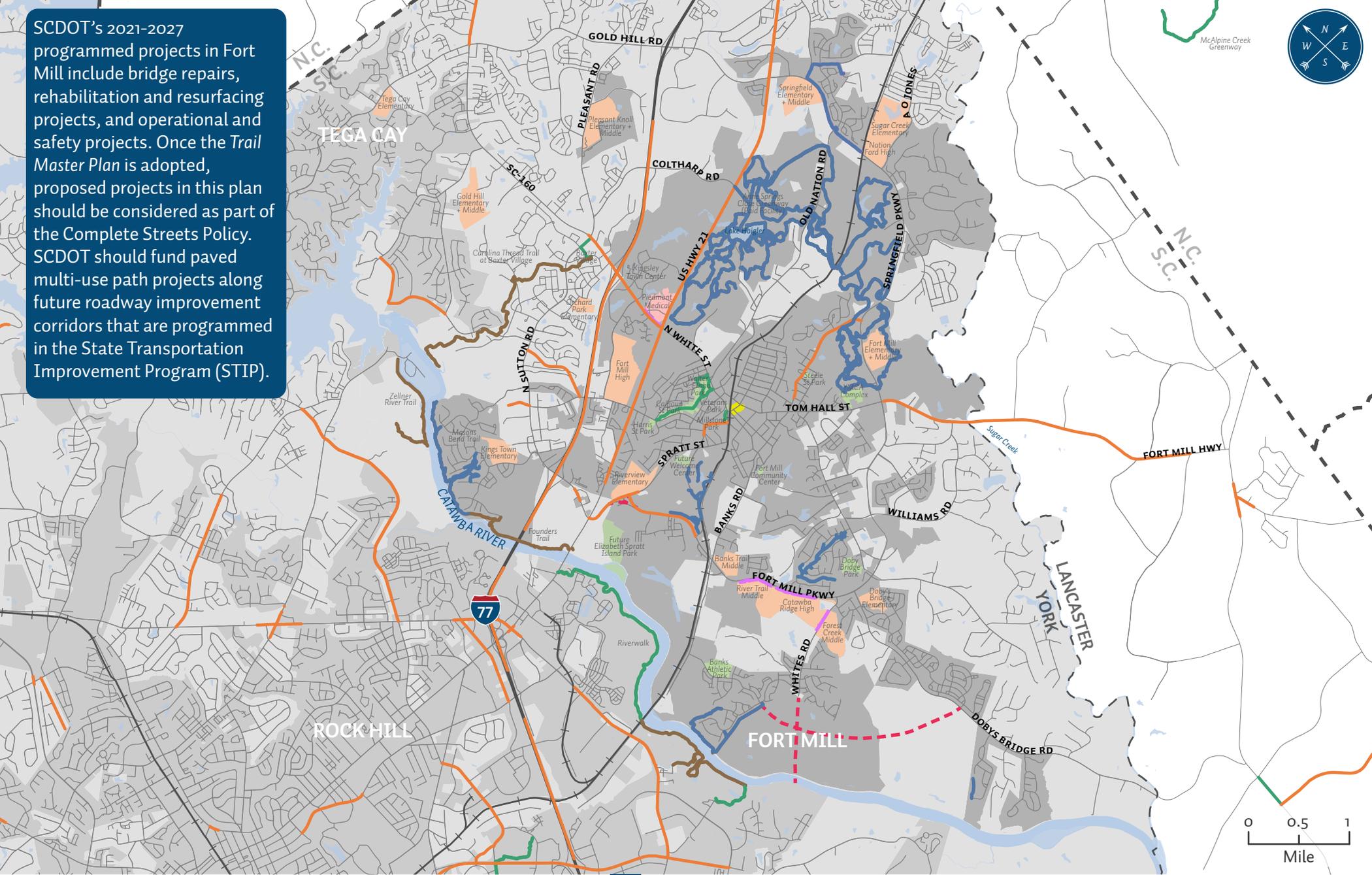
# FORT MILL TRAIL MASTER PLAN

## Future Land Use

### LEGEND

- Crossroads Project
- Commercial
- Employment
- Road
- Industrial
- Town Center
- Railroad
- Institutional
- Water
- Parks, Recreation, + Open Space
- Residential
- Fort Mill
- Golf Course
- Mixed Use
- Other Jurisdiction
- Institutional
- York County
- County Boundary

SCDOT's 2021-2027 programmed projects in Fort Mill include bridge repairs, rehabilitation and resurfacing projects, and operational and safety projects. Once the *Trail Master Plan* is adopted, proposed projects in this plan should be considered as part of the Complete Streets Policy. SCDOT should fund paved multi-use path projects along future roadway improvement corridors that are programmed in the State Transportation Improvement Program (STIP).



# FORT MILL TRAIL MASTER PLAN

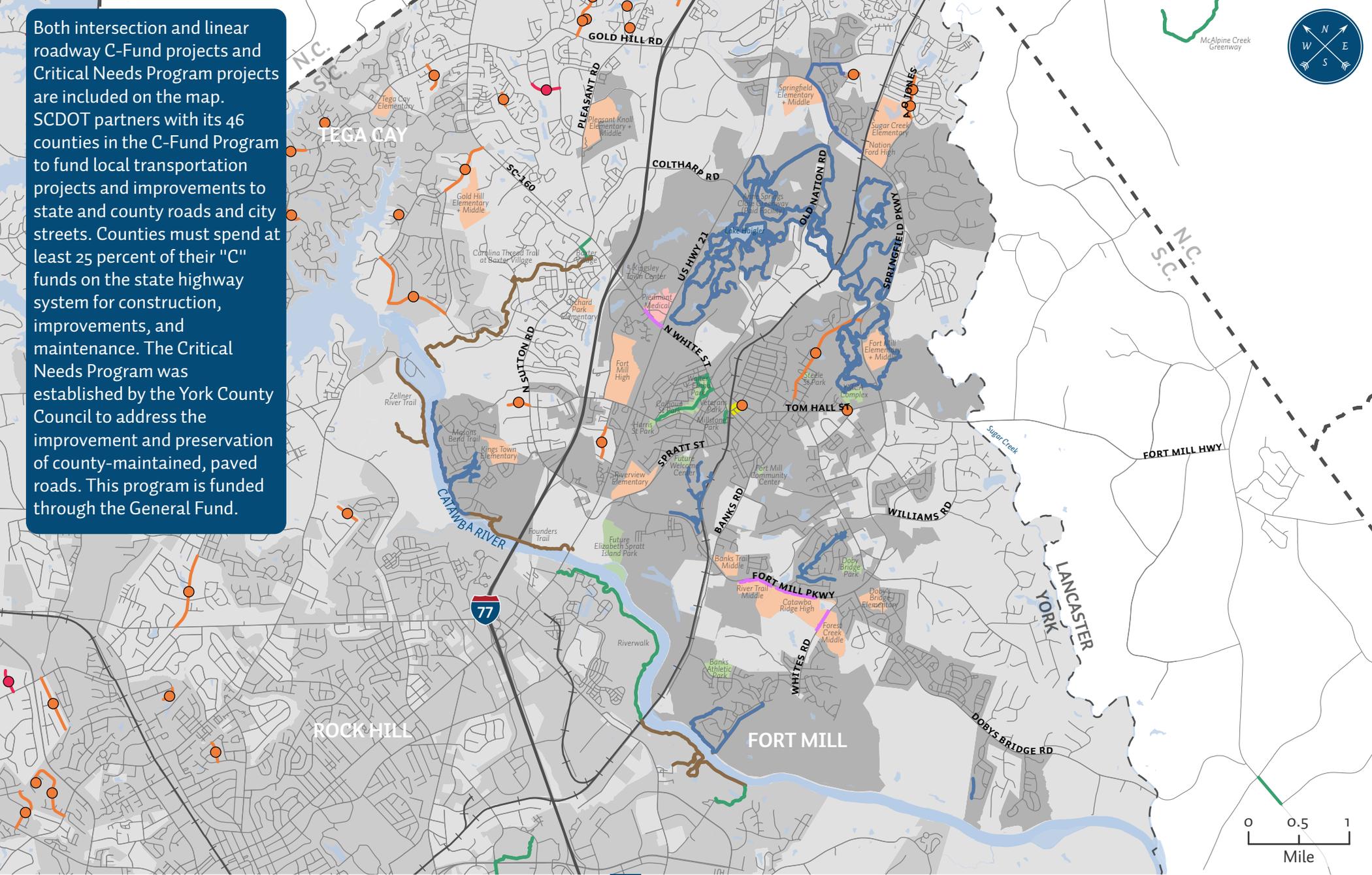
## SCDOT Programmed Projects + Planned Roadways

**LEGEND**

- Programmed Project
  - - - Planned Road
  - Road
  - + + Railroad
  - Private Facility
- MULTI-USE PATH/TRAIL STATUS**
- Complete Paved Trail
  - Complete Natural Surface Trail
  - Complete Sidepath
- Fort Mill School
  - Fort Mill Park
  - Hospital
  - Downtown/Main St
  - Water
  - Fort Mill
  - Other Jurisdiction
  - York County
  - County Boundary



Both intersection and linear roadway C-Fund projects and Critical Needs Program projects are included on the map. SCDOT partners with its 46 counties in the C-Fund Program to fund local transportation projects and improvements to state and county roads and city streets. Counties must spend at least 25 percent of their "C" funds on the state highway system for construction, improvements, and maintenance. The Critical Needs Program was established by the York County Council to address the improvement and preservation of county-maintained, paved roads. This program is funded through the General Fund.



# FORT MILL TRAIL MASTER PLAN

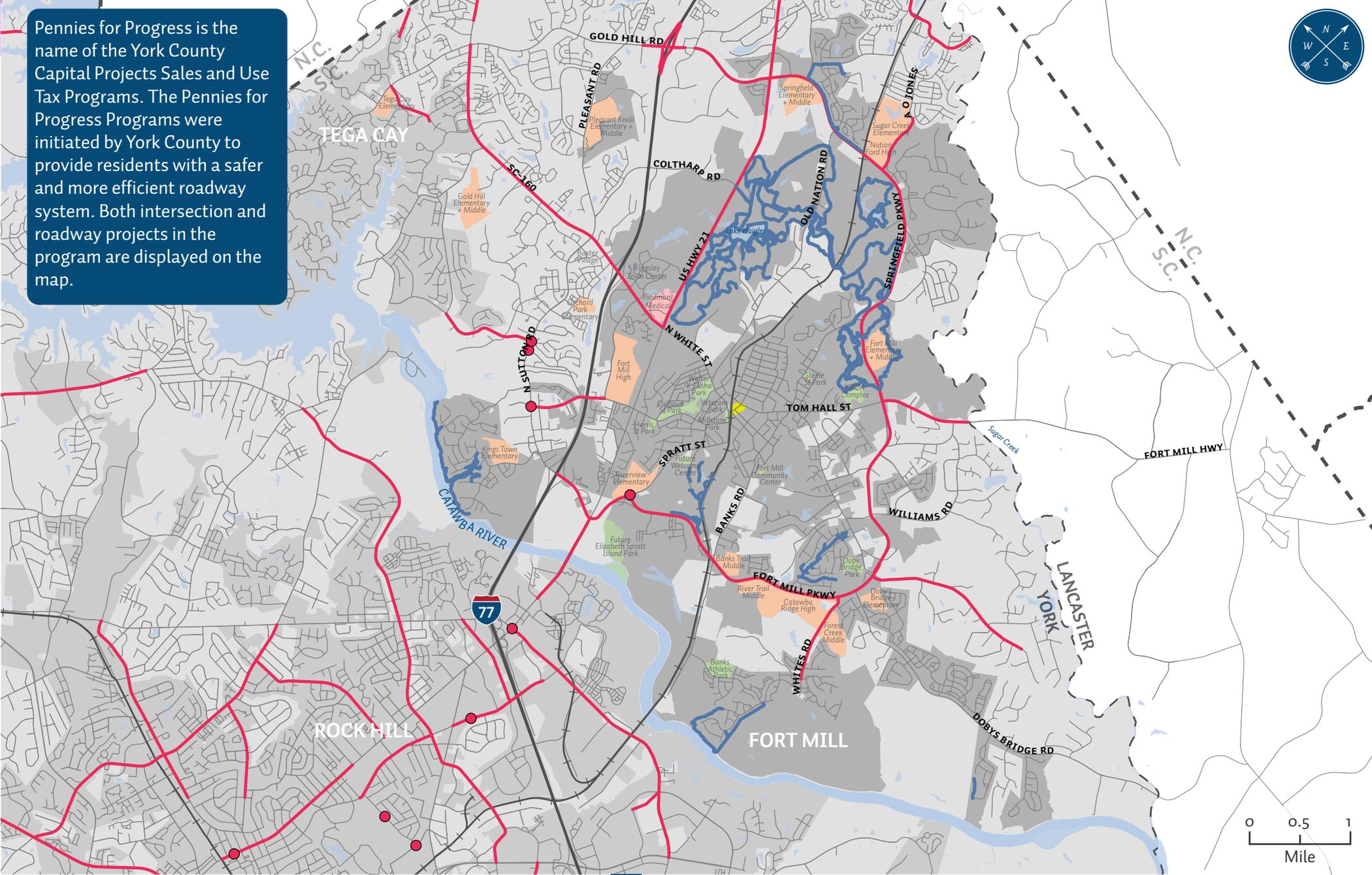
## C-Fund + Critical Needs Program Projects

### LEGEND

- Road
- ++ Railroad
- Private Facility
- PROJECT TYPES**
- Critical Needs
- C-Fund
- Critical Needs
- C-Fund
- MULTI-USE PATH/TRAIL STATUS**
- Complete Paved Trail
- Complete Natural Surface Trail
- Complete Sidepath
- Fort Mill School
- Fort Mill Park
- Hospital
- Downtown/Main St
- Water
- Fort Mill
- Other Jurisdiction
- York County
- County Boundary



Pennies for Progress is the name of the York County Capital Projects Sales and Use Tax Programs. The Pennies for Progress Programs were initiated by York County to provide residents with a safer and more efficient roadway system. Both intersection and roadway projects in the program are displayed on the map.

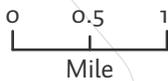


# FORT MILL TRAIL MASTER PLAN

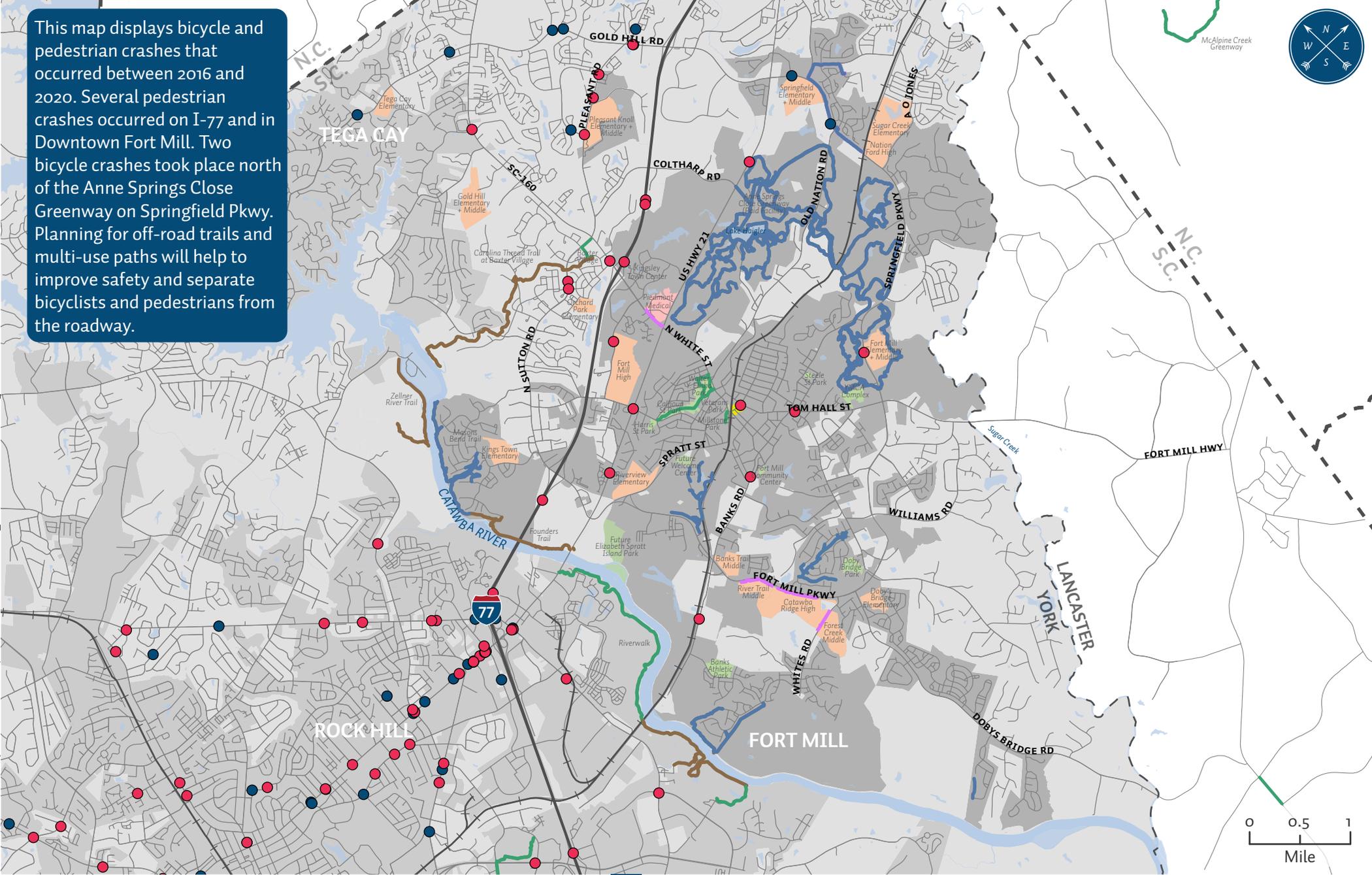
## Pennies for Progress Projects

### LEGEND

- Road
  - ⊕ Railroad
  - Private Facility
- PROJECT TYPES**
- Intersection Project
  - Roadway Project
- Fort Mill School
  - Fort Mill Park
  - Hospital
  - Downtown/Main St
  - Water
  - Fort Mill
  - Other Jurisdiction
  - York County
  - County Boundary



This map displays bicycle and pedestrian crashes that occurred between 2016 and 2020. Several pedestrian crashes occurred on I-77 and in Downtown Fort Mill. Two bicycle crashes took place north of the Anne Springs Close Greenway on Springfield Pkwy. Planning for off-road trails and multi-use paths will help to improve safety and separate bicyclists and pedestrians from the roadway.



# FORT MILL TRAIL MASTER PLAN

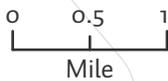
## Bicycle + Pedestrian Crashes (2016-2020)

### LEGEND

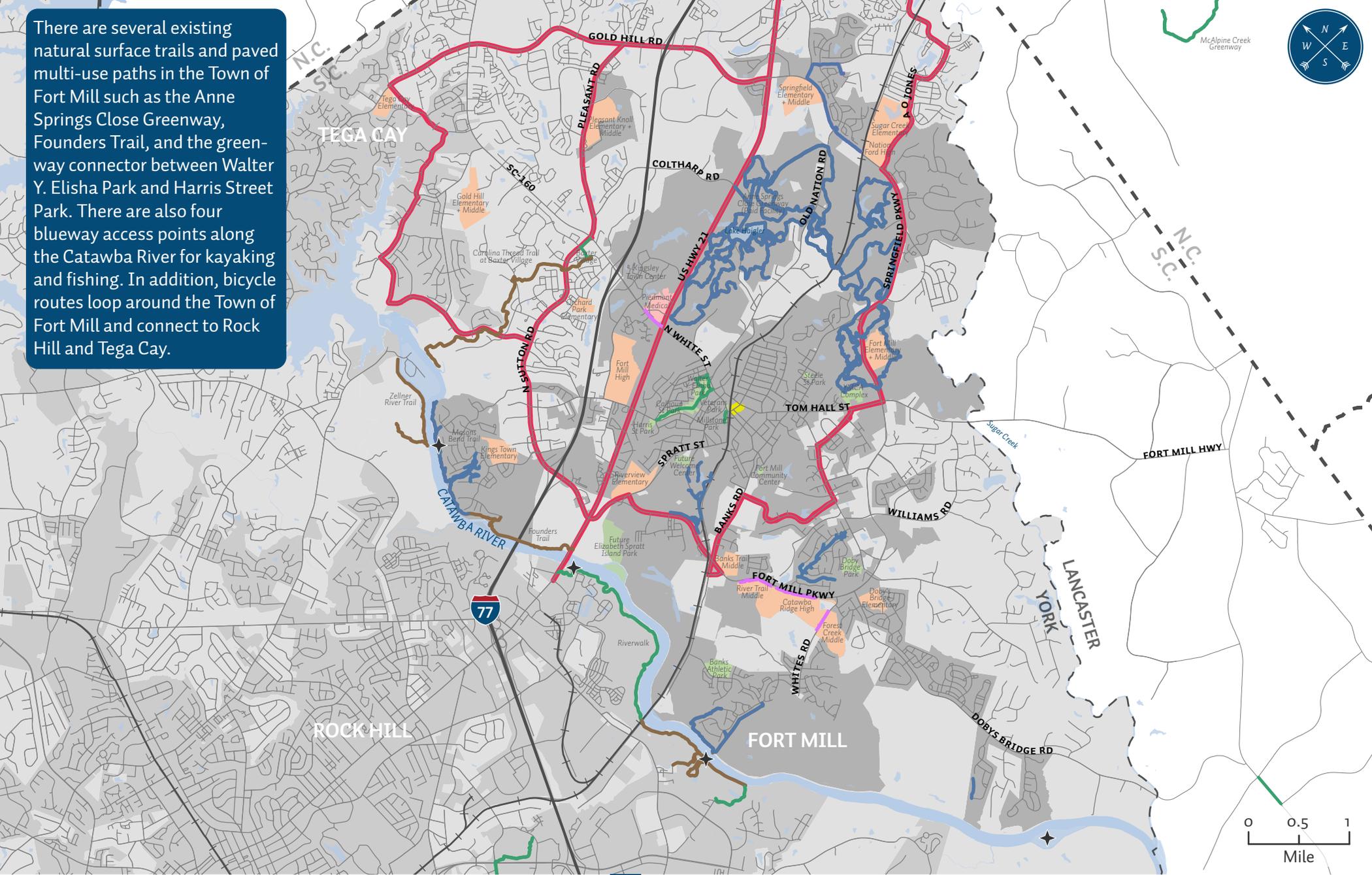
- Pedestrian Crash
- Bicycle Crash
- Road
- +— Railroad
- Private Facility

- MULTI-USE PATH/TRAIL STATUS**
- Complete Paved Trail
  - Complete Natural Surface Trail
  - Complete Sidepath

- Fort Mill School
- Fort Mill Park
- Hospital
- Downtown/Main St
- Water
- Fort Mill
- Other Jurisdiction
- York County
- County Boundary



There are several existing natural surface trails and paved multi-use paths in the Town of Fort Mill such as the Anne Springs Close Greenway, Founders Trail, and the greenway connector between Walter Y. Elisha Park and Harris Street Park. There are also four blueway access points along the Catawba River for kayaking and fishing. In addition, bicycle routes loop around the Town of Fort Mill and connect to Rock Hill and Tega Cay.



# FORT MILL TRAIL MASTER PLAN

## Existing Active Transportation Facilities

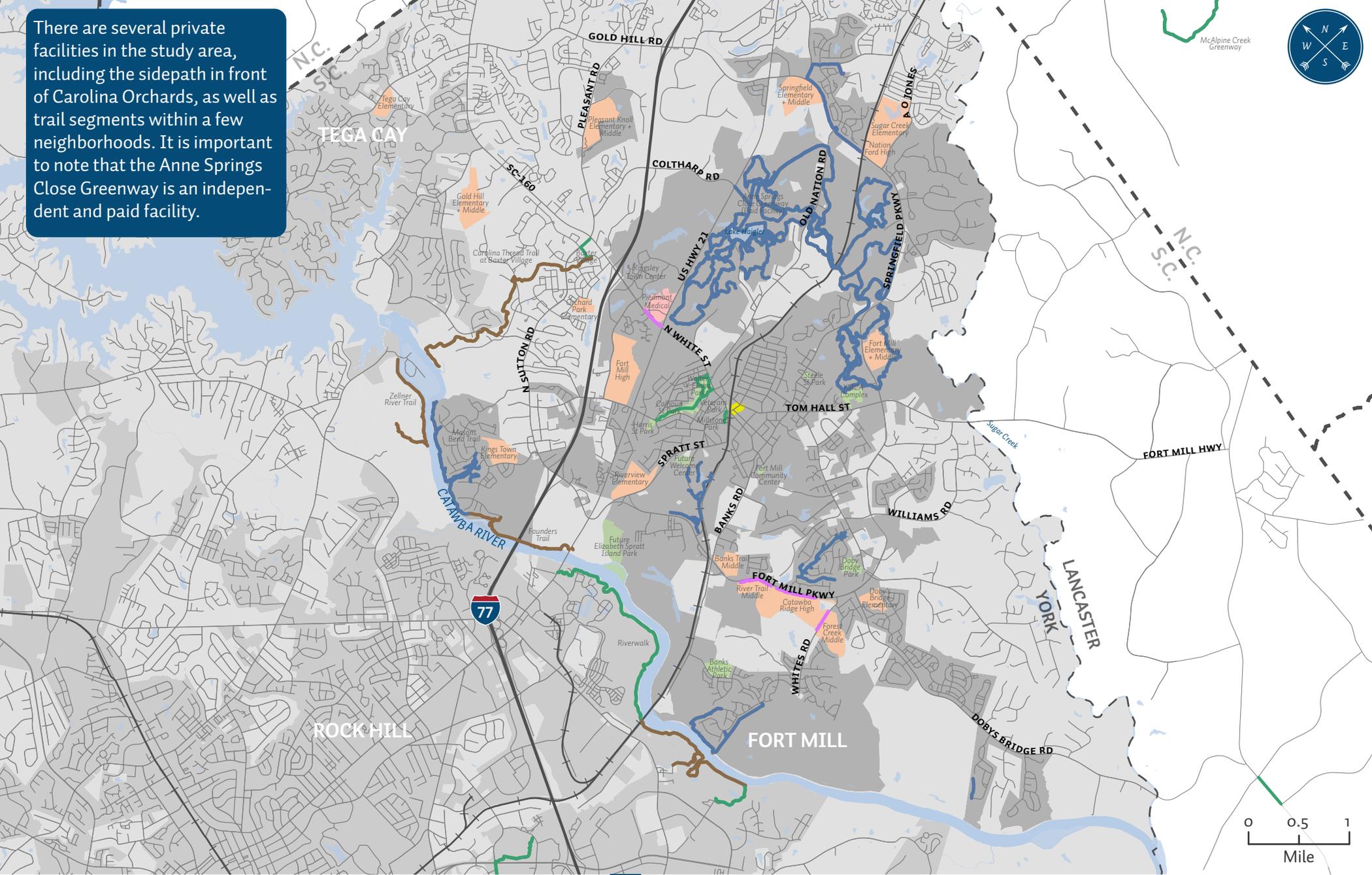
### LEGEND

- ★ Existing Blueway Access
- Road
- +— Railroad
- Private Facility
- Bicycle Route

- MULTI-USE PATH/TRAIL STATUS**
- Complete Paved Trail
  - Complete Natural Surface Trail
  - Complete Sidepath

- Fort Mill School
- Fort Mill Park
- Hospital
- Downtown/Main St
- Water
- Fort Mill
- Other Jurisdiction
- York County
- County Boundary

There are several private facilities in the study area, including the sidepath in front of Carolina Orchards, as well as trail segments within a few neighborhoods. It is important to note that the Anne Springs Close Greenway is an independent and paid facility.



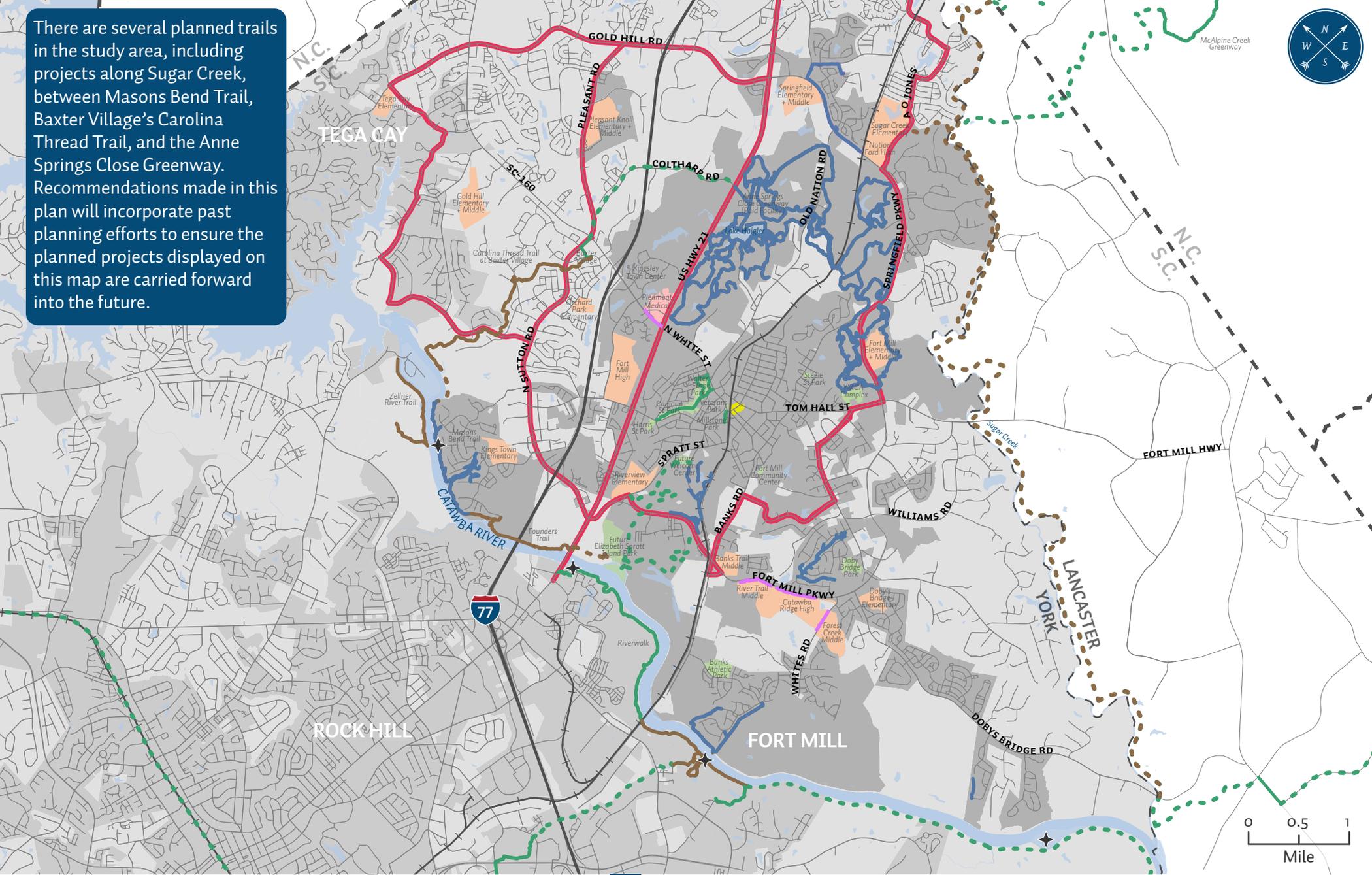
# FORT MILL TRAIL MASTER PLAN

## Private Trails

### LEGEND

- Road
- ⊕ Railroad
- Private Facility
- MULTI-USE PATH/TRAIL STATUS**
  - Complete Paved Trail
  - Complete Natural Surface Trail
  - Complete Sidepath
- Fort Mill School
- Fort Mill Park
- Hospital
- Downtown/Main St
- Water
- Fort Mill
- Other Jurisdiction
- York County
- County Boundary

There are several planned trails in the study area, including projects along Sugar Creek, between Masons Bend Trail, Baxter Village's Carolina Thread Trail, and the Anne Springs Close Greenway. Recommendations made in this plan will incorporate past planning efforts to ensure the planned projects displayed on this map are carried forward into the future.



# FORT MILL TRAIL MASTER PLAN

## Planned Trail Segments

### LEGEND

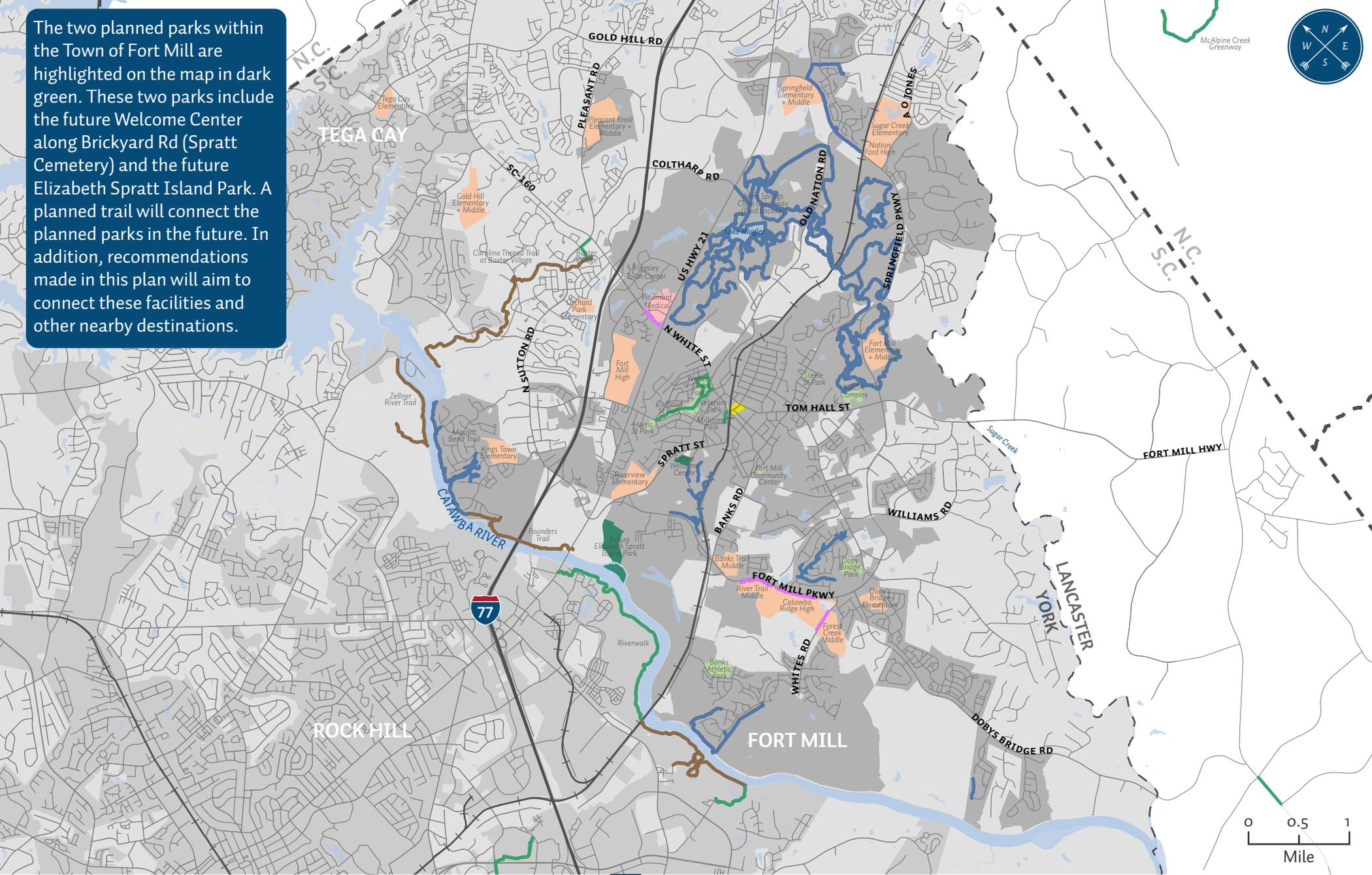
- ◆ Existing Blueway Access
- Road
- ⊕ Railroad
- Bicycle Route
- Private Facility

- MULTI-USE PATH/TRAIL STATUS**
- Complete Paved Trail
  - ⋯ Planned Paved Trail
  - Complete Natural Surface Trail
  - ⋯ Planned Natural Surface Trail
  - Complete Sidepath

- Fort Mill School
- Fort Mill Park
- Hospital
- Downtown/Main St
- Water
- Fort Mill
- Other Jurisdiction
- York County
- County Boundary



The two planned parks within the Town of Fort Mill are highlighted on the map in dark green. These two parks include the future Welcome Center along Brickyard Rd (Spratt Cemetery) and the future Elizabeth Spratt Island Park. A planned trail will connect the planned parks in the future. In addition, recommendations made in this plan will aim to connect these facilities and other nearby destinations.



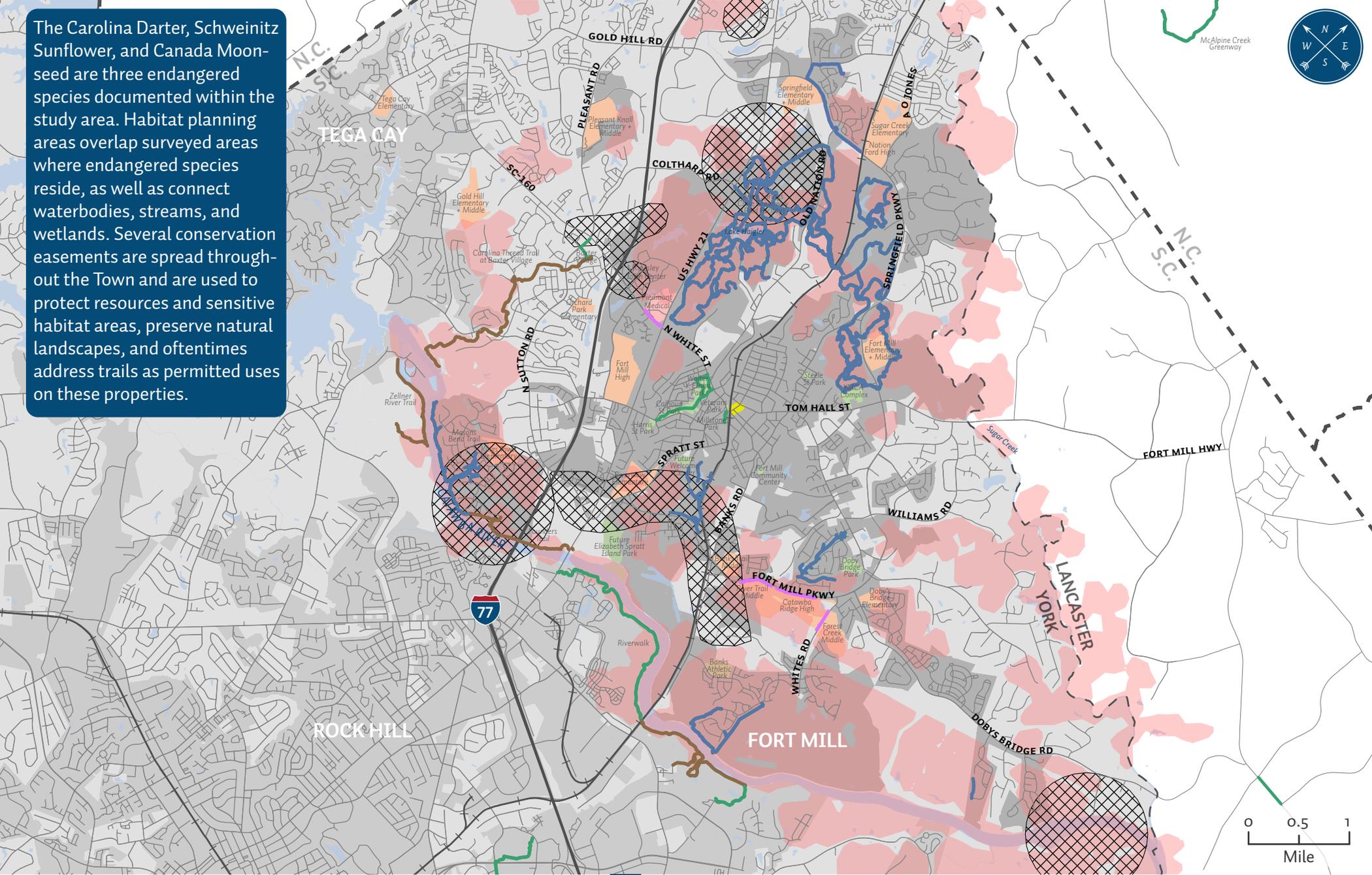
# FORT MILL TRAIL MASTER PLAN

## Planned Parks

### LEGEND

- Road
- ⊕ Railroad
- Private Facility
- Complete Paved Trail
- Complete Natural Surface Trail
- Complete Sidepath
- Planned Park
- Fort Mill School
- Fort Mill Park
- Hospital
- Downtown/Main St
- Water
- Fort Mill
- Other Jurisdiction
- York County
- County Boundary

The Carolina Darter, Schweinitz Sunflower, and Canada Moonseed are three endangered species documented within the study area. Habitat planning areas overlap surveyed areas where endangered species reside, as well as connect waterbodies, streams, and wetlands. Several conservation easements are spread throughout the Town and are used to protect resources and sensitive habitat areas, preserve natural landscapes, and oftentimes address trails as permitted uses on these properties.



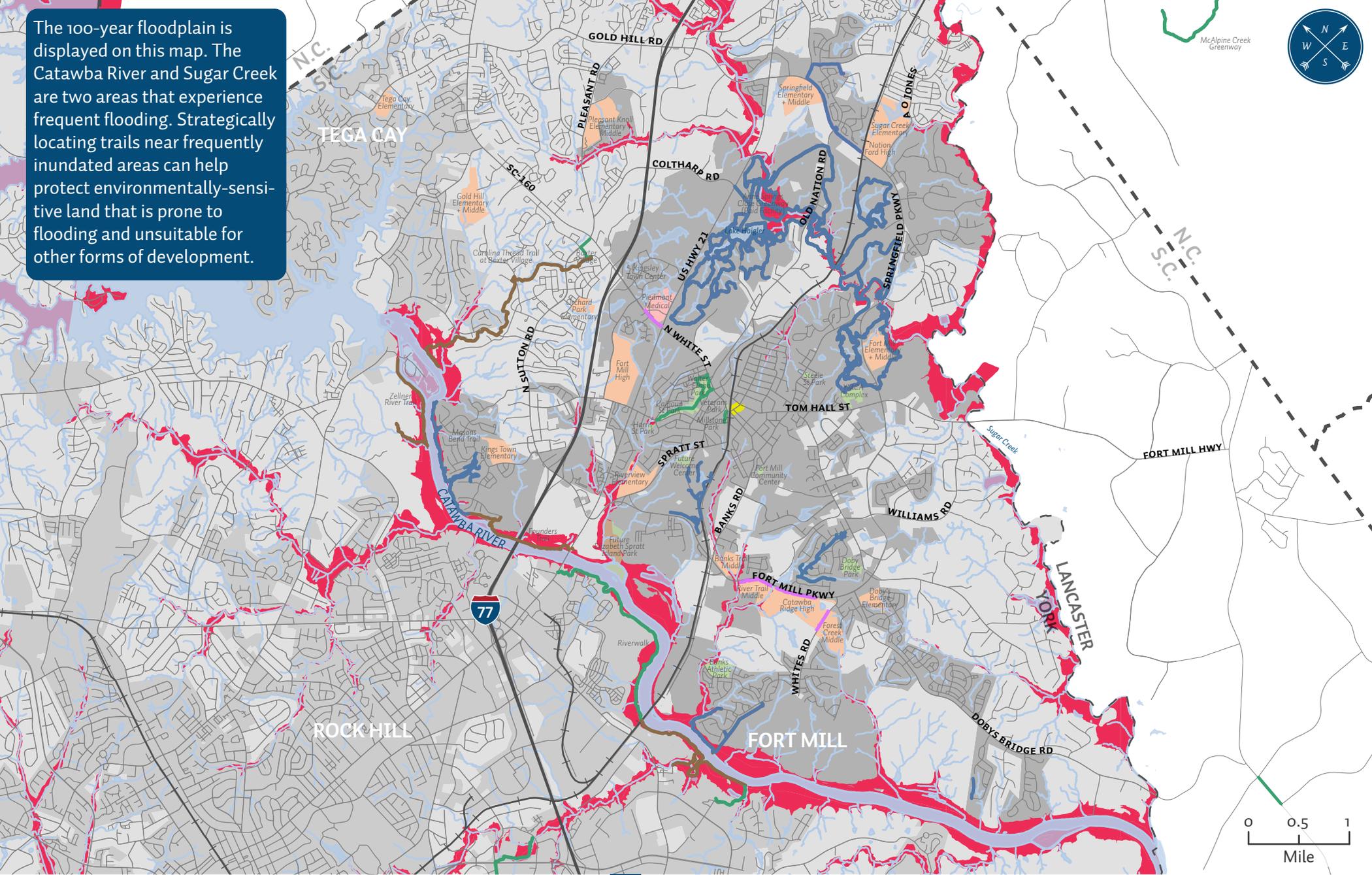
# FORT MILL TRAIL MASTER PLAN

## Endangered Species + Habitat Planning Areas

**LEGEND**

- Road
- ⊕ Railroad
- Private Facility
- Complete Paved Trail
- Complete Natural Surface Trail
- Complete Sidepath
- ⊗ Endangered Species
- Habitat Core Planning Area
- Fort Mill School
- Fort Mill Park
- Hospital
- Downtown/Main St
- Water
- Fort Mill
- Other Jurisdiction
- York County
- ⋯ County Boundary

The 100-year floodplain is displayed on this map. The Catawba River and Sugar Creek are two areas that experience frequent flooding. Strategically locating trails near frequently inundated areas can help protect environmentally-sensitive land that is prone to flooding and unsuitable for other forms of development.



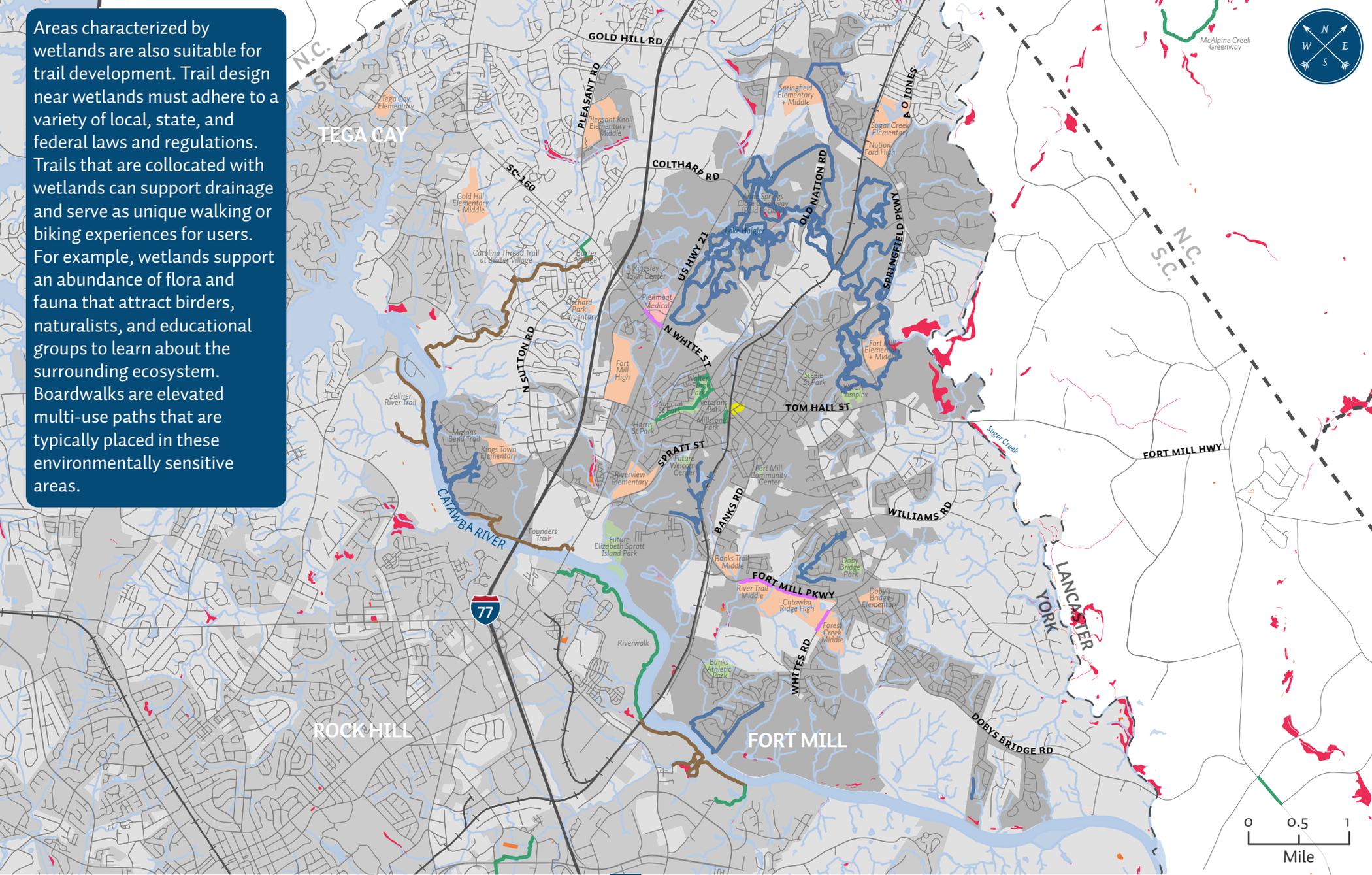
# FORT MILL TRAIL MASTER PLAN

## Flooding

### LEGEND

- Road
  - ⚓ Railroad
  - Stream
  - Private Facility
- MULTI-USE PATH/TRAIL STATUS**
- Complete Paved Trail
  - Complete Natural Surface Trail
  - Complete Sidepath
  - 100-Year Floodplain
- Fort Mill School
  - Fort Mill Park
  - Hospital
  - Downtown/Main St
  - Water
  - Fort Mill
  - Other Jurisdiction
  - York County
  - County Boundary

Areas characterized by wetlands are also suitable for trail development. Trail design near wetlands must adhere to a variety of local, state, and federal laws and regulations. Trails that are collocated with wetlands can support drainage and serve as unique walking or biking experiences for users. For example, wetlands support an abundance of flora and fauna that attract birders, naturalists, and educational groups to learn about the surrounding ecosystem. Boardwalks are elevated multi-use paths that are typically placed in these environmentally sensitive areas.



# FORT MILL TRAIL MASTER PLAN

## Wetlands

### LEGEND

- Road
  - ⊕ Railroad
  - Stream
  - Private Facility
- MULTI-USE PATH/TRAIL STATUS**
- Complete Paved Trail
  - Complete Natural Surface Trail
  - Complete Sidepath
  - Freshwater Emergent Wetland
  - Freshwater Forested/Shrub Wetland
- Fort Mill School
  - Fort Mill Park
  - Hospital
  - Downtown/Main St
  - Water
  - Fort Mill
  - Other Jurisdiction
  - York County
  - County Boundary





*Rock Hill Residents at a Local Bike Repair Shop Across from the Riverwalk*

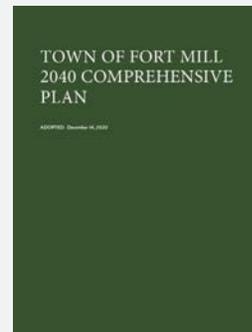
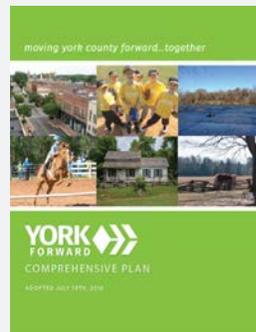
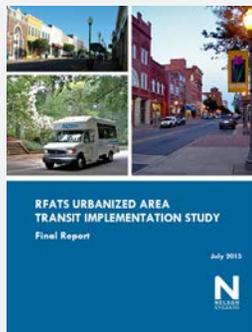
## PREVIOUS PLANNING EFFORTS

Fort Mill, York County, local agencies, and adjoining jurisdictions in the study area have prioritized active transportation in planning efforts over the past decade. The Anne Springs Close Greenway and the Carolina Thread Trail serve as major greenway and trail destinations for residents and visitors traveling to the area. These trails, along with others, are included in locally adopted plans, such as the *York County Comprehensive Plan*, the *Town of Fort Mill Comprehensive Plan*, and the *Rock Hill – Fort Mill Area Transportation Study (RFATS)* efforts.

The project team reviewed relevant planning documents such as the *Town of Fort Mill Comprehensive Plan*, *RFATS Bicycle and Pedestrian Connectivity Plan*, *York County Comprehensive Plan*, and the *Carolina Thread Trail Master Plan*. The team also reviewed relevant plans, studies, and policies related to greenways and trails at the local, regional, and state levels. The project team coordinated with Town of Fort Mill staff to differentiate between the recommendations documented in previous planning efforts that have been completed versus those that remain priorities to the Town. Recommendations that remain a priority were incorporated into the *Trail Master Plan* in Chapter 4: Recommendations.

The following table provides a summary of key active transportation, general transportation, land use, and parks and recreation recommendations and findings from previous plans and studies that are relevant to the *Fort Mill Trail Master Plan*. The following plans and associated recommendations related to the *Fort Mill Trail Master Plan* are included in the table on the following pages:

- ▶ Carolina Thread Trail Master Plan for York County Communities, 2009
- ▶ RFATS Urbanized Area Transit Implementation Study, 2015
- ▶ Fort Mill Historic District Design Guidelines, 2016
- ▶ York Forward Comprehensive Plan, 2016
- ▶ RFATS Regional Bicycle and Pedestrian Connectivity Plan, 2016
- ▶ York County Multi-Jurisdictional Hazard Mitigation Plan, 2017
- ▶ RFATS 2045 Long Range Transportation Plan, 2017
- ▶ RFATS Collector Street Plan, 2017
- ▶ Catawba Regional Comprehensive Economic Development Strategy, 2020
- ▶ Town of Fort Mill 2040 Comprehensive Plan, 2020



EXISTING PLAN / STUDY	KEY RECOMMENDATIONS RELATED TO THE FORT MILL TRAIL MASTER PLAN
Carolina Thread Trail Master Plan for York County Communities, 2009	<p>According to this plan, The Town of Fort Mill's primary trail system is the (currently closed) Nation Ford Greenway, a 30-mile loop envisioned around Fort Mill which will border Sugar Creek and the Catawba River. The Town currently operates five park and recreation facilities and contains the Anne Springs Close Greenway. A new museum is planned to be built on the banks of the Catawba River that will have connections to the (currently closed) Nation Ford Greenway and the Carolina Thread Trail.</p> <p>At a public open house for the Carolina Thread Trail, community members asked for trail connections to the interstate rapid transit/ Lynx system which would be useful in high-development areas like Fort Mill. Community members also said that the Hwy 21 bridge between Rock Hill and Fort Mill should be converted into a recreation node that will link outdoor enthusiasts to the Catawba River.</p>
RFATS Urbanized Area Transit Implementation Study, 2015	<p>RFATS seeks to expand access to transit systems to improve access between residential areas, major employers, and activity centers. Fort Mill and York County are two areas prioritized for this recommendation. This study provides an overview of the study area including population trends, major employers, and activity centers. It also details a market analysis including employment and population densities and opportunities for transit services in the RFATS area. Within the study, seven fixed-route services were designed to serve the City of Rock Hill and one route was designed to serve the SC 160 corridor between Fort Mill and Tega Cay. Several potential areas with high transit potential include Downtown Fort Mill, east Riverview, and along the I-77 Corridor near the North Carolina state line. Charlotte Area Transit System (CATS) currently serves Baxter Village in Fort Mill (Route 82X).</p> <p>In the major employer analysis, Wells Fargo Home Mortgage was found to be the largest private employer located in Fort Mill, employing just over 2,000 employees. The new Piedmont Medical Center off I-77 is estimated to bring 400 new jobs and over \$500,000 per year to the Town of Fort Mill. Identifying and prioritizing trail connections to transit service connections may help transit riders with first-mile/last-mile commutes in the study area.</p>
Fort Mill Historic District Design Guidelines, 2016	<p>The Town of Fort Mill was awarded a FY 2015 Federal Historic Preservation Grant by the South Carolina Department of Archives and History for the purpose of developing a Historic District Guidelines Manual. The goals of this manual are to establish objective, locally inspired and historically appropriate guidelines for construction, rehabilitation, and modifications to buildings within the Town's Historic District; preserve the integrity of the Town's historic resources; and ensure predictability and consistency in decision related to design, architecture, and materials within the Historic District.</p> <p>Chapter 6 of this document focuses on design guideline recommendations for pedestrian and site features to ensure that projects are scalable, and promote active, pedestrian-oriented street fronts. Design standards are provided for outdoor amenity spaces, site furnishings, awnings, canopies, lighting, service areas and building equipment, site features, and parking areas.</p>
York Forward Comprehensive Plan, 2016	<p>The <i>York Forward Comprehensive Plan</i> (2016) is the County's planning effort to update the current comprehensive plan. The plan states that due to York County's growing office market, the County should focus on developing mixed-use town centers that emphasize parks and greenways for exercise and relaxation. It also finds that there are opportunities for natural resource-based tourism. The plan proposed a county-wide interconnected greenway system and a developed blueway on the Catawba and Broad Rivers which could attract outdoor enthusiasts. To achieve this, the plan proposed to create a countywide, multijurisdictional trail and greenway plan. It also recommends that the County should adopt design incentive provisions to allow developers to donate a fee in-lieu instead-of the required open space dedication for residential developments not located along proposed greenways and dedicated open space.</p>

## EXISTING PLAN / STUDY

## KEY RECOMMENDATIONS RELATED TO THE FORT MILL TRAIL MASTER PLAN

RFATS Regional Bicycle and Pedestrian Connectivity Plan, 2016

RFATS developed the *Regional Bicycle and Pedestrian Connectivity Plan* to identify and prioritize multi-modal network enhancements within the region that provide an expanded range of connectivity and choice beyond municipal boundaries. In analyzing areas of concern for bicyclist and pedestrian safety, this plan found that collision hotspots in Fort Mill are on Harris Street/Harris Road crossing the 21 Bypass and on SC 160 crossing I-77. It also found that the highest levels of comfort for bicyclists are in Downtown Fort Mill, as well as in neighborhoods that have low-speed roads, low-volume traffic, and an existing sidewalk network in place. Outreach from this plan identified a community desire to ride their bike to work in Downtown Fort Mill from nearby neighborhoods. The community also noted that they want to bike or walk to Main Street and to schools along Springfield Parkway.

York County Multi-Jurisdictional Hazard Mitigation Plan, 2017

The *York County Multi-Jurisdiction Hazard Mitigation Plan* (2017) is required by the Federal Emergency Management Agency (FEMA) for all counties in the State of South Carolina. This plan was designed to be a logical, information-driven plan that systematically identifies and guides the implementation of mitigation actions, including policies or site-specific projects designed to make York County and its municipalities safer from the threat of natural hazards.

According to the plan, there are numerous areas within York County that are mapped by FEMA as prone to flooding (received a medium overall vulnerability to flooding). Potential strategies to adapt to the stormwater impacts associated with increased flooding include restricting the use of floodplains along rivers and creeks for open space, greenways and other purposes that can withstand periodic flooding.

RFATS 2045 Long Range Transportation Plan, 2017

The *RFATS 2045 Long Range Transportation Plan* (2017) is a multi-modal planning effort, providing recommendations for highways, public transportation, freight, bicycle, and pedestrian travel, as well as aviation. It includes a financial plan for transportation expenditures to 2045, as well as a congestion management process. According to the plan, Fort Mill's historic district is supportive of biking and walking due to its grid-like street pattern; however, it is challenged with connecting new developments to the historic district as it continues to grow. The Anne Springs Close Greenway also serves as a green belt around the Town. Several proposed bicycle and pedestrian improvements are proposed in the plan.

**EXISTING PLAN / STUDY****KEY RECOMMENDATIONS RELATED TO THE FORT MILL TRAIL MASTER PLAN**

**RFATS Collector Street Plan, 2017**

The *RFATS Collector Street Plan* (2017) states that the region has experienced significant and steady growth over the past few decades. Due to desirable qualities within the region, elevated growth rates are expected to continue for the foreseeable future. Examples of collector streets in the Town of Fort Mill are Sutton Road and Banks Road. The plan states that traffic congestion is significant along I-77, US 21, Springfield Parkway, SC 160, and Dobys Bridge Road. The plan also outlines design considerations and typical sections for collector streets which include planting strips, sidewalks, and bicycle lanes. Traffic calming is also recommended on collector streets to reduce the speed and volume of traffic to acceptable levels, reduce crashes, and to provide safe environments for pedestrians, cyclists, and children.

**Catawba Regional Comprehensive Economic Development Strategy, 2020**

*Catawba's Regional Comprehensive Economic Development Strategy* (CEDS) is a formalized planning process developed by the federal Economic Development Administration (US EDA) to promote an area-wide assessment of economic trends and infrastructure needs within multi-county districts such as the Catawba Region. The economic potential of the Catawba Region is promising which includes the counties of Chester, Lancaster, Union, and York.

The study found that total employment increased in the region due to increased population and tourism within the Rock Hill / Fort Mill area of York County and the Indian Land area of Lancaster County. The study notes that the Catawba Region has many of the necessary pieces to encourage and accommodate future economic growth so they must focus on certain factors such as preserving the natural environment and expanding cultural opportunities. The addition of greenways and trails in Fort Mill may be able to promote economic development within the Catawba Region.

## EXISTING PLAN / STUDY

Town of Fort Mill 2040  
Comprehensive Plan, 2020

## KEY RECOMMENDATIONS RELATED TO THE FORT MILL TRAIL MASTER PLAN

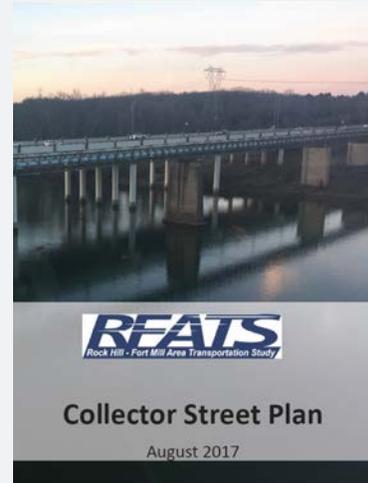
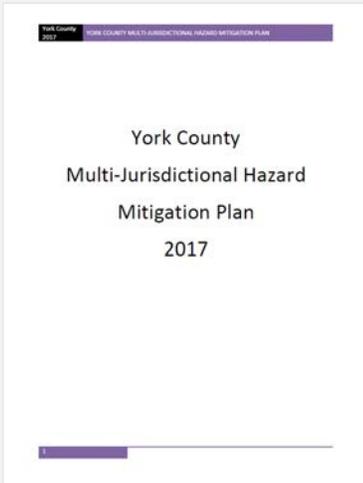
The *Town of Fort Mill's 2040 Comprehensive Plan* (2020) describes the goals and strategies for guiding future land use and development within the Town of Fort Mill. This plan includes a vision, strategies, and implementation guide to address issues and pursue opportunities related to transportation, economic development, land use, housing, natural and cultural resources, community facilities, and public health.

One of the key guiding principles in this plan is to create and support a livable built environment. The Town would like to achieve attractive and walkable neighborhoods, distinctive mixed-use centers with strong commercial corridors that are connected to each other by greenways and trails and accessed by an efficient multimodal transportation system that includes a variety of options. The plan outlines several strategies for implementation of greenways and trails which include coordinating implementation plans with regional bicycle and pedestrian partners, applying for Carolina Thread Trail Implementation Grant funding to construct greenway projects for connectivity, and developing self-guided tours of historic sites and trails for economic development and tourism.

Imagine Fort Mill, a community survey report detailed in the plan, documented the following key findings:

- ▶ With walking and running trails being the most desired type of new recreational facility, the survey also provides support to require sidewalks in more zoning districts and to perhaps consider capital expenditures to make connections in our existing sidewalk network.
- ▶ The community wants to prioritize the development of the Cross Charlotte Trail and connect through Fort Mill to Rock Hill.
- ▶ The community wants to develop a Discovery Trail which would provide more access to users with varying ages and abilities.
- ▶ The community would like for the Town to require developers to provide/build trail; at a minimum build natural surface trail.
- ▶ The community wants to become a Trail Town that accommodates bicyclists through Town on a new segment of the Carolina Thread Trail.

2045 Long Range Transportation Plan



## DEVELOPMENT MASTER PLANS + RELEVANT SITE PLANS

There are several approved developments underway in the Town of Fort Mill. Many of these developments have trails and/or greenways included in the site plans. Future developments are listed below by development type and include any relevant information related to trails.

### Planned Neighborhood Developments:

*Springfield* – Pedestrian or bicycle paths, decorative landscape features, fences, monuments, signage, trail related structures and shelters may be placed within these landscape corridors. Dedicated greenways will be owned and maintained by a Homeowner's Association. A minimum of 35% of the total development area (including the Springfield Golf Course property, along with any areas designated for greenway donation as specified in item 26) will be open space. A proposed public access for the Nations Ford Greenway will be provided, in a location to be determined by the developer, and as constructed with surface materials that will be determined at the developer's discretion. A Homeowner's Association will maintain this area. As indicated on the Planned Neighborhood Development Site Plan, a minimum of 20 parking spaces will be provided/reserved for public access to the greenway easement/buffer. The developer agrees to donate the land for a buffer along Sugar Creek as a part of the overall future greenway system linking Charlotte to Rock Hill. All dedicated greenways shall be included in allowable Open Space calculations and density calculations. In addition to donating the land, the developer agrees to work with the appropriate environmental organizations in bringing the trail in Springfield to fruition. The Sugar Creek buffer, Sugar Creek Trail, trail connections and parking area will be dedicated for public use and donated for York County's consideration of acceptance. The trail boundary will be determined at time of construction and the entire trail corridor will be conveyed fee-simple to the Nations Ford Land Trust.

Unless otherwise noted, trails and greenways are permitted in mixed-use development plans. Dedicated greenways are included in allowable open space calculations and density calculations (min. 20% of total development area should be open space).

### Mixed-Use Developments:

- ▶ *Arden Mill* (Formerly Fort Mill Reserve) – The project consists of approximately 53 acres which will have 99 single-family home lots, 2 retail parcels, and 1 office parcel.
- ▶ *Banks Street* – Approximately 13 acres located at 513, 609 and 615 Banks Street, and 490 Academy Street, were previously annexed to and made a part of the Town of Fort Mill by ordinance adopted by Fort Mill. Trails are a permitted use within residential areas and greenways are included in allowable open space calculations.
- ▶ *Carolina Orchard* – The development is located between Bartlett Street and York Southern Road. It will contain up to 731 residential units surrounded by common open space/landscaped area.
- ▶ *Riverchase* – This project is located between Dobys Ridge Road and the Catawba River. Plans contain open spaces, a trail system, a rustic open air recreation area, and natural buffers surrounding the proposed development. Open space (excluding dedicated greenways) will be owned and maintained by a Homeowner's Association. Dedicated greenways will be included in allowable open space calculations (min. 20%). The developer agrees to donate the land or a buffer along the Catawba River as part of overall future greenway system linking Charlotte to Rock Hill. Trail clearing

and mulching will be provided for a trail corridor along the southern border of the property running parallel to the Catawba River.

- ▶ *Rutledge* – This project will be located between the southwest corner of Springfield Parkway and US Highway 21 Bypass.
- ▶ *Sutton Road (Masons Bend Suttonview)* – Approximately 281 acres at 1999 Suttonview Road were annexed and made part of Fort Mill. A walking trail will be constructed along the Catawba River (5,000+ ft) using soft surface natural material. The developer may offer the trail for public dedication or prepare and record a public access easement. The developer will be responsible for constructing 2 public parking areas for the proposed walking trail and clearing and constructing the trail within project limits. The walking trail will be maintained by the Homeowner’s Association or Property Owners Association. Dedicated greenways are included in allowable open space calculations and density calculations (min. 20% of total development area should be open space).
- ▶ *Waterside on the Catawba* – York County parcels were annexed and made a part of Fort Mill, totaling almost 470 acres. Dedicated greenways are included in allowable open space calculations and density calculations (min. 20% of total development area should be open space).

#### Other Development Agreements:

- ▶ *Pecan Ridge* – This future residential development will be zoned as R-5. The agreement does not include a description of land dedicated for public uses. Trails and greenways are not included in the agreement.
- ▶ *Kanawha Land LLC (Lennar at Elizabeth)* – Seven parcels currently zoned for light industrial will be rezoned to mixed-use in Fort Mill. The concept plan shows a proposed greenway trail that creates a loop within the southern parcels. The developer shall donate the greenway trail property for use, improvement, operation, and maintenance as a public trail system. They will also be responsible for developing and installing a portion of the proposed trail. The trail will be at least ten (10) feet in and sections that are located within or adjacent to a public right-of-way shall be paved. Any trails, or portions of trails, which are proposed to be dedicated to the Town for ownership and maintenance and which are not located within or adjacent to a public right-of-way, shall be located within a public trail right-of-way that is at least 30 feet wide. The developer shall install any pedestrian bridges or walkways which may be necessary to provide safe crossing over waterways, steep or unusual topography, or other obstacles along the trail. The developer shall also install at least one pedestrian underpass under Fort Mill Parkway.
- ▶ *Nims Village* – There is a development agreement for York County property (44.6 acres) located on Fort Mill Parkway. Trails are permitted as a private neighborhood amenity. Greenways and trails may be included in density calculations.

## POLICY REVIEW

Existing local and state policies were reviewed to better understand how they influence trail design and development for active transportation facilities in the Town of Fort Mill. The following table provides a summary of key policies from SCDOT, York County, and the Town of Fort Mill that may guide or influence the development of the trail network within the Town.

The following policies, ordinances, and guidelines were reviewed as part of this exercise:

- ▶ Town of Fort Mill Code of Ordinances
- ▶ Town of Fort Mill Zoning Ordinance
- ▶ York County Code of Ordinances
- ▶ SCDOT Complete Streets Policy
- ▶ SCDOT Roadway Design Manual, 2021

EXISTING POLICY	KEY FINDINGS RELATED TO THE FORT MILL TRAIL MASTER PLAN
<p><b>TOWN OF FORT MILL ZONING ORDINANCE</b></p>	<p>The Fort Mill Zoning Ordinance includes local regulations that control the use and development of land in the Town of Fort Mill. There are several references to trails and greenways within the zoning ordinance. For example, it defines pedestrian pathways as a sidewalk, trail or other facility designated for use by pedestrians. Constructed alongside streets, roads, parking areas or through public or private spaces, a pedestrian pathway has a paved surface and is designed to meet ADA requirements. Such facilities may also accommodate bicycles, roller and inline skates, skateboards, and other non-motorized vehicles provided, however, that the restriction on motorized vehicles shall not apply to motorized wheelchairs. Where permitted by the Town Council or this ordinance, battery powered golf carts and self-balancing personal transport vehicles may also be allowed on pedestrian pathways. Typically separate from motor vehicle traffic, the facility may serve both transportation and recreation purposes. Recreational trails are also defined as a way designed for and used by equestrians, pedestrians, and cyclists using nonmotorized bicycles.</p> <p>The zoning ordinance also specifies that greenways (or bike paths) may qualify for a density bonus of up to five percent above the allowable base density. To qualify, the bike paths or greenways shall form an integrated system of access within the development and to principal off-site destinations. In addition, resource conservation districts may be applied in areas to retain open spaces and greenways to protect their environmentally sensitive character.</p>

## EXISTING POLICY

## KEY FINDINGS RELATED TO THE FORT MILL TRAIL MASTER PLAN

TOWN OF FORT MILL  
CODE OF ORDINANCES

The Code of Ordinances for the Town of Fort Mill was originally adopted in April of 1996. The following sections focus on trails and/or greenways:

Sec. 6. Planned Neighborhood Development District: A system of bike paths/pedestrian greenways may qualify for a density bonus of up to five percent above the allowable base density. To qualify, the bike paths or greenways shall form an integrated system of access within the development and to principal off-site destinations.

Sec. 13. Resource Conservation District: The resource conservation district is intended to be applied to areas along watercourses and important open spaces within the Town's planning jurisdiction to retain open spaces and greenways to protect their environmentally sensitive character. Picnic grounds, parks, hiking or horseback riding trails, open space, and other similar public and private recreational uses that do not require extensive use of fences or walls are permitted uses within a resource conservation district.

Sec. 14. Transitional Commercial District: Parks and greenways are permitted uses within this district.

Sec. 17. Limited Industrial District: Parks and greenways are permitted uses within this district.

Sec. 19. Mixed Use Development District: The following may be counted towards required dedicated open space: conservation lands, natural areas, formal greens, plazas and courtyards, trails, buffers held in common ownership, and parks and recreation areas, including ball fields, golf courses (excluding vertical structures: clubhouse and maintenance facilities), tennis and basketball courts, playgrounds, and other areas used for active or passive recreation. Trails may be incorporated into required buffer areas provided adequate width (min. 15 ft) is added to the required buffer width to accommodate both the trail and the required buffer plantings. Buffers with trails may also count toward the provision of open space for the development.

Sec. 22. Urban Development District: Trails may be incorporated into required buffer areas provided adequate width (min. 15 ft) is added to the required buffer width to accommodate both the trail and the required buffer plantings. Buffers with trails may also count toward the provision of open space for the development.

Div. 3. Flood Hazard Reduction: Permissible uses within floodways may include the following: general farming, pasture, outdoor plant nurseries, horticulture, forestry, wildlife sanctuary, game farm and other similar agricultural, wildlife and related uses. Also, lawns, gardens, play areas, picnic grounds and hiking and horseback riding trails are acceptable uses, if they do not employ structures or fill. Substantial development of a permissible use may require a no-rise certification. The uses listed in this subsection are permissible only if and to the extent that they do not cause any increase in base flood elevations.

## EXISTING POLICY

## KEY FINDINGS RELATED TO THE FORT MILL TRAIL MASTER PLAN

## YORK COUNTY CODE OF ORDINANCES

Trails and greenways are referenced several times throughout the York County Code of Ordinances:

- ▶ § 32.61 PLAN FOR PROTECTION; LIST OF SIGNIFICANT RESOURCES: The plan for the protection or preservation of significant resources in the county may include plans and recommendations for the protection of recreation land for hunting, fishing, water access, and trails.
- ▶ § 70.10 REGULATION OF OFF-HIGHWAY VEHICLES: Except as otherwise provided in this code, it is unlawful to operate an off-highway vehicle upon public rights-of-way, utility rights-of-way or easements, public highways, streets, sidewalks, trails, parks and roads or upon private property without the written consent of the owner.
- ▶ § 151.36 SPECIFIC STANDARDS: Permissible uses within floodways or non-encroachment areas may include: general farming, pasture, outdoor plant nurseries, horticulture, forestry, wildlife sanctuary, game farm, and other similar agricultural, wildlife, and related uses. Also, lawns, gardens, play areas, picnic grounds, and hiking and horseback riding trails are acceptable uses, provided that they do not employ structures or fill.
- ▶ § 154 LAND DEVELOPMENT CODE: Townhouse subdivisions - Required open space areas must have a minimum width and depth of at least 40 feet, excluding right-of-way. Areas that are at least 20 feet wide and that have hard surface trails at least five feet wide connecting at least two points in a sidewalk system may also count towards required open space area; Where a trail network runs through or adjacent to a townhome subdivision, the pedestrian walkways must connect to it; Cluster subdivisions shall include an interconnected pedestrian network comprised of sidewalks and multi-use trails; Multi-use trails may be located on one or both sides of the road and shall: (a) Be located in open space areas between cluster groups, (b) Connect to the sidewalks located in cluster groups, (c) Have a minimum width of ten feet, and (d) Be covered by a pedestrian easement; Sidewalks and multi-use trails shall provide a continuous pedestrian network throughout a cluster subdivision.
- ▶ § 154.122 TRAILS AND MULTI-USE PATHS. Trails and multi-use paths shall be designed and constructed to the width and with the surface types established in the York County Land Development Manual for the designated type of facility. All trails and multi-use paths shall be constructed within an easement that dedicates the facility to public use in perpetuity. This shall not apply to any trail constructed within an area under a conservation easement that generally provides for public access. Prior to final plat approval, the developer shall establish a means of providing for the maintenance of all trails and appurtenant facilities. This may be through the owner, a Property Owners Association, or a third-party that agrees to assume maintenance responsibility and liability for the trail. If a third party (other than a Property Owners Association) is designated to maintain the trail(s), the legal instrument establishing such acceptance of responsibility shall be provided to the County prior to final plat approval.

**EXISTING POLICY****KEY FINDINGS RELATED TO THE FORT MILL TRAIL MASTER PLAN****SCDOT COMPLETE STREETS POLICY**

The SCDOT Complete Streets Policy was adopted in February 2021. This policy requires the consideration of accommodations for bicycling, walking, and transit in the design, construction, maintenance, and operations of the state transportation network.

Key components of the Policy include:

- ▶ Funding for these accommodations is to be included in the budget for each project if warranted on the individual project and in accordance with the regional plans.
- ▶ SCDOT will update and modernize its design manuals to include multimodal accommodations.
- ▶ SCDOT will establish a council to facilitate ongoing communication to seek continuous improvement opportunities and initiatives.

**SCDOT ROADWAY DESIGN MANUAL, 2021**

- ▶ The SCDOT Roadway Design Manual was created in 2021. This document outlines engineering design requirements for design of roadway facilities within the SCDOT ROW and relies heavily on established engineering standards, such as the American Association of State Highway and Transportation Officials (AASHTO) Green Book, guidance documents from the Federal Highway Administration (FHWA), and other engineering design references.
- ▶ Chapter 13, Multimodal Transportation, provides a detailed overview of the design requirements for bicycle and pedestrian facilities, as well as transit stops, and bus turnouts built along SCDOT roadways. Guidance includes topics such as minimum width, minimum vertical clearance, minimum railing height, roadway crossings, grade separation, and curb ramps. Together, these factors will influence future design and construction of any portion of the trail within SCDOT right-of-way.
- ▶ Additionally, SCDOT has a clear zone requirement for its roadways, as defined in the AASHTO Roadside Design Guide. The clear zone is defined as “The unobstructed traversable area provided beyond the edge of the traveled way is termed the clear zone. This area is used for the recovery of errant vehicles and includes shoulders, bike lanes, and auxiliary lanes.” The width of the clear zone requirement varies based on the traffic volume, design speed, and cross slope of shoulder for the roadway in question. This clear zone can have implications for trail development as it may increase the amount of needed ROW.

## Existing Pedestrian Education + Encouragement Programs

There are several existing programs within the Town of Fort Mill that promote, educate, and/or encourage residents and visitors to walk, run, hike, or bike within the Town. Several groups have implemented walking/running programs and activities such as the Walk with a Doc Program, local businesses like Fleet Feet, the Anne Springs Close Greenway, and the Fort Mill History Museum. The following programs exist within the Town of Fort Mill:

- ▶ Anne Springs Close Greenway – The local greenway holds programmed hiking tours on site along with the Love the Greenway Month event.
- ▶ Walk with a Doc – Walks are currently on hold, but they normally take place at New South Family Medicine, led by Dr. Mendelsohn.
- ▶ Fort Mill History Museum Walking Tours – Fort Mill History Museum offers guided tours of the historic district (Main Street between White Street and Clebourne Street) with museum docents.
- ▶ Fleet Feet – Fleet Feet, a local running store in Fort Mill, offers various running and walking events to the community throughout the year.

Please visit **Chapter 5: Policy + Program Recommendations** to learn more about the policy and program recommendations made in this plan.





*Love the Greenway Month Program at Anne Springs Close Greenway, Fort Mill*

## OPPORTUNITIES + CONSTRAINTS ANALYSIS

Site visits were performed in the study area to gain a better understanding of the surroundings in Fort Mill. The project team conducted site visits to key destinations (i.e., existing parks, shopping centers, essential services, and residential neighborhoods), existing bicycle and pedestrian facilities, roadways, bridges, environmentally-sensitive areas, and constrained areas. The observations recorded during site visits informed the opportunities and constraints analysis. This analysis also considers connectivity to existing and/or planned active transportation facilities, connectivity to destinations, critical crossings, barriers to accessibility, and gaps between the proposed trail corridors and surrounding municipalities.

The following table provides a summary of the opportunities and constraints of existing facilities that impact the development of Fort Mill's trail network. Numbered sections in this table correspond with specific opportunities and constraints identified on the map following the table.

OPPORTUNITIES + CONSTRAINTS	DESCRIPTION
1. Neighborhood Connectivity	<p>The lack of pedestrian connectivity between neighborhoods limits Fort Mill residents' ability to walk or bicycle from their homes to local parks, schools, shops, jobs, and adjacent neighborhoods. This is an issue for most neighborhoods, given the geographic spread of the Town.</p> <p>One example of a neighborhood with limited connectivity is Carolina Orchards. This neighborhood has an existing sidepath (private) that runs adjacent to SC Hwy 460 and ends right before it meets the railroad track. The extension of this sidepath along SC Hwy 460 would help connect Carolina Orchards to both Nations Ford High School and Springfield Country Club.</p>
2. Multimodal Connectivity	<p>Fort Mill's active transportation network contains several gaps that, if completed, would help connect nearby residents and businesses. These gaps serve as major barriers to both pedestrian and bicycle access within and around the Town.</p> <p>One opportunity to enhance multi-modal connectivity is a new connection to Baxter Village where the only transit stop exists. The public survey indicated that residents use the Carolina Thread Trail at Baxter Village to access this transit stop. Attendees at the Public Workshop also voiced support for a future trail connection between the existing Masons Bend Trail (private) and the Carolina Thread Trail to Baxter Village. This connection would also support future access to the transit stop.</p>

OPPORTUNITIES + CONSTRAINTS	DESCRIPTION
<p><b>3. Connections to Schools</b></p>	<p>One school that is well-connected to nearby neighborhoods is Kings Town Elementary School. An initial site visit to the Town revealed that the bike rack at the school was overwhelmed with the number of bicycles and e-scooters from students who live in Masons Bend.</p> <p>Despite this example, several schools have sidepaths or sidewalks that lack connections to surrounding neighborhoods. Providing these bicycle and pedestrian connections may encourage students to bike or walk to school. It would also reduce dependence on carpooling to school, which contributes to increased traffic congestion in school zones.</p> <p>Both River Trail Elementary School and Catawba Ridge High School have existing sidewalks that run parallel to Fort Mill Parkway, but they do not connect to each other.</p>
<p><b>4. Connections to Parks and the Catawba River</b></p>	<p>Fort Mill is home to numerous parks that support an expansive list of outdoor recreation opportunities. While some park properties have sidewalks that lead to their entrances, others do not and lack connections between them.</p> <p>There is an existing multi-use path that partially connects Walter Y. Elisha Park to Harris Street Park that could benefit from signage, as well as visible crossings for users.</p> <p>The Catawba River also supports paddle trails and provides several kayak launches that are accessible from the Fort Mill side. Residents along the waterline would benefit from future trail connections between the neighborhoods and kayak launches.</p>
<p><b>5. Connections to Downtown Fort Mill</b></p>	<p>Existing sidewalks in Downtown Fort Mill contains several gaps, and many require maintenance. For example, existing sidewalks in the historic Paradise neighborhood are narrow and in need of maintenance. The community would benefit from a trail segment that connects Steel Street Park in the neighborhood to Downtown Fort Mill. Residents that live south of Downtown Fort Mill also lack safe facilities to access Main Street, Banks Road, and Fairway Drive.</p>
<p><b>6. Connections to Shopping Centers</b></p>	<p>Kingsley Town Center (next to I-77) and the Walmart (Downtown Fort Mill) are two popular shopping locations that lack safe pedestrian connections and crossings from adjacent roadways and neighborhoods. The intersection of Tom Hall Street and N Dobys Bridge Road needs high-visibility crosswalks to provide a safe connection for nearby residents to the Walmart shopping center. The Kingsley Town Center has internal sidewalks that could benefit from future trail connections to other nearby destinations like the Fort Mill Gateway Trail, the Timberline Trailhead, or the Anne Springs Close Greenway entrance to Lake Haigler.</p>

OPPORTUNITIES + CONSTRAINTS	DESCRIPTION
7. Connections to Anne Springs Close Greenway Trails and Facilities	Anne Springs Close Greenway is considered by many in Fort Mill as the main greenway system within the Town. This greenway and park facility provides several other activities and destinations such as the Gateway Canteen, hiking, biking, and equestrian trails, a greenway adventure center, and the Dairy Barn. Some of the trails cross roadways (i.e., Old Nation Road and SC Hwy 460) and could benefit from improved crosswalks. The Greenway Adventure Center could also benefit from connections to Springfield Country Club across SC Hwy 460.
8. Connections to Piedmont Medical Center	The Piedmont Medical Center is located north of N White Street between Kingsley Town Center and US Hwy 21 and serves as a major generator for jobs in the area. It is also located close to the historic peach stand, as well as the Anne Springs Close Greenway stables and trails on Horse Road. There is an existing sidepath that connects the hospital to Kingsley Town Center and an existing sidewalk (Fort Mill Gateway Trail) across the road. Despite these facilities, there are no high-visibility crosswalks within the vicinity. The addition of crosswalks in this area could support safer connections between the hospital, Kingsley Town Center, The Peach Stand/Exxon, and the historic peach stand.
9. Improve crosswalks in Downtown Fort Mill	Downtown Fort Mill contains several crosswalks constructed from red brick that lack enough visibility for motorists driving on Main Street. The installation of high-visibility crosswalks could improve safety measures for pedestrians walking in the area.
10. Improve railroad crossing in Downtown Fort Mill.	The Fort Mill Farmer’s Market requires visitors to use an existing sidewalk and cross over the railroad to reach the Downtown area. To access businesses on the opposite side of Main Street, visitors must follow the same path and use the crosswalk over Main Street that is directly past Confederate Street. Additional facilities and crossings on the south side of Main Street would support travel to and from these businesses. Railroad Avenue at Main Street would also benefit from a high-visibility crosswalk.
11. Widening existing sidewalks to multi-use paths (where feasible)	Several of the existing sidewalks within the Town could be widened into sidepaths, where feasible. The Paradise community on Steele Street may be a good candidate for these improvements where the sidewalks are in need of repair. A second location for sidewalk enhancements is N White Street (SC 160) in front of the Farmer’s Market. Other locations for similar improvements may include existing sidewalk that tie into school properties (e.g., Riverview Elementary School where the sidewalk connects the school to Plum Branch Lane and at Nation Ford High School along A O Jones Boulevard).

OPPORTUNITIES + CONSTRAINTS	DESCRIPTION
<p>12. Increase trail signage to promote use of existing trails in the Town</p>	<p>According to the community survey and public workshop, several community members are unaware that other trails and multi-use paths exist outside of the Anne Springs Close Greenway. Additional trail signage and wayfinding may help improve the visibility of trails within the community and direct users to their desired trail destinations. Although it is currently closed, the Nation Ford Greenway is a prime example of an existing greenway in the Town that lacks signage. The trail can be found on Google Maps, but it is not well marked on A O Jones Boulevard and could easily be confused for an entrance to service utilities. A crossing over A O Jones between Nation Ford High School and the greenway would also encourage use of the Nation Ford Greenway once (and if) it is repaired for use again in the future.</p>
<p>13. Improve connectivity over major roadways</p>	<p>Both I-77 and US Hwy 21 are barriers to pedestrian and bicyclist mobility. The installation of new crossings and trail infrastructure would improve the safety and comfort of residents and visitors crossing I-77 and US-Hwy 21 to reach destinations via walking and biking.</p>
<p>14. Extend existing sidewalks that dead end along roadways</p>	<p>There is an existing sidewalk on the west side of N Dobys Bridge Road that runs between Doby Bridge Park and ends between Williams Road and Whitegrove Drive. Widening (where feasible) and extension of this facility north towards Tom Hall Street would help link several residential areas to the park and Downtown Fort Mill. Crosswalks over N Dobys Bridge Road would also improve connectivity to several schools that are located on the east side of the road.</p>
<p>15. Improve connectivity to Banks Athletic Park</p>	<p>Although there are several sidewalks and trails within the Waterside at the Catawba community, there are no safe trails or walkways to access Banks Athletic Park from Banks Road or Fairway Drive. Improved connectivity along these roads would encourage residents living north of the Waterside to walk or bike to the new park.</p>

Photos exhibiting the opportunities and constraints are annotated below. The following map shows the Opportunities and Constraints Analysis.



**Existing Trails Lack Signage - Trail Connector to Walter Y. Elisha Park**



**The Railroad Serves as a Barrier to Improving Cross-Town Connectivity - Downtown Fort Mill**



**Existing Sidewalks in Need of Maintenance - Steele St**



**Several Schools Lack Sidewalks - Fort Mill High School**

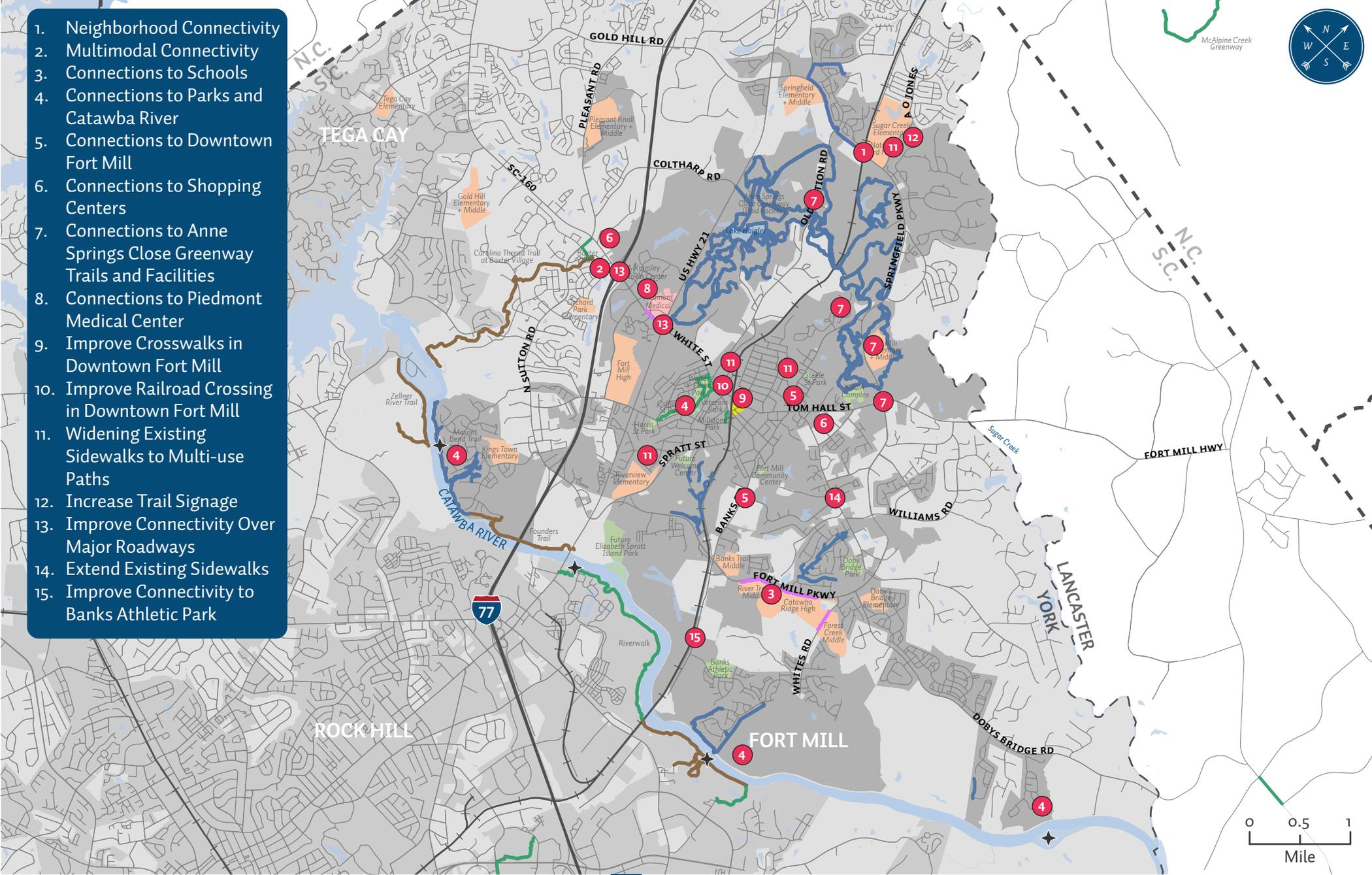


**Existing Pedestrian Crosswalks Lack High-Visibility Markings - Harris St**



**Existing Crossings Could Benefit from the Installation of Rectangular Rapid Flashing Beacons - Main St**

1. Neighborhood Connectivity
2. Multimodal Connectivity
3. Connections to Schools
4. Connections to Parks and Catawba River
5. Connections to Downtown Fort Mill
6. Connections to Shopping Centers
7. Connections to Anne Springs Close Greenway Trails and Facilities
8. Connections to Piedmont Medical Center
9. Improve Crosswalks in Downtown Fort Mill
10. Improve Railroad Crossing in Downtown Fort Mill
11. Widening Existing Sidewalks to Multi-use Paths
12. Increase Trail Signage
13. Improve Connectivity Over Major Roadways
14. Extend Existing Sidewalks
15. Improve Connectivity to Banks Athletic Park

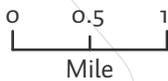


# FORT MILL TRAIL MASTER PLAN

## Opportunities + Constraints

### LEGEND

- ⊕ Opportunities + Constraints
  - ◆ Blueway Access
  - Road
  - +— Railroad
  - Private Facility
- MULTI-USE PATH/TRAIL STATUS**
- Complete Paved Trail
  - Complete Natural Surface Trail
  - Complete Sidepath
- Fort Mill School
  - Fort Mill Park
  - Hospital
  - Downtown/Main St
  - Water
  - Fort Mill
  - Other Jurisdiction
  - York County
  - County Boundary



# WHAT DOES SUCCESS FOR THE FORT MILL TRAIL

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Public  
Excitement

Healthy  
Participation!

- TRAIL
- connects to other TRAILS
  - variety of end points or stopping points
  - include as many points of interest as possible

Economic Opportunities

Loving  
outdoors

WALKABLE AND SAFE



VISIONING EXERCISE  
FORT MILL TOWN HALL

Fort Mill is

DOES IT LOOK LIKE  
THEIR PLAN?

Funding

PARTNERS  
- MPO  
- SCH

Fun  
Community

# 03

## COMMUNITY ENGAGEMENT

### OVERVIEW

Community engagement is an essential part of any planning process. The most effective plans are firmly rooted in the realities and visions of the communities that created them. This plan relies on a combination of input from community members, Steering Committee members, supporting agencies, and non-profit organizations to inform the trail network for the Town of Fort Mill. Several engagement events took place throughout the study process:

- ▶ 3 Steering Committee Meetings
- ▶ 1 Parks + Recreation Subcommittee Meeting
- ▶ 1 Community Survey
- ▶ 1 Public Workshop

### ENGAGEMENT TIMELINE



## STEERING COMMITTEE

A Steering Committee supported the recommendations made in this plan and was composed of representatives from the Town of Fort Mill, Anne Springs Close Greenway, Carolina Thread Trail, Fort Mill Economic Partners, and the Fort Mill School District. Steering Committee members met three times throughout the duration of the project and provided guidance for the plan by reviewing and sharing feedback on relevant data, community engagement efforts, network recommendations, and implementation strategies. Members also supported the plan by disseminating information and communication materials to the public.



*Steering Committee Members Providing Support at the Public Workshop*

### STEERING COMMITTEE MEMBERS

Davy Broom, Town Manager, Town of Fort Mill

Chris Pettit, Assistant Town Manager, Town of Fort Mill

Tim Kramer, Planner, Town of Fort Mill

Penelope G. Karagounis, Planning Director, Town of Fort Mill

Guynn Savage, Mayor, Town of Fort Mill

Jan Martin, Member of Fort Mill Economic Partners

John Gordon, Anne Springs Close Greenway

Bret Baronak, Carolina Thread Trail

James Traynor, Planning Commission Member

Joe Romenick, Fort Mill School District

SEPT  
27

### Steering Committee Meeting #1

The first Steering Committee Meeting was held in-person at the Town Hall building in Fort Mill on September 27, 2022. During this meeting, the project team reviewed the goals and objectives of the plan, worked with attendees to identify critical issues associated with the creation of a municipal-wide trails plan, and provided an initial overview of the various factors within the Town, including its demographic profile, environmental issues, and previous planning efforts in the region.

JAN  
17

### Steering Committee Meeting #2

The second Steering Committee Meeting was held in-person at the Town Hall building in Fort Mill on January 17, 2023. During this meeting, the project team reviewed the public survey results, the recommended trail network, recommended policies and programs, and introduced the prioritization process. The project team also worked with attendees to refine the draft trail network.

JUN  
05

### Steering Committee Meeting #3

The third Steering Committee Meeting was a hybrid meeting held on June 5, 2023. The purpose of this meeting was to provide a tour of the draft final *Trail Master Plan* and for Steering Committee members to provide input on the Plan before the project team presents it to Town Council for adoption.

JAN  
24

### Parks + Recreation Subcommittee Meeting

The Parks + Recreation Subcommittee Meeting was held in-person at the Town Hall building in Fort Mill on January 24, 2023. The purpose of this meeting was to update the Subcommittee on the plan progress and review the draft trail network, as well as program and policy recommendations.

#### KEY FINDINGS

- ▶ Attendees asked for trail connections to the historic Paradise community on Steele Street.
- ▶ The Steering Committee would like to understand the trail surface preferences within the community.
- ▶ Attendees envision a trail network that connects to all major destinations within the Town and gets the community excited to exercise outside.

#### KEY FINDINGS

- ▶ The trail connection between Walter Y. Elisha Park and Harris Street Park was identified as a top priority.
- ▶ Parking on Main St will likely be revamped in the future which could potentially support trail expansion in the Downtown area.
- ▶ The Town is currently working on a wayfinding plan.

#### KEY FINDINGS

- ▶ According to the Complete Streets Policy, SCDOT will only pay for concrete paved trails (not asphalt).
- ▶ Steering Committee members asked to revise typical section widths from 10 ft to 10-12 ft to allow for variation in the facility width where space permits.

#### KEY FINDINGS

- ▶ The Parks + Recreation Subcommittee fully supported the project and the plan's progress.
- ▶ Attendees discussed the difference between using trails for transportation and recreational purposes.
- ▶ Businesses in Downtown Fort Mill are supportive of future connections to Downtown Fort Mill.

## COMMUNITY SURVEY

The project team launched a community survey on November 1, 2022, and it was open for public comment through December 2, 2022, via PublicInput.com. The survey was linked on the Town's website, and members of the Steering Committee also disseminated survey information to the community. The survey attracted 1,244 participants who provided 2,641 comments related to the project. The feedback obtained through this survey supported the framework for developing the proposed trail network in the Town of Fort Mill.

The project team distributed the survey to help accomplish the following:

- ▶ Introduce the project and gauge public support.
- ▶ Solicit and compile public comment on destinations, opportunities and challenges, user preferences, and route preferences.
- ▶ Fulfill requests for information.
- ▶ Develop an email contact list for interested parties.

The survey contained the following three sections: project-specific questions, interactive mapping questions, and [optional] demographic questions. Public feedback is summarized on the following pages by theme or specific comment. Overall, the comments collected were generally positive and include several different perspectives on the project.

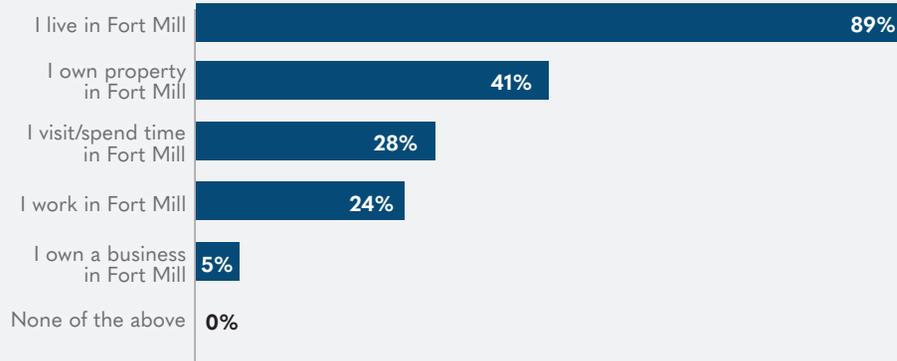
Key takeaways from the survey include the following:



### Survey Question Summary

- ▶ Most survey respondents live in the Town of Fort Mill (89%).
- ▶ Most respondents currently use trails/multi-use paths for health and exercise or recreation (90%).
- ▶ Close to 40% use trails/multi-use paths a few times a week and a little over 30% use them a few times a month. Approximately 10% never use the facilities.
- ▶ Approximately half of respondents use trails/multi-use paths the same amount as they did before the covid-19 pandemic.
- ▶ Over 60% of respondents (65%) commute to work or school by car, 4% walk, and 4% bike.
- ▶ Over 40% of respondents marked that they would like to commute in the future by bike (45%), foot (40%), or by car (43%).
- ▶ A lack of safe biking and walking connections to and from existing trails/multi-use paths ranked (63%) as the main factor that discourages respondents from using trails/multi-use paths in Fort Mill.
- ▶ Constructing new trails/multi-use paths would encourage respondents to use trails/multi-use paths more frequently in the Town (92%).
- ▶ Almost 90 percent (88%) of respondents marked that health, exercise, and recreation are the most important factors of the Town's trail network.
- ▶ Restrooms (70%) and wayfinding signs (69%) are the two most important amenities to respondents.
- ▶ Natural surface trails and paved multi-use paths are preferred by respondents at 67% and 46%, respectively.
- ▶ Roughly 65% of respondents said they would support the trail network through bonds.
- ▶ More than half (59%) of the survey participants heard about the survey through social media platforms.

### RELATIONSHIP TO THE TOWN



### Q1. WHAT IS YOUR RELATIONSHIP TO FORT MILL? (Select all that apply)

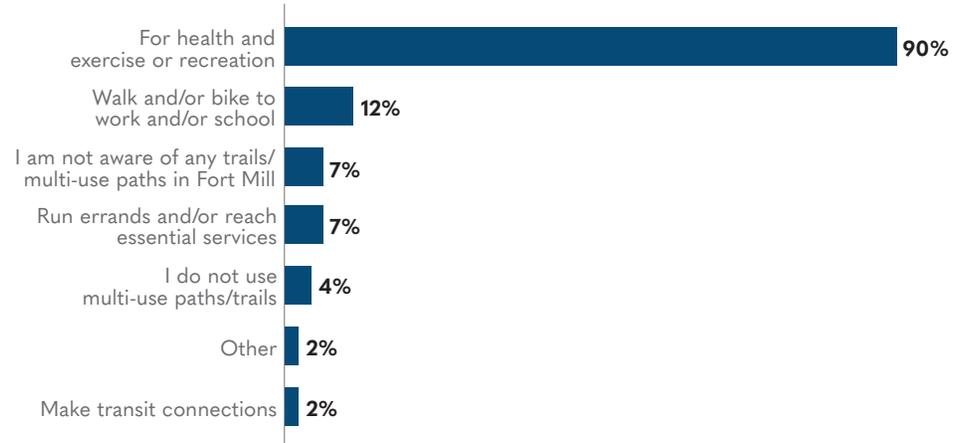
Participants were asked what their relationship is to the Town. Almost 90 percent (89%) of respondents live in Fort Mill and 41% own a property within the Town. Only 5% of respondents own a business in the Town.

### Q2. HOW DO YOU CURRENTLY USE TRAILS OR MULTI-USE PATHS IN FORT MILL?

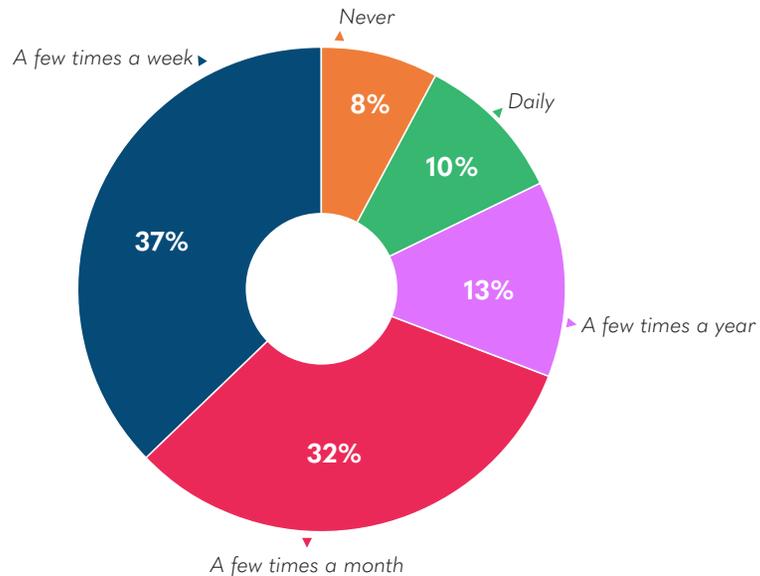
(Select all that apply)

The majority of respondents (90%) responded that they currently use trails or multi-use paths for health and exercise or recreation. Twelve percent (12%) use the existing facilities to walk and/or bike to work and/or school. Only 2% of survey participants use the facilities to make transit connections.

### CURRENT TRAIL/MULTI-USE PATH USE



### TRAIL USE FREQUENCY



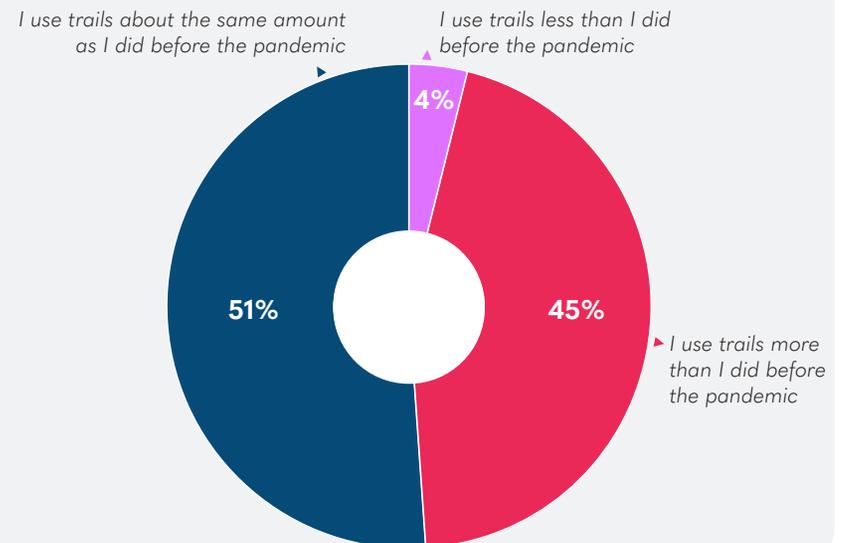
### Q3. HOW FREQUENTLY DO YOU USE TRAILS OR MULTI-USE PATHS IN FORT MILL?

Most survey participants use trails/multi-use paths a few times a week (37%), followed closely by those who use the facilities a few times a month (32%). Almost 10 percent (8%) responded that they have never used the existing trails/multi-use paths within the Town.

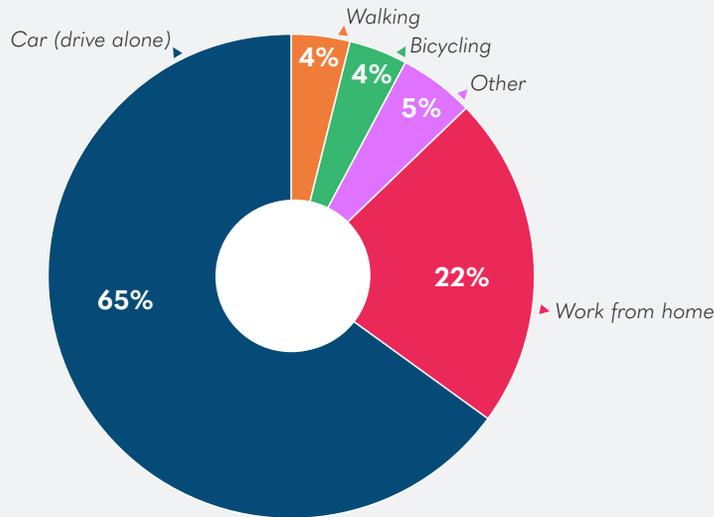
### Q4. HOW HAVE YOUR TRAIL HABITS CHANGED DURING THE COVID-19 PANDEMIC?

A little over half (51%) of respondents said that they use trails about the same amount as they did before the Covid-19 pandemic. Forty-five percent (45%) use trails more than they did before the pandemic and very few (4%) use trails less than they did before the Covid-19 pandemic.

### TRAIL USE FREQUENCY



EXISTING MODE OF COMMUTE



Q5. WHAT IS YOUR PRIMARY MODE OF TRANSPORTATION FOR COMMUTING TO WORK AND/OR SCHOOL?

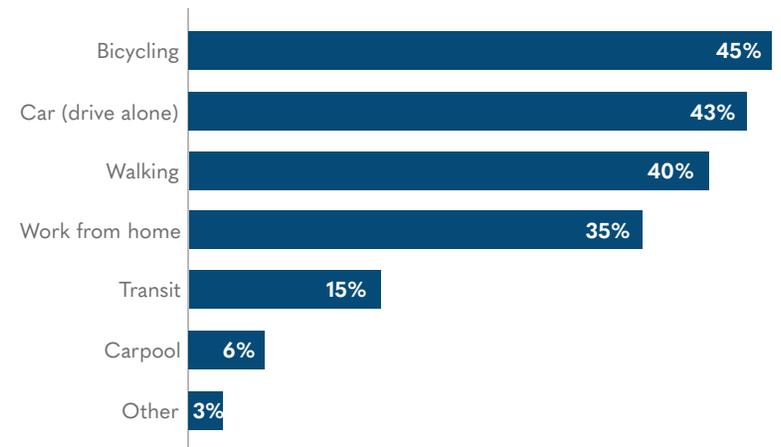
Most respondents (65%) currently commute to work and/or school by car. Twenty-two percent (22%) of respondents work from home so they do not commute to work. Less than 5 percent of respondents walk (4%) or bike (4%) to work and/or school.

Q6. NOW CONSIDER YOUR DESIRED COMMUTE IN THE FUTURE. WHICH MODES WOULD YOU LIKE TO CHOOSE?

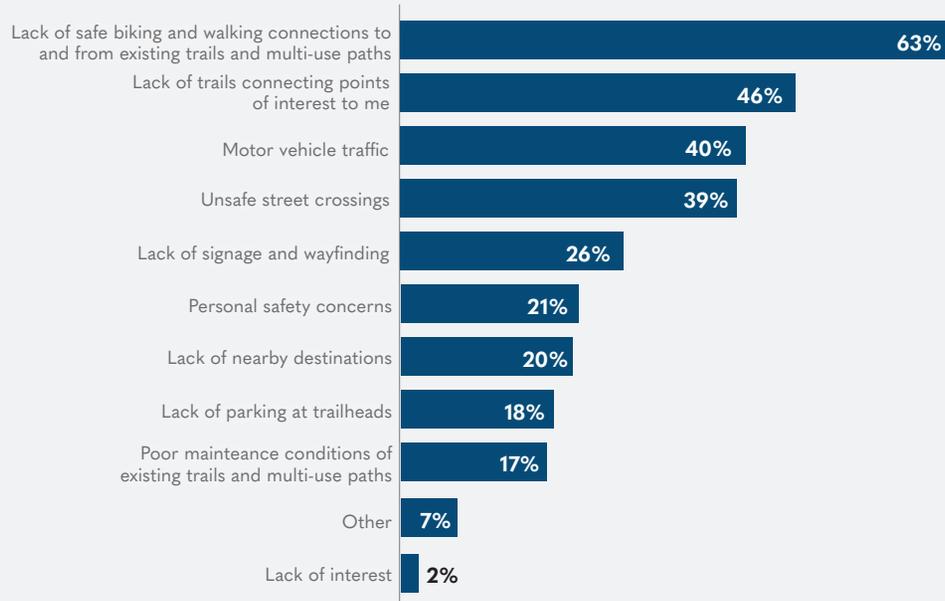
(Select all that apply)

Survey respondents would prefer to commute via bike (45%), car (43%), and by foot (40%) in the future. Very few respondents were interested in taking transit or carpooling in the future.

FUTURE MODE OF COMMUTE



## DISCOURAGING FACTORS



### Q7. WHAT FACTORS DISCOURAGE YOU FROM USING TRAILS OR MULTI-USE PATHS IN FORT MILL? (Select all that apply)

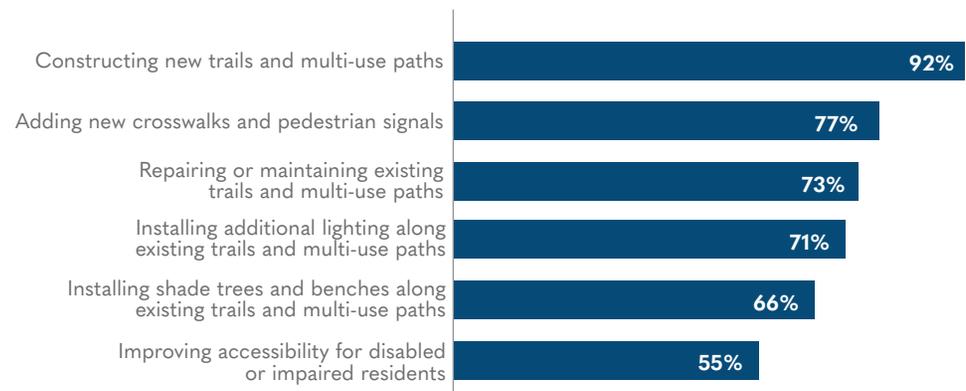
The survey found that 63% of respondents are discouraged from using trail/multi-use paths in Fort Mill due to a lack of safe biking and walking connections to and from existing facilities. A lack of connecting points to respondents (46%), motor vehicle traffic (40%), and unsafe street crossings (39%) were also listed as top discouraging factors. The survey indicated that only 2% of respondents lack interest in using trails/multi-use paths within the Town.

### Q8. RANK WHICH INFRASTRUCTURE OR PROGRAM IMPROVEMENTS WOULD ENCOURAGE YOU TO USE TRAILS AND MULTI-USE PATHS MORE FREQUENTLY IN FORT MILL?

(Rank your top 5 from 1, most important, to 5, least important)

Based on the survey, the Town should consider constructing new trails/multi-use paths for residents which ranked as the top infrastructure and program improvement at 92%.

## PREFERRED INFRASTRUCTURE + PROGRAM IMPROVEMENTS

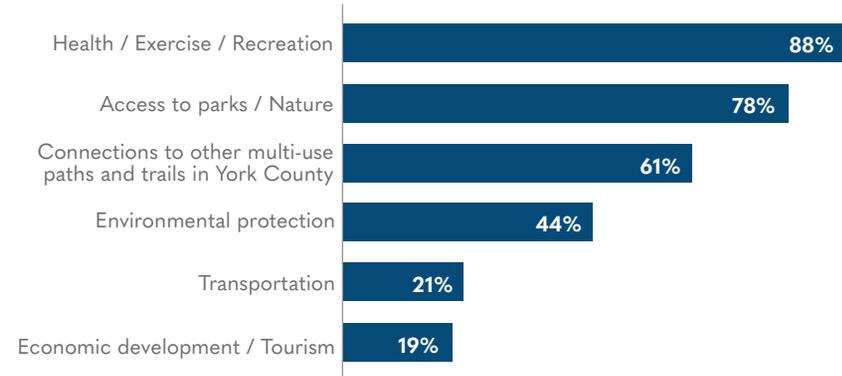


**Q9. WHICH FACTORS OF FORT MILL'S TRAIL NETWORK ARE MOST IMPORTANT TO YOU?**

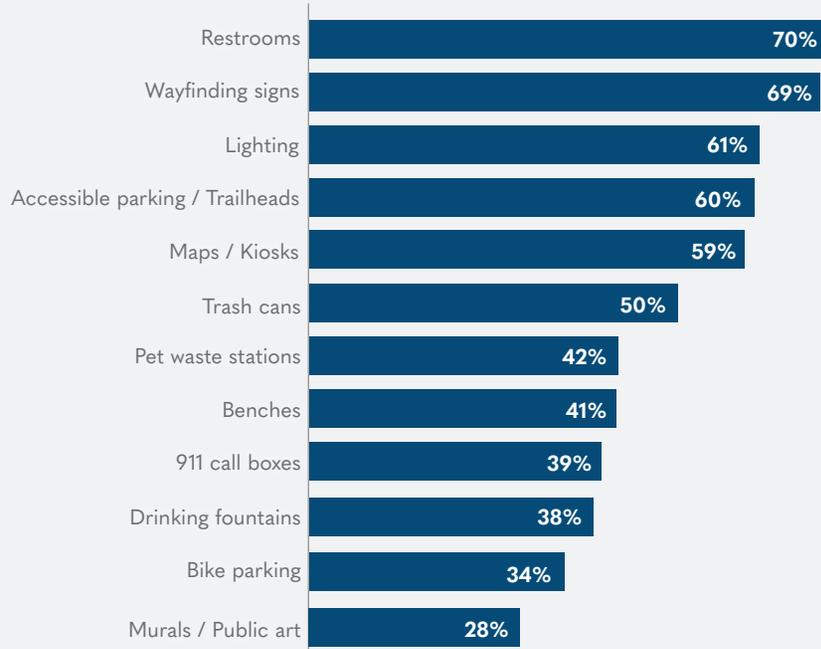
**(Select all that apply)**

Health, exercise, and recreation was cited as the most important factor for respondents as it relates to the Town's trail network. Survey respondents were less interested in trails supporting economic development and tourism within the Town at 19%.

**IMPORTANT NETWORK FACTORS**



**IMPORTANT AMENITIES**

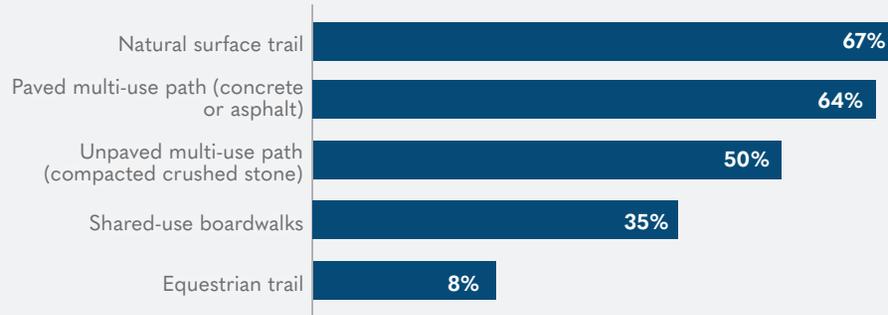


**Q10. WHAT AMENITIES ARE MOST IMPORTANT TO YOU FOR TRAILS AND MULTI-USE PATHS?**

**(Rank your top 5 from 1, most important, to 5, least important)**

Restrooms (70%), wayfinding signage (69%), and lighting (61%) ranked as the top three trail/multi-use path amenities for survey respondents. Murals and public art ranked the lowest at 28%.

## FACILITY PREFERENCE



**Q11. MULTI-USE PATHS AND TRAILS CAN BE CONSTRUCTED USING SEVERAL DIFFERENT MATERIALS. PLEASE SELECT YOUR PREFERRED FACILITIES.**

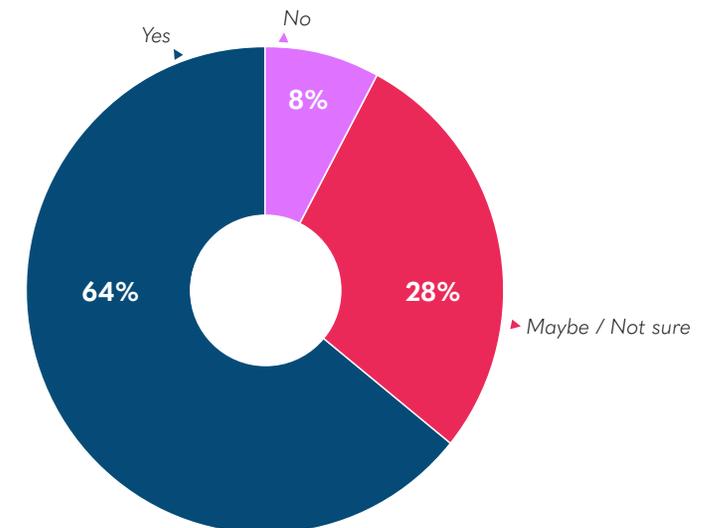
**(Select all that apply)**

Respondents prefer to use trail/multi-use paths that are either constructed using natural surface materials (67%) or paved (64%). There was little interest in equestrian trails at 8%.

**Q12. TRAIL NETWORKS ARE OFTEN FUNDED THROUGH BONDS. BONDS ARE DEBT ISSUED BY LOCAL GOVERNMENTS THAT ARE OFTEN USED TO FINANCE LARGE CAPITAL PROJECTS. IF THE OPPORTUNITY PRESENTS ITSELF, WOULD YOU PAY ADDITIONAL TAXES TO SUPPORT A BOND TO EXPAND THE DEVELOPMENT OF FORT MILL'S TRAIL NETWORK?**

Sixty-four (64%) of respondents would support bonds to expand the development of Fort Mill's trail network. Eight percent (8%) would not support the bonds and 28% said maybe or unsure.

## BOND PREFERENCES



"As a physician that lives and works in this community, I would love to bike to work! Additional trail network/bike path along 160 and/or 21 connecting existing trails would be incredible for those that want to commute safely on two wheels. Thank you for considering this healthy option!"

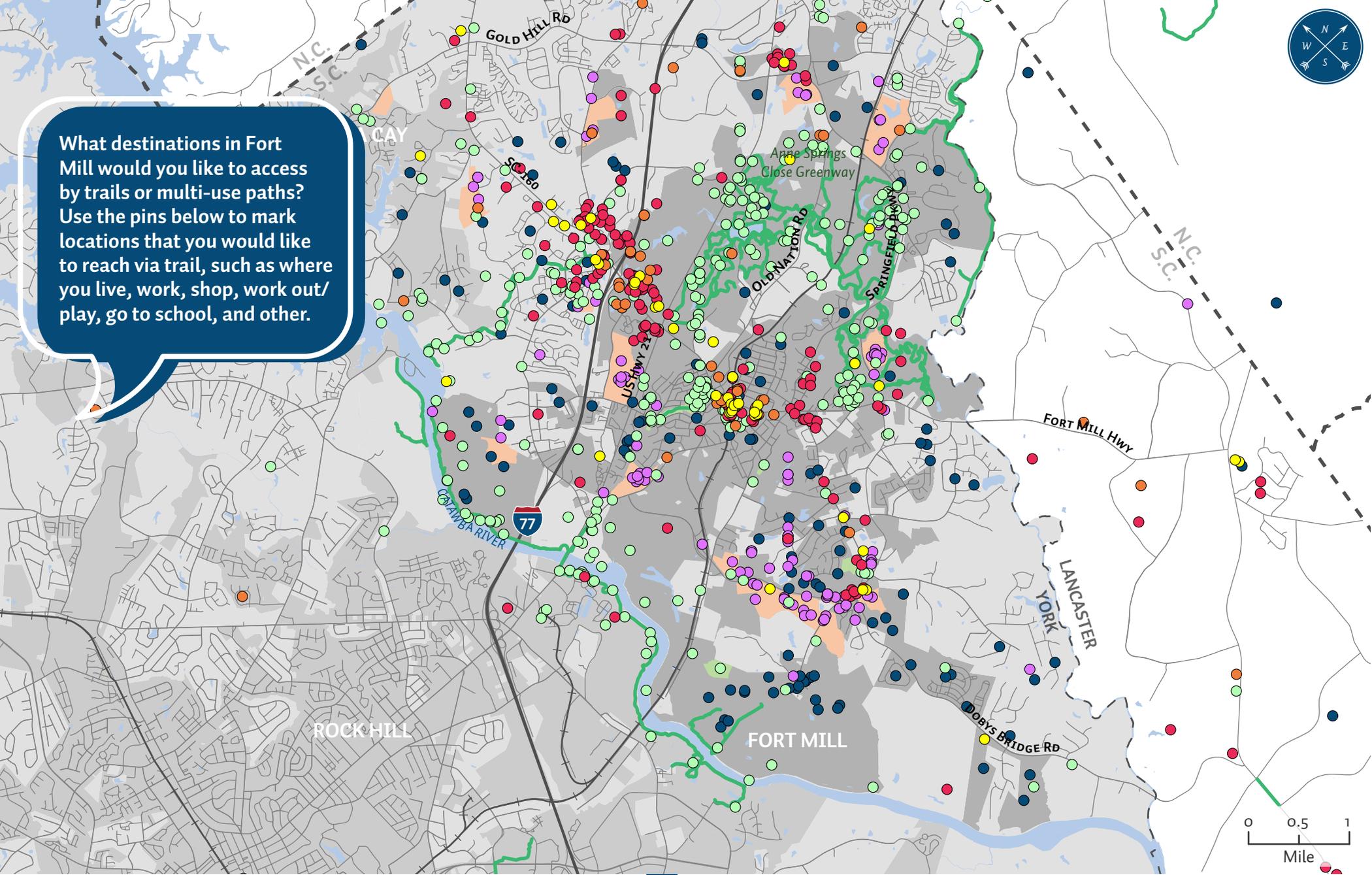
"Being a nature lover, I would love to see more opportunities to connect to nature. What fun it would be to walk from greenway to town to River walk as well!"

"Fort Mill has a strong special needs community. Please consider inclusion in all aspects of this venture."

"I live on Dogwood lane and have been observing the new sewer line project progress. I sincerely hope that the 20 foot wide path cleared through the woods is used for a trail once the pipe project is completed. I'm happy to see this trail plan is in the works. Trails are a great way to walk and explore without being near traffic, as we would be on sidewalks."

"As a runner and biker, I would love safe routes from where I live in Masons Bend off exit 83 to Downtown Fort Mill, Baxter Village, and to the Riverwalk. Being able to bike to things like Saturday morning coffee would be a game changer!"

What destinations in Fort Mill would you like to access by trails or multi-use paths? Use the pins below to mark locations that you would like to reach via trail, such as where you live, work, shop, work out/play, go to school, and other.



# FORT MILL TRAIL MASTER PLAN

## Preferred Destinations

LEGEND

<p><b>PREFERRED DESTINATIONS</b></p> <ul style="list-style-type: none"> <li><span style="color: purple;">●</span> School</li> <li><span style="color: lightgreen;">●</span> Recreation</li> <li><span style="color: red;">●</span> Shopping</li> <li><span style="color: darkblue;">●</span> Home</li> <li><span style="color: orange;">●</span> Work</li> <li><span style="color: yellow;">●</span> Other</li> </ul>	<ul style="list-style-type: none"> <li><span style="color: green;">—</span> Multi-use Path/Trail</li> <li><span style="color: gray;">—</span> Road</li> <li><span style="color: gray;">++</span> Railroad</li> </ul>	<ul style="list-style-type: none"> <li><span style="color: lightblue;">■</span> Water</li> <li><span style="color: orange;">■</span> Fort Mill School</li> <li><span style="color: lightgreen;">■</span> Fort Mill Park</li> <li><span style="color: gray;">■</span> Fort Mill</li> <li><span style="color: lightgray;">■</span> Other Jurisdiction</li> <li><span style="color: lightgray;">■</span> York County</li> <li><span style="border: 1px dashed gray; display: inline-block; width: 10px; height: 10px;"></span> County Boundary</li> </ul>
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## Interactive Mapping Question #1

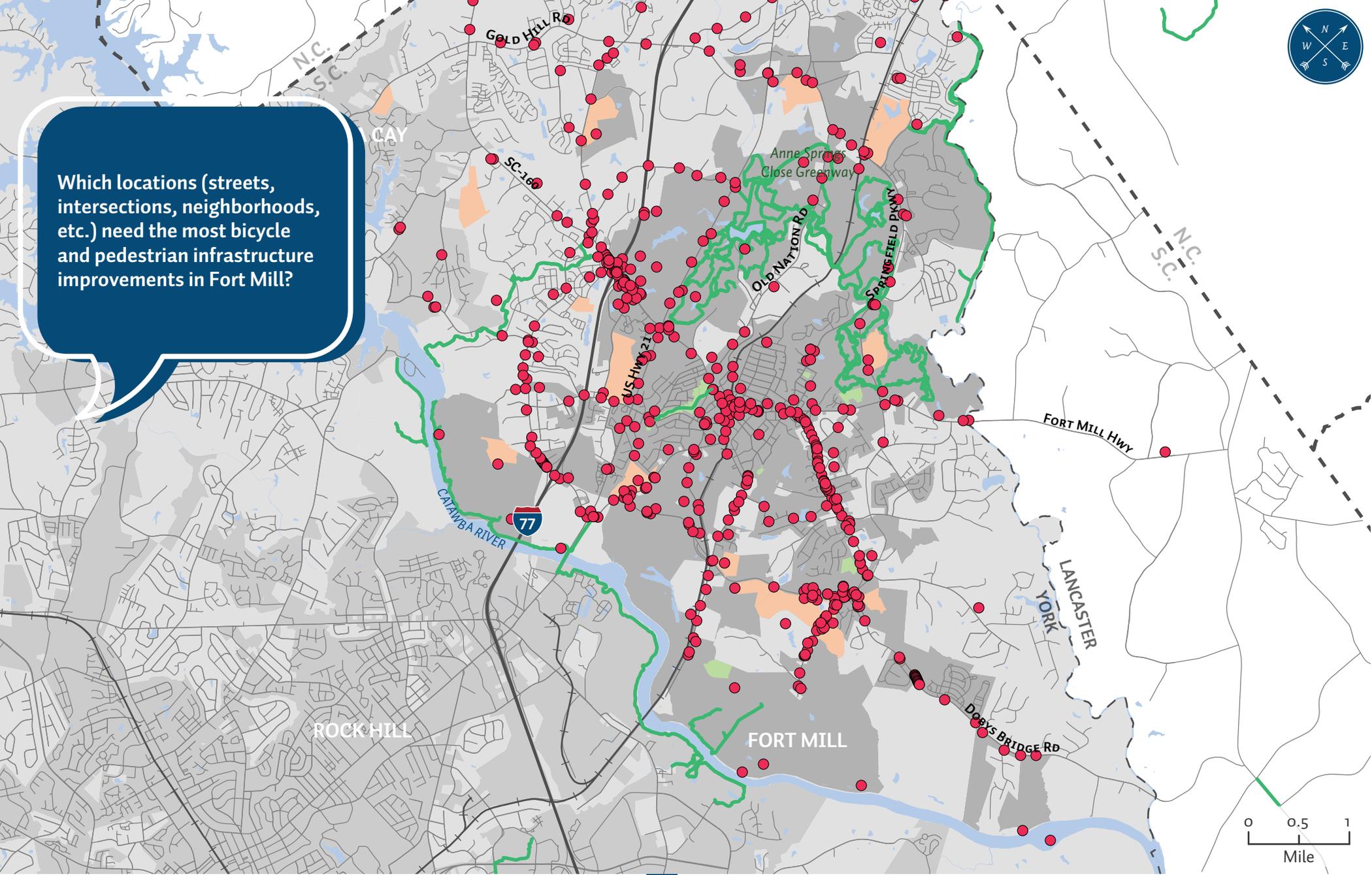
The first interactive mapping question asked participants to map their preferred destinations that they would like to access via the future trails network in Fort Mill. Participants could mark preferred destinations for their house, work location, recreational areas, schools, shopping destinations, and others. The results are outlined in the table below by category.

CATEGORY	PREFERRED DESTINATIONS
School	<ul style="list-style-type: none"> <li>▸ Fort Mill High School</li> <li>▸ Riverview Elementary School</li> <li>▸ Fort Mill Elementary School</li> <li>▸ Along Fort Mill Parkway</li> </ul>
Recreation	<ul style="list-style-type: none"> <li>▸ Baxter Village</li> <li>▸ Anne Springs Close Greenway</li> <li>▸ Downtown Fort Mill</li> <li>▸ YMCA</li> <li>▸ Catawba River/Riverwalk</li> </ul>
Shopping	<ul style="list-style-type: none"> <li>▸ Baxter Village</li> <li>▸ Kingsley Town Center</li> <li>▸ Downtown Fort Mill</li> </ul>
Home	<ul style="list-style-type: none"> <li>▸ Riverview area</li> <li>▸ Pecan Ridge/Waterside neighborhoods (Sc-158)</li> </ul>
Work	<ul style="list-style-type: none"> <li>▸ Downtown Fort Mill</li> <li>▸ Kingsley Town Center</li> <li>▸ Piedmont Medical Center</li> </ul>
Other	<ul style="list-style-type: none"> <li>▸ West Town Market area (SC-160)</li> <li>▸ Downtown Fort Mill</li> </ul>

The comments associated with the map question are summarized below:

- "A path along bypass 21 would make access to the main Anne Springs Close Greenway accessible via bike."
- "A path on 460 would make the Adventure Road entrance safely accessible via bike."
- "A safe path along the Fort Mill Parkway/bypass would make the Founders Trail entrance accessible via bike."
- "A safe way to access the Complex YMCA would allow recreation to start from your front door and eliminate the need to drive. This could be achieved with a bike lane on Tom Hall Street."
- "Improve access to Kingsley."
- "Connect neighborhoods to Baxter Village."
- "There is concern for publicizing trails that cut through neighborhoods."
- "Improve access to the river."
- "Improve crosswalks within the Town."

Which locations (streets, intersections, neighborhoods, etc.) need the most bicycle and pedestrian infrastructure improvements in Fort Mill?



# FORT MILL TRAIL MASTER PLAN

Areas in Need of Bicycle + Pedestrian Infrastructure Improvements

## LEGEND

- |  |   |   |
|--|---|---|
| <b>INFRASTRUCTURE NEEDS</b>                            | <span style="color: green;">—</span> Multi-use Path/Trail | <span style="color: lightblue;">■</span> Water                  |
| <span style="color: red;">●</span> Identified Location | <span style="color: gray;">—</span> Road                  | <span style="color: orange;">■</span> Fort Mill School          |
|  | <span style="color: gray;">+ +</span> Railroad            | <span style="color: lightgreen;">■</span> Fort Mill Park        |
|  |   | <span style="color: gray;">■</span> Fort Mill                   |
|  |   | <span style="color: lightgray;">■</span> Other Jurisdiction     |
|  |   | <span style="border: 1px dashed gray;">□</span> York County     |
|  |   | <span style="border: 1px dashed gray;">□</span> County Boundary |

## Interactive Mapping Question #2

The second interactive mapping question asked participants to mark areas (e.g., streets, intersections, neighborhoods, etc.) that need the most pedestrian and bicycle infrastructure improvements in the Town of Fort Mill.

The following were the top four areas that survey participants identified as needing bicycle and pedestrian infrastructure needs within the Town:

- ▶ Kingsley Town Center off SC-160
- ▶ Downtown Fort Mill along Main Street
- ▶ N Doby's Bridge Road between the Walmart and Doby Bridge Park
- ▶ Fort Mill Parkway area between Catawba Ridge High School and Doby's Bridge Elementary School

The comments associated with the map question are summarized below:

- ▶ "Improve connectivity to and between schools and neighborhoods."
- ▶ "Connect Baxter Village, Kingsley Town Center, and the Anne Springs Close Greenway."
- ▶ "Connect parks and shopping plazas to neighborhoods."
- ▶ "Fort Mill Parkway and Springfield Parkway need separated paths."
- ▶ "Need to connect Masons Bend to the rest of the community via trails along Sutton Road."
- ▶ "Need safer crossing to the Walmart near Downtown Fort Mill."
- ▶ "Consider connections across I-77."
- ▶ "There is preference for a trail between Oakland Pointe Community, YMCA, and neighborhoods surrounding the Walmart."
- ▶ "Along Dobys Bridge Road, especially as it approaches Tom Hall."

FEB  
8

## PUBLIC WORKSHOP

A public workshop was held on February 8, 2023, at Unity Presbyterian Church in Downtown Fort Mill. Printed boards contained information on the public survey results, draft trail network, and draft program and policy recommendations. A handout was also distributed to attendees that explained the purpose of the project, as well as the vision and goals to guide the plan. Community members were asked to provide feedback on the draft trail network by selecting their top five projects on the map.

100+  
ATTENDEES

*Public Workshop Attendees Discussing the Draft Trail Network*

### KEY FINDINGS

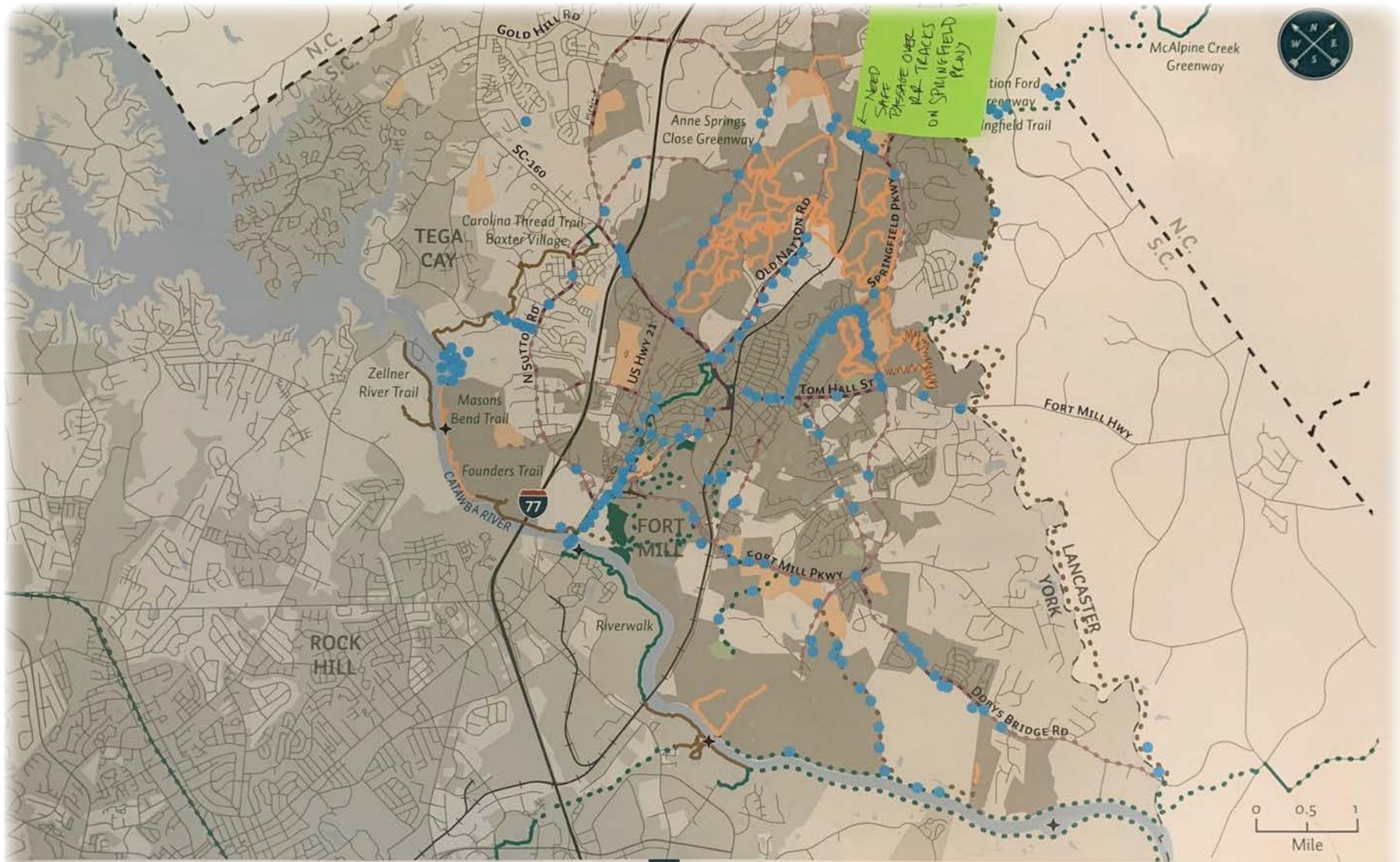
The top areas identified on the map to the right include:

- ▶ Gap between Masons Bend Trail and the Carolina Thread Trail – Baxter Village
- ▶ Proposed trail behind Riverview Elementary School
- ▶ Steele Street (Historic Paradise Community)

### Priority Projects

During an interactive mapping exercise, meeting attendees placed sticky dots on the draft trail network map to indicate areas that represented their top five priority projects. Attendees also submitted comment sheets that contained additional details related to the areas they selected. The top project areas identified during the meeting are listed above. Other key comments captured in the comment sheets are provided below.

- ▶ Close the sidewalk gap between River Trail and Catawba Ridge High School.
- ▶ Add a trail connection between Sutton Road, the Carolina Thread Trail at Baxter Village, and Founders Trail.
- ▶ Add trail connections between the Old Orchard neighborhood and Downtown Fort Mill.
- ▶ Repair existing sidewalks on Old Nations Road.
- ▶ Add a sidepath along New Gray Rock Road to link Sutton/Baxter with the new Catawba Park.



# FORT MILL TRAIL MASTER PLAN

## Draft Trail Network

Please mark your top five priority projects on the map.

### LEGEND

- ◆ Existing Blueway Access
  - Road
  - ⊕ Railroad
  - Private Facility
  - Existing Sidewalk
- MULTI-USE PATH/TRAIL STATUS**
- Complete Paved Trail
  - ⋯ Planned Paved Trail
  - Complete Natural Surface Trail
  - ⋯ Planned Natural Surface Trail
  - Complete Sidepath
  - ⋯ Planned Sidepath
- Fort Mill School
  - Fort Mill Park
  - Future Park
  - Water
  - Fort Mill
  - Other Jurisdiction
  - York County
  - County Boundary



CATAWBA RIVER

FOUNDERS TRAIL OVERLOOK



# 04

## NETWORK RECOMMENDATIONS

### OVERVIEW

This chapter provides insight into design guidance standards that apply to the recommended trail network in this plan. Typical cross sections are also described and exhibited throughout this section. The end of the chapter discusses the recommended trail network for the Town and highlights the proposed crossing treatment enhancements that will help trail users safely and efficiently traverse trail corridors.

Sections:

- ▶ Design Guidance
  - » Typical Cross Sections
- ▶ Comprehensive Trail Network
- ▶ Prioritization
- ▶ Priority Projects
- ▶ Trail Amenities

## DESIGN GUIDANCE

This chapter translates the vision for an accessible trail system that connects people, parks, and frequently visited destinations in the community into a strategic set of recommendations with proposed strategies for network design and trail amenity development.

The following publications were referenced during the development of the trail network for this plan:

- ▶ National Association of City Transportation Officials' Urban Bikeway Design Guide and Urban Street Guide;
- ▶ American Association of State Highway and Transportation Officials' Guide for the Development of Bicycle Facilities and Guide for the Planning, Design and Operation of Pedestrian Facilities;
- ▶ Manual on Uniform Traffic Control Devices;
- ▶ Americans with Disabilities Act; and
- ▶ SCDOT Complete Streets Guidelines

### Design Standards

The standards and methods in which trails and multi-use paths (i.e., sidepaths and greenways) are designed impact the experience and safety of the diverse set of users that take to these facilities for a variety of recreational, utilitarian, health, and transportation purposes. This section illustrates aspects of facility design to help guide future actions by the Town of Fort Mill and its partners in planning for, designing, constructing, and maintaining a trail network that connects to a variety of destinations, promotes a diverse user experience, and is built to a maintainable scale.

### Materials Selection

Factors to be considered in the selection of materials for trails and multi-use paths include anticipated facility type (recreational versus commuter), expected use activities (i.e., walking, bicycling, running, or hiking), age and ability of users, environmental conditions, construction cost, maintenance burden and costs, and funding source requirements among others. A variety of materials are described and illustrated on the following pages.



**Concrete Trail - Downtown Greenway, Greensboro, NC**



**Asphalt Trail - Lower McAlpine Greenway, Charlotte, NC**



**Compacted Aggregate Trail - Peavine Trail, Marion, NC**



**Compacted Soil Trail - American Tobacco Trail, Apex, NC**

**Paved Surface**

Paved trail surfaces in the form of asphalt or concrete offer great accessibility to accommodate users of all ages and abilities. Asphalt pavement tends to be the most widely-used and cost effective surface type (i.e., sidepaths and greenways). Concrete pavement is more durable, but often costs more than asphalt pavement.

As such, concrete trails are typically more common in urban settings (where projected user volumes are high or the trail may be subject to vehicular loading more often) or in areas subject to heavy flooding and more prone to water damage.

**Natural Surface**

Compacted aggregates and compacted native soil are two types of natural surfaces considered for the trail network. Both are budget-friendly options that have been implemented in trail systems across South Carolina and beyond to provide a durable alternative to paved trails.

Granite fines are commonly used for compact aggregates due to their availability from local quarries and their ability to achieve compaction of a level surface that is accessible to most users. Proper compaction and handling adjacent drainage are keys to the success of using this material.

Annual maintenance is required to ensure that crown pitches drain.

This typically includes laying and grooming additional aggregate as necessary to ensure that water rills and deep grooves do not form within the trail surface following regular rain events. It is critical to establish drainage adjacent to the trail during construction to ensure concentrated runoff does not cross the trail perpendicularly. Over time, these types of flows will erode the trail and surfacing.

Compacted native soil is one of the most affordable and easily constructed natural surface options. Similar to compacted aggregate, this surfacing method relies heavily on material compaction and adjacent drainage.

The existing soil must be able to achieve compaction levels of 95-98% to ensure trail integrity. If native soil is unsuitable, material may be imported at an additional cost and installed with a roller and small excavating equipment depending on topography.

Surface drainage should not be allowed to cross perpendicular to the trail to prevent washout and rills within the walking surface. Annual maintenance is required and includes re-compaction to ensure the trail remains crowned to properly pitch water to adjacent drainage features. Installation of additional soil in rain wash out areas may also be required.

### Boardwalk

Timber and concrete (cast-in-place or pre-cast) are the two most popular material options for boardwalk decks. Timber has traditionally been used for its lower construction cost, but it requires regular maintenance and deck repairs/replacement to extend its functional life. Concrete deck options typically cost more upfront but have lower life-cycle costs due to reduced maintenance requirements.

Regardless of the deck type, safety rails and handrails should still be provided in accordance with applicable building codes. Timber, metal, and composite railing options are available depending on site conditions, desired aesthetics, and budget. Boardwalk substructure design and materials may vary depending upon specific site conditions and geotechnical recommendations.



**Timber Deck Boardwalk - White Oak Creek Greenway, Cary**



**Concrete Deck Boardwalk - Toby Creek Greenway, Charlotte**



**Asphalt Surface Greenway - Swamp Rabbit Trail, SC**  
 Credit: Chris Decker, Firewater Photography

## Typical Cross Sections

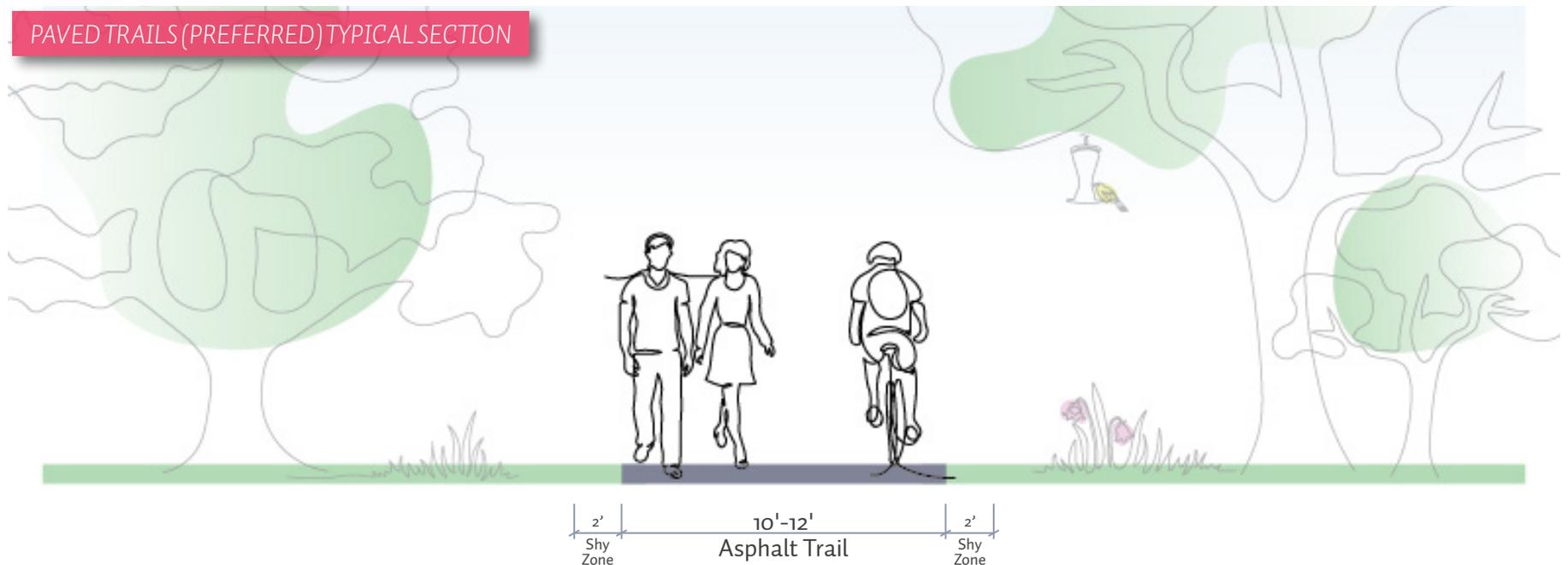
The following typical cross sections are discussed in this section:

- ▶ Paved Trails (Preferred)
- ▶ Paved Trails (Constrained)
- ▶ Boardwalk
- ▶ Sidepath

### Paved Trails (Preferred)

A 10 to 12 ft wide paved trail is recommended for the preferred typical section, as it requires the least amount of long-term maintenance and greater eligibility from the widest variety of funding sources. Asphalt pavement is recommended based on site conditions within Fort Mill, anticipated facility use, and cost considerations. Limited sections of concrete pavement may be required to accommodate site conditions as necessary. Shoulders or shy zones of 2 ft or greater should be kept clear of any obstacles to ensure full trail width remains usable.

### PAVED TRAILS (PREFERRED) TYPICAL SECTION



### Paved Trails (Constrained)

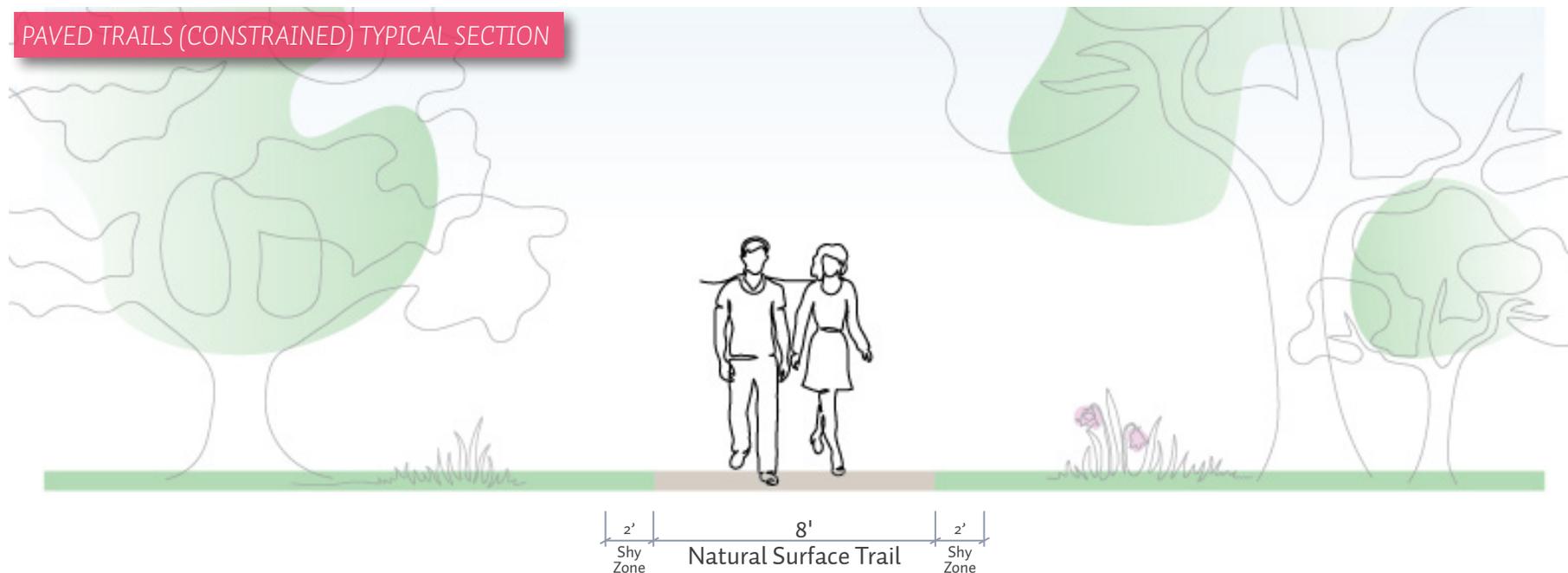
A slightly narrower 8 ft wide natural surface or asphalt trail may be considered for segments within the trail network that are constrained due to environmental or right-of-way issues. This typical section may also be considered for neighborhood connections to trail corridors.

When considering the use of natural surface for constrained trail segments, it is important to evaluate the overall life-cycle costs of unpaved surfaces, which may be higher due to long-term maintenance requirements. As in the preferred typical section, shoulders, or shy zones of 2 ft or greater should be kept clear of any obstacles to ensure the full trail width remains usable.



Natural Surface - The Palmetto Trail, SC  
Credit: Visit Oconee

#### PAVED TRAILS (CONSTRAINED) TYPICAL SECTION



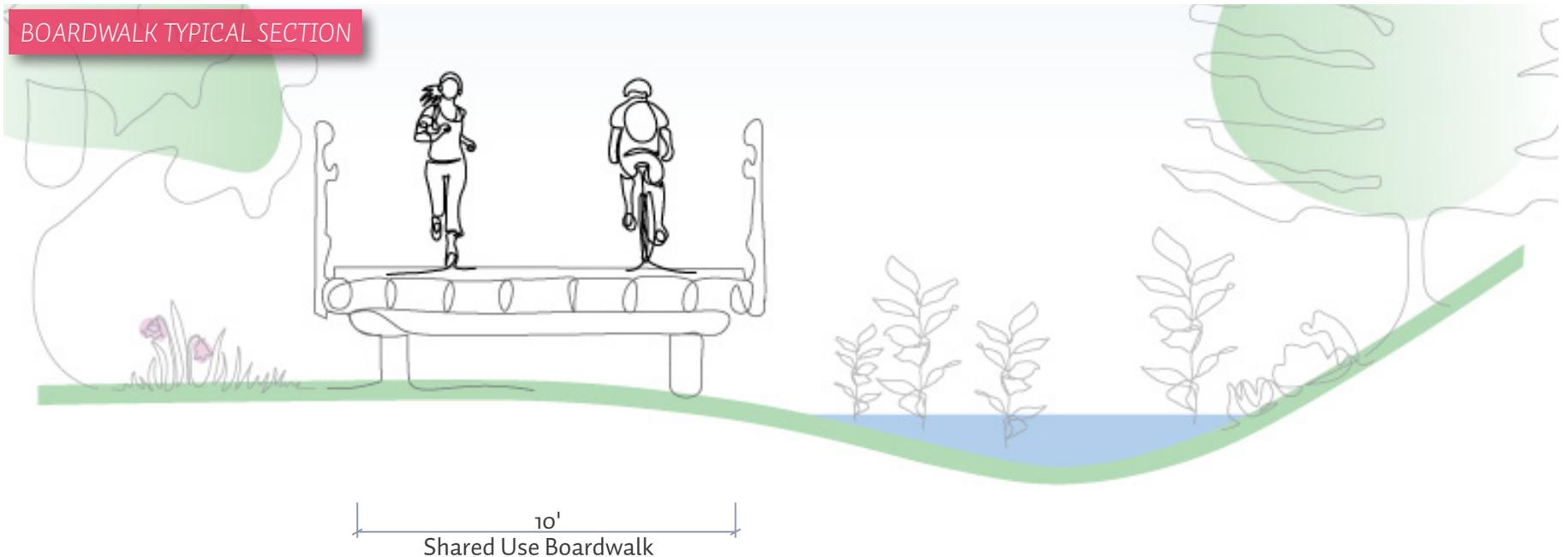


Concrete Deck Boardwalk on Lake Crabtree - Cary, NC

### Boardwalk

A 10 ft clear width elevated boardwalk is recommended in areas where multi-use paths or trails cross streams or wetland areas. For deck surface considerations, concrete provides greater friction to reduce the risks of slips and falls and reduces long-term maintenance burdens compared to those associated with other materials such as timber. Timber safety rails and handrails are shown with a timber pile substructure system in the image to the left. Boardwalk substructure design and materials may vary depending upon specific site conditions and geotechnical recommendations.

#### BOARDWALK TYPICAL SECTION

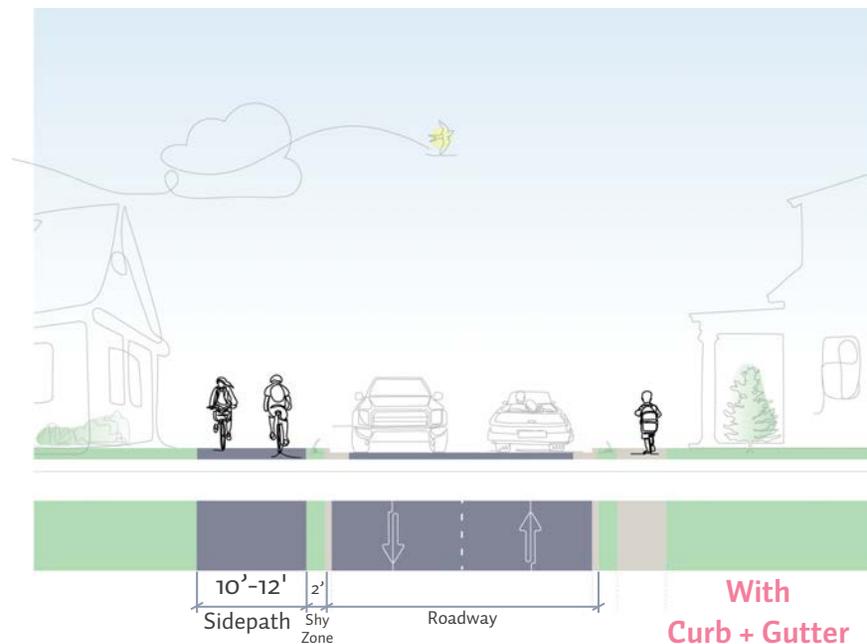
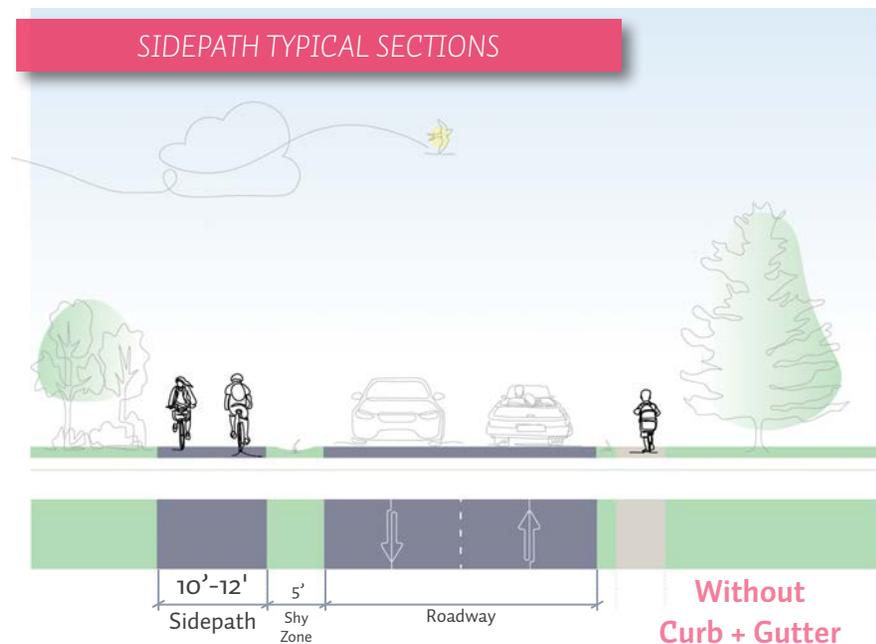


## Sidepath

A 10 to 12 ft paved sidepath is recommended along roadway corridors where the trail network connects with the transportation network, allowing residents and visitors to reach destinations in residential neighborhoods and commercial areas. Asphalt pavement is recommended based on site conditions within Fort Mill, anticipated facility use, and cost considerations. Limited sections of concrete pavement may be required to accommodate site conditions as necessary. A 5-ft planting strip is provided to separate users from motorized traffic on the adjacent roadway. Planting strip width may be reduced to 2 ft in highly constrained areas where curb and gutter are present. A slightly narrower 8 ft wide sidepath may be considered for segments within the trail network that are constrained.



Sidepath - American Tobacco Trail, NC



## Intersection Treatments + Crossings

Most conflicts between active transportation users and motor vehicles occur at intersections. Current roadway design decisions increase exposure and risk for bicyclists and pedestrians, reduce user safety and comfort of the trail network, and discourage biking and walking between destinations. As communities work to make facilities safer for users of all ages and abilities, intersection design is key.



**Crosswalk + Refuge Island**

### Crosswalk

A crosswalk is a designated place for trail users to cross a road at an intersection or mid-block crossing. High-visibility crosswalk markings, such as bar pairs, continental, zebra, and ladder are recommended to alert motorists of trail or multi-use path crossing points across roadways. Inlay or thermoplastic tape is also recommended for highly reflective crosswalk markings. Crosswalks should be accompanied by MUTCD pedestrian warning and advance yield signs W11-2, W16-7P, and R1-6 to alert motorists of a crosswalk.

### Refuge Island

Pedestrian refuge islands allow users to stop before they finish crossing the road and reduces the exposure of the pedestrian in the roadway. These islands are typically made of pavement and are surrounded by asphalt or other road materials.



**Rectangular Rapid Flashing Beacon**

### Rectangular Rapid Flashing Beacon

A Rectangular Rapid Flash Beacon (RRFB) is a pedestrian-actuated signal used in combination with pedestrian or trail crossing warning signage to improve safety at uncontrolled, marked crosswalks. The device includes two rectangular-shaped yellow indications, each with an LED-array-based light source, that flash with high frequency when activated. RRFBs should be accompanied by MUTCD pedestrian warning and advance yield signs W11-2 and W16-7P to alert motorists of a crosswalk.

RRFBs are placed on both ends of a crosswalk. If the crosswalk contains a pedestrian refuge island or other type of median, an RRFB should be placed to the right of the crosswalk and on the median (instead of the left side of the crosswalk). RRFBs typically draw power from standalone solar panel units but may also be wired to a traditional power source.



**High Intensity Activated Crosswalk**

### High-Intensity Activated Crosswalk

A High-Intensity Activated Crosswalk (HAWK) signal, also known as a Pedestrian Hybrid Beacon, is a traffic control signal designed to help pedestrians and greenway users safely cross streets. HAWK signals like other push-button activated traffic signals stop traffic with a red signal, allowing pedestrians to cross with a WALK signal. In addition to the signal head displays, stop lines, and marked crosswalks are required at HAWK crossings. Advance stop lines should be used on multi-lane crossings to reduce the potential for crashes. HAWKs should be accompanied by MUTCD pedestrian warning and advance yield signs W11-2, W16-7P, R10-23, and R9-8 to alert motorists of the signalized pedestrian crossing.

### Shared Use Tunnel

A 10 ft clear width, concrete paved tunnel is recommended for grade-separated crossings under roadways to connect multi-use path and trail sections at either end of the tunnel. The desired minimum vertical clearance inside the tunnel is 12 ft. Designs should maximize the vertical clearance within the tunnel to the extent practicable based on specific site constraints to maintain a sense of openness and security for users.

Lighting inside of tunnels is recommended to ensure continual visibility and user safety. Consideration should also be given to potential incorporation of dynamic lighting, vibrant murals, or other public art elements that create a comfortable and inviting environment for users.



Davis Dr Side-Street Trail Greenway - Cary, NC

#### SHARED USE TUNNEL TYPICAL SECTION



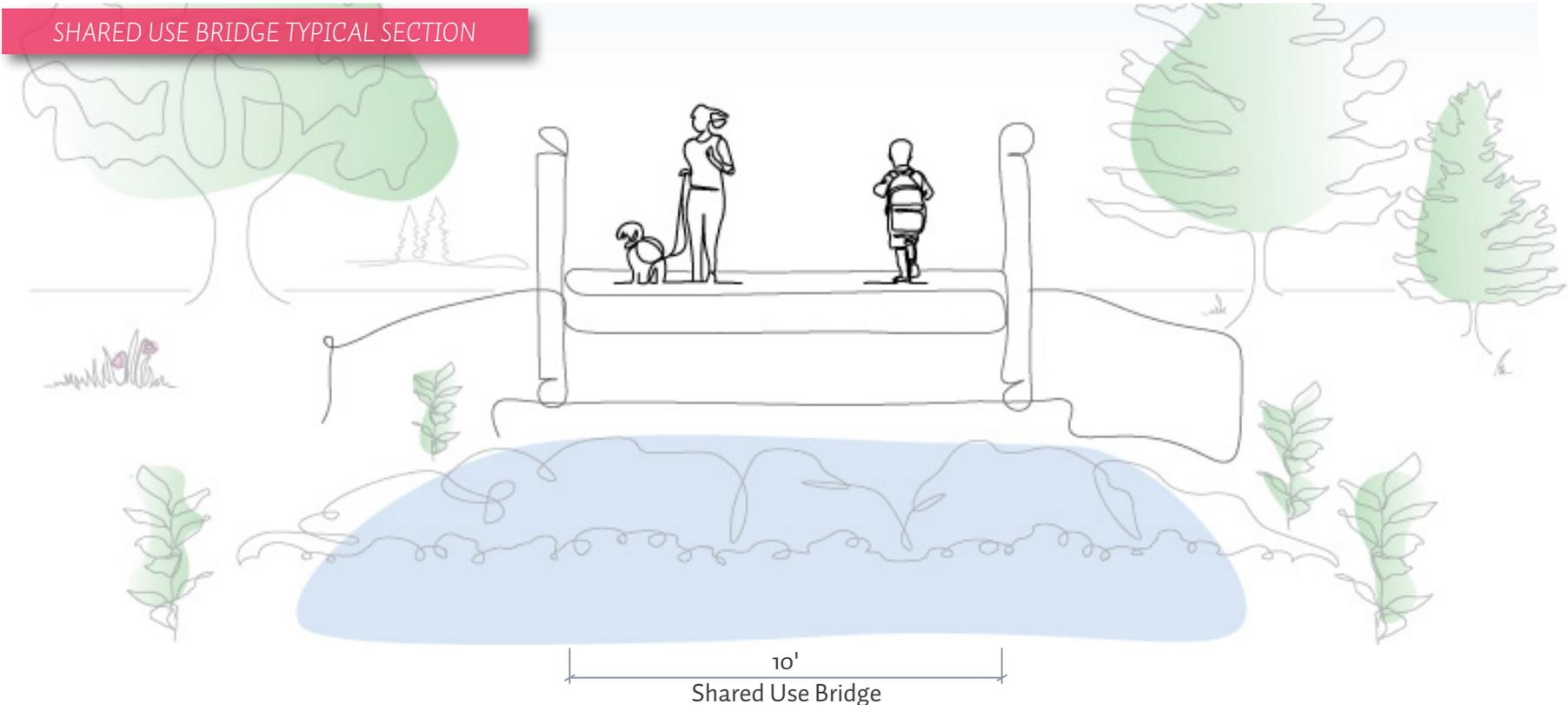


Concrete Deck Bridge - Neuse River Trail, NC

### Shared Use Bridge

A 10 ft clear width bridge is recommended for grade-separated crossings over streams and roadways to connect greenway sections at either end of the bridge. A variety of design types are available including truss bridges, girder (beam) bridges, arch bridges, suspension bridges, and cable-stayed bridges among others. Bridge design type selection will vary depending on specific site conditions, cost constraints, and potential impacts to traffic during construction.

Safety rails and handrails should be provided in accordance with applicable building codes and the SCDOT Bridge Policy. Minimum vertical clearance over roadways shall be provided based on SCDOT Bridge Policy requirements. For deck surface considerations, concrete provides greater friction to reduce the risks of slips and falls and reduces long-term maintenance burdens compared to those associated with other materials such as timber.



## COMPREHENSIVE TRAIL NETWORK

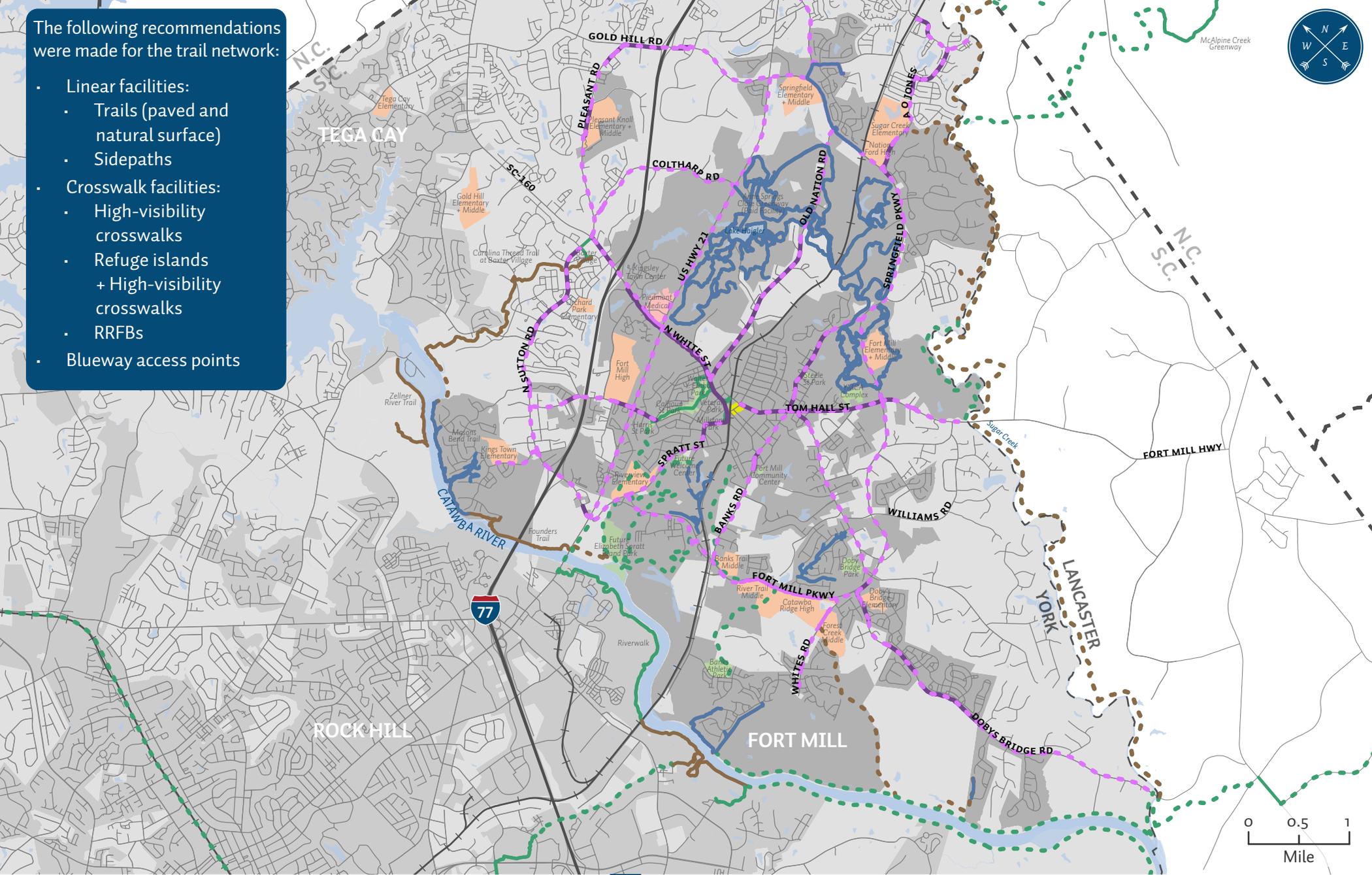
Following both the existing conditions analysis and the opportunities and constraints analysis, the project team consolidated proposed trail corridors into one Town-wide network plan. This plan incorporates any updates to trail routing based on opportunities and constraints identified in the analysis to minimize environmental impact, reduce cost, and improve user safety and experience.

The proposed network seeks to connect desired locations (e.g., schools, parks, business districts, employment centers, etc.) with greenways, trails, multi-use paths and/or sidewalks and bike lanes (where appropriate) to provide a complete system of active transportation alternatives. The network is designed to suit a wide range of users and will serve all residents of various ages and abilities within the Town of Fort Mill. The proposed network reflects the input received from Town staff and other stakeholders during engagement efforts held throughout the duration of the project. Network recommendations outlined in this plan aim to complement previous planning efforts by the Town of Fort Mill including land use, zoning, transportation, and utility expansion plans. The following map displays the proposed trail network for the Town of Fort Mill.

Network recommendations for the Town are categorized by facility type. Intersection treatments along each facility corridor are also documented later in this chapter.

The following recommendations were made for the trail network:

- Linear facilities:
  - Trails (paved and natural surface)
  - Sideways
- Crosswalk facilities:
  - High-visibility crosswalks
  - Refuge islands + High-visibility crosswalks
  - RRFBs
- Blueway access points



# FORT MILL TRAIL MASTER PLAN

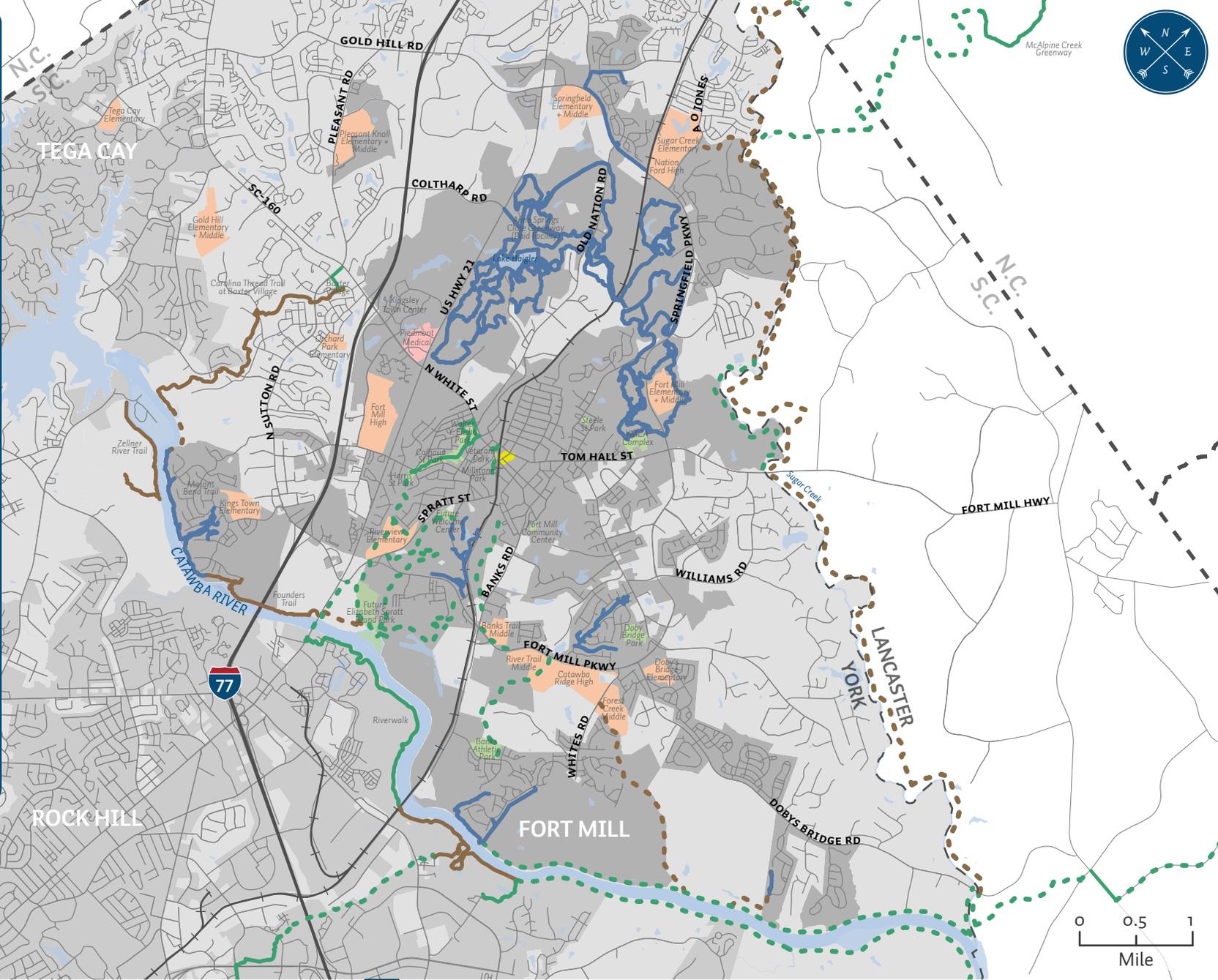
## Proposed Trail Network

### LEGEND

- Road
  - ⊕ Railroad
  - Private Facility
  - Existing Sidewalk
- MULTI-USE PATH/TRAIL STATUS**
- Complete Paved Trail
  - - - Proposed Paved Trail
  - Complete Natural Surface Trail
  - - - Proposed Natural Surface Trail
  - Complete Sidewalk
  - - - Proposed Sidewalk
- Fort Mill School
  - Fort Mill Park
  - Hospital
  - Downtown/Main St
  - Water
  - Fort Mill
  - Other Jurisdiction
  - York County
  - County Boundary



The proposed trails in the network aim to bridge the gap between existing trails, private trails, schools, and parks. Several of the proposed trails on this map were already planned for at the beginning of the planning process for this project. Two examples include the proposed segment between the Masons Bend Trail (private) and the Carolina Thread Trail to Baxter Village and the trails connecting the future Elizabeth Spratt Island Park to the Future Welcome Center (Spratt Cemetery). As identified in this plan, a new trail connector will serve as a recreational amenity between Riverview Elementary and Harris Street Park. Another proposed trail segment will tie the old landfill site on the Sugar Creek to proposed sidepaths along Fort Mill Hwy at the Town's most eastern limits. A third trail will hug the northern edge of the Catawba River to provide river access to Town residents.



# FORT MILL TRAIL MASTER PLAN

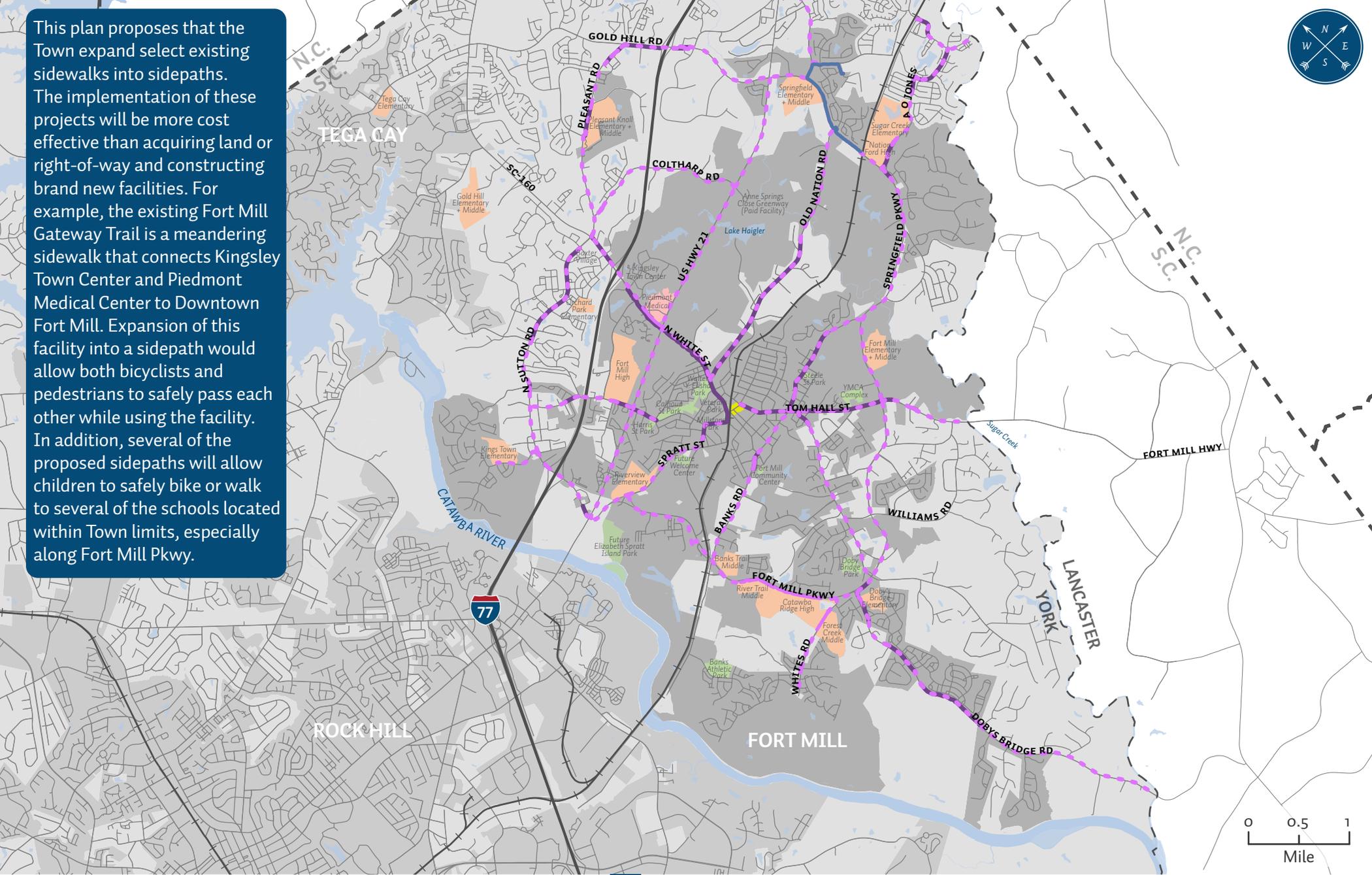
## Existing + Proposed Trails

### LEGEND

- Road
  - ⊕ Railroad
  - Private Facility
- MULTI-USE PATH/TRAIL STATUS**
- Complete Paved Trail
  - - - Proposed Paved Trail
  - Complete Natural Surface Trail
  - - - Proposed Natural Surface Trail
- Fort Mill School
  - Fort Mill Park
  - Hospital
  - Downtown/Main St
  - Water
  - Fort Mill
  - Other Jurisdiction
  - York County
  - County Boundary



This plan proposes that the Town expand select existing sidewalks into sidepaths. The implementation of these projects will be more cost effective than acquiring land or right-of-way and constructing brand new facilities. For example, the existing Fort Mill Gateway Trail is a meandering sidewalk that connects Kingsley Town Center and Piedmont Medical Center to Downtown Fort Mill. Expansion of this facility into a sidepath would allow both bicyclists and pedestrians to safely pass each other while using the facility. In addition, several of the proposed sidepaths will allow children to safely bike or walk to several of the schools located within Town limits, especially along Fort Mill Pkwy.



# FORT MILL TRAIL MASTER PLAN

## Existing + Proposed Sidepaths

**LEGEND**

- Road
  - Railroad
  - Existing Sidewalk
  - Private Facility
- SIDEPATH STATUS**
- Complete Sidepath
  - Proposed Sidepath
- Fort Mill School
  - Fort Mill Park
  - Hospital
  - Downtown/Main St
  - Water
  - Fort Mill
  - Other Jurisdiction
  - York County
  - County Boundary



## Carolina Thread Trail

The Carolina Thread Trail is a regional network of connected greenways, trails and blueways that spans 15 counties and 2 states and reaches 2.9 million people. There are over 300 miles of trails and 170 miles of blueway open to the public that aim to link people to places, and communities to each other. The Thread Trail preserves natural areas and is a place for recreation, transportation, and conservation. As a landmark project, the Carolina Thread Trail provides public and community benefits for people of every age, every background, and in every community in the region. Its mission is to create a legacy of conservation and connectivity through a regional network of trails.

### Regional Trails

Within the immediate region surrounding Fort Mill, the following existing trails are part of the Carolina Thread Trail:



#### Baxter Village Trail

The Baxter Village Trail is part of a 9-mile network of trails within the Baxter Village community. This 3.3-mile segment of the Carolina Thread Trail is moderately hilly and features several streams along its length.



#### Blue Star Trail

The Blue Star Trail is 7.2 miles long, of which 1.4 miles is designated Thread Trail. The Blue Star Trail is part of the larger Anne Springs Close Greenway network of trails totaling 40 miles.



#### Founders Trail

Founders Trail is a beautiful natural surface trail along the Catawba River with a sidewalk connection to the Riverwalk: Piedmont Medical Center Trail and River Park.



#### Masons Bend Trail

Masons Bend Trail is a natural surface trail that connects to the Founders Trail and then to Riverwalk along the Catawba River. It backs up to the Masons Bend neighborhood in Fort Mill, SC.



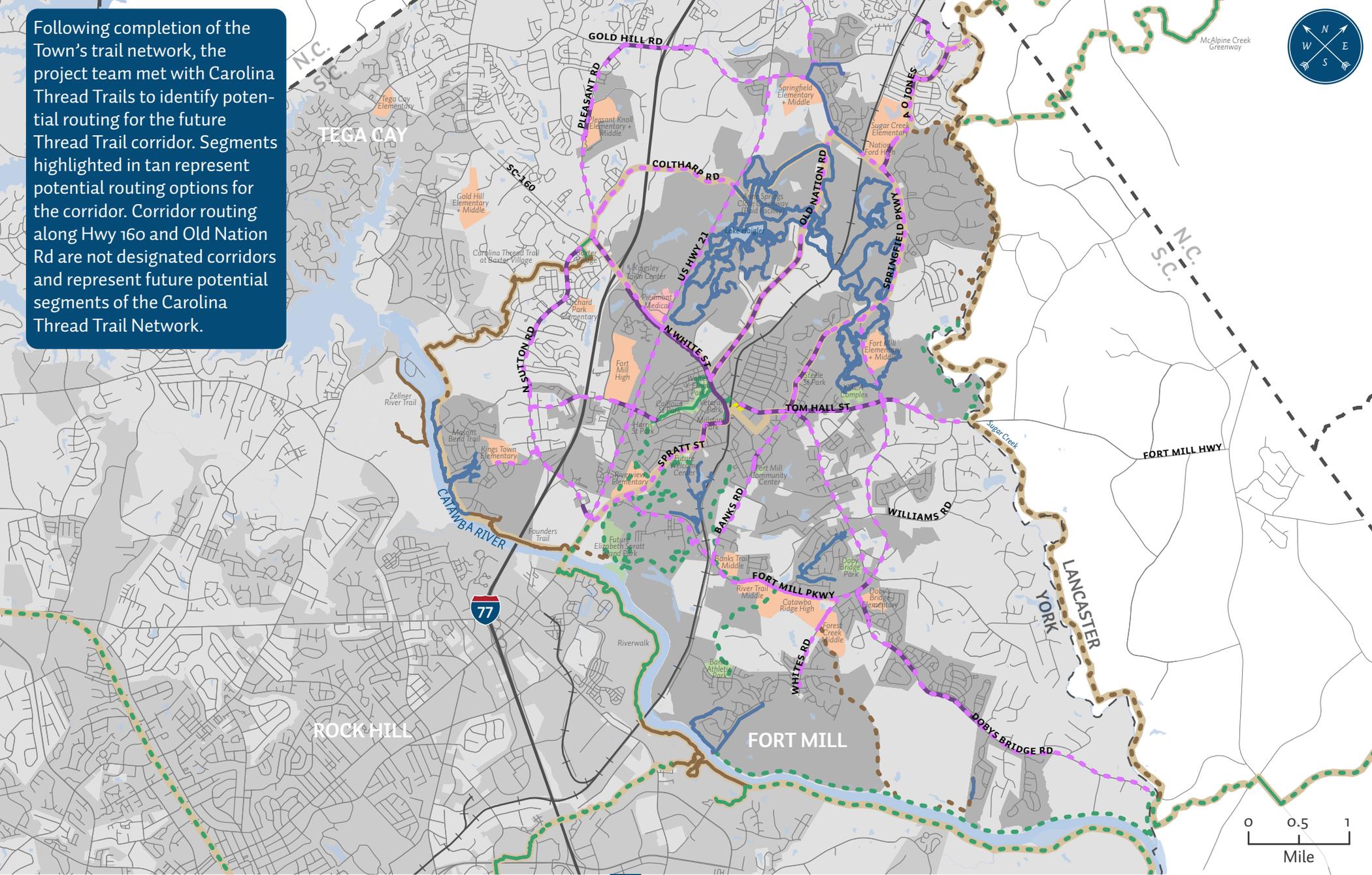
#### Riverwalk (Rock Hill)

The Riverwalk, also known as the Piedmont Medical Center Trail, is a greenway that runs adjacent to both the Riverwalk community and the Catawba River and connects south to River Park.



Credits: Carolina Thread Trail

Following completion of the Town's trail network, the project team met with Carolina Thread Trails to identify potential routing for the future Thread Trail corridor. Segments highlighted in tan represent potential routing options for the corridor. Corridor routing along Hwy 160 and Old Nation Rd are not designated corridors and represent future potential segments of the Carolina Thread Trail Network.



# FORT MILL TRAIL MASTER PLAN

## Proposed Trail Network + Potential Carolina Thread Trail (CTT) Network

### LEGEND

- Road
  - ⊕ Railroad
  - Private Facility
  - Existing Sidewalk
  - Potential CTT Corridor
- MULTI-USE PATH/TRAIL STATUS**
- Complete Paved Trail
  - ⋯ Proposed Paved Trail
  - Complete Natural Surface Trail
  - ⋯ Proposed Natural Surface Trail
  - Complete Sidepath
  - ⋯ Proposed Sidepath
- Fort Mill School
  - Fort Mill Park
  - Hospital
  - Downtown/Main St
  - Water
  - Fort Mill
  - Other Jurisdiction
  - York County
  - County Boundary



## Recommended Crossing Treatments and Blueway Access Locations

Following the completion of the trail network, the project team identified crossing treatments for intersections, roadways, and commercial driveways. Recommended crossing improvements displayed in the map to the right include high-visibility crosswalks and mid-block crossings (RRFBs). Several existing crosswalks were recommended as new high-visibility crosswalks to enhance the visibility of the crossings to motorists in the vicinity of a trail. Future implementation of these crossing treatments may foster an environment that is both safer and more comfortable for trail users in the Town.

In addition to crossing improvements, the project team recommended future blueway access points within the trail network. These future access points will enable residents and visitors to access the Sugar Creek and the Catawba River via the proposed trail network. Please note that these locations are subject to change and must undergo a feasibility study to determine exact corridor alignments.

Applications of the crossing treatments and blueway access points are provided below. More information on crossing treatments is provided at the beginning of this chapter.

### Crossing Treatments

Marked crosswalks may indicate a safe crossing path for trail users over a road, but they may not always be sufficient for drivers as a cue to yield to bicyclists or pedestrians. Restriping crosswalks or installing new crossings as high-visibility crosswalks may help bring more attention to a trail user that is crossing a road or driveway. A Rectangular Rapid Flash Beacon (RRFB) is an example of a crossing enhancement that may be installed at mid-block crossings in the Town to improve safety for users. RRFBs are pedestrian-activated signals that flash once the button is pushed.

Mid-block crossings (RRFBs) are recommended at the following locations:

- ▶ Downtown Fort Mill on Main Street
- ▶ Springfield Parkway at Anne Springs Close Greenway - Adventure Road
- ▶ Harris Street between Calhoun Street Park and Harris Street Park
- ▶ Banks Road where it connects to the proposed greenway (east of the existing railroad tracks)

There are 248 proposed high-visibility crossings within the recommended trail network. The majority of these crossings are located at driveways, commercial building/shopping area entrances, over roadways, or at intersections.

### Blueway Access

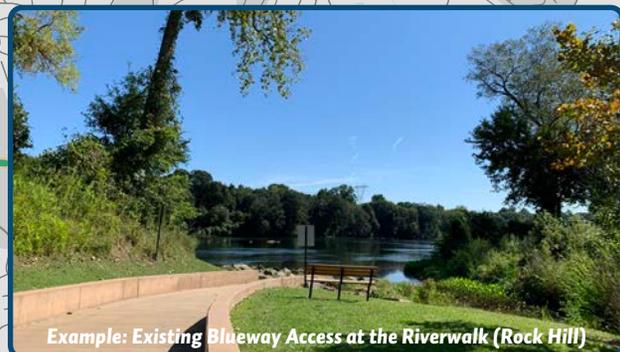
The Carolina Thread Trail defines blueways (or water trails) as a water path or trail that is developed with launch points, camping locations or points of interest for canoeists, paddle boarders, and kayakers. There are four existing blueways shown along the Catawba River in the map to the right (two in Fort Mill and two in Rock Hill). Two new blueway access points are proposed on the Sugar Creek near the old landfill site off Fox Run Drive and at Doby's Bridge Road near the York County boundary line. A third blueway access point is located south of Doby's Bridge Road near the Catawba River.



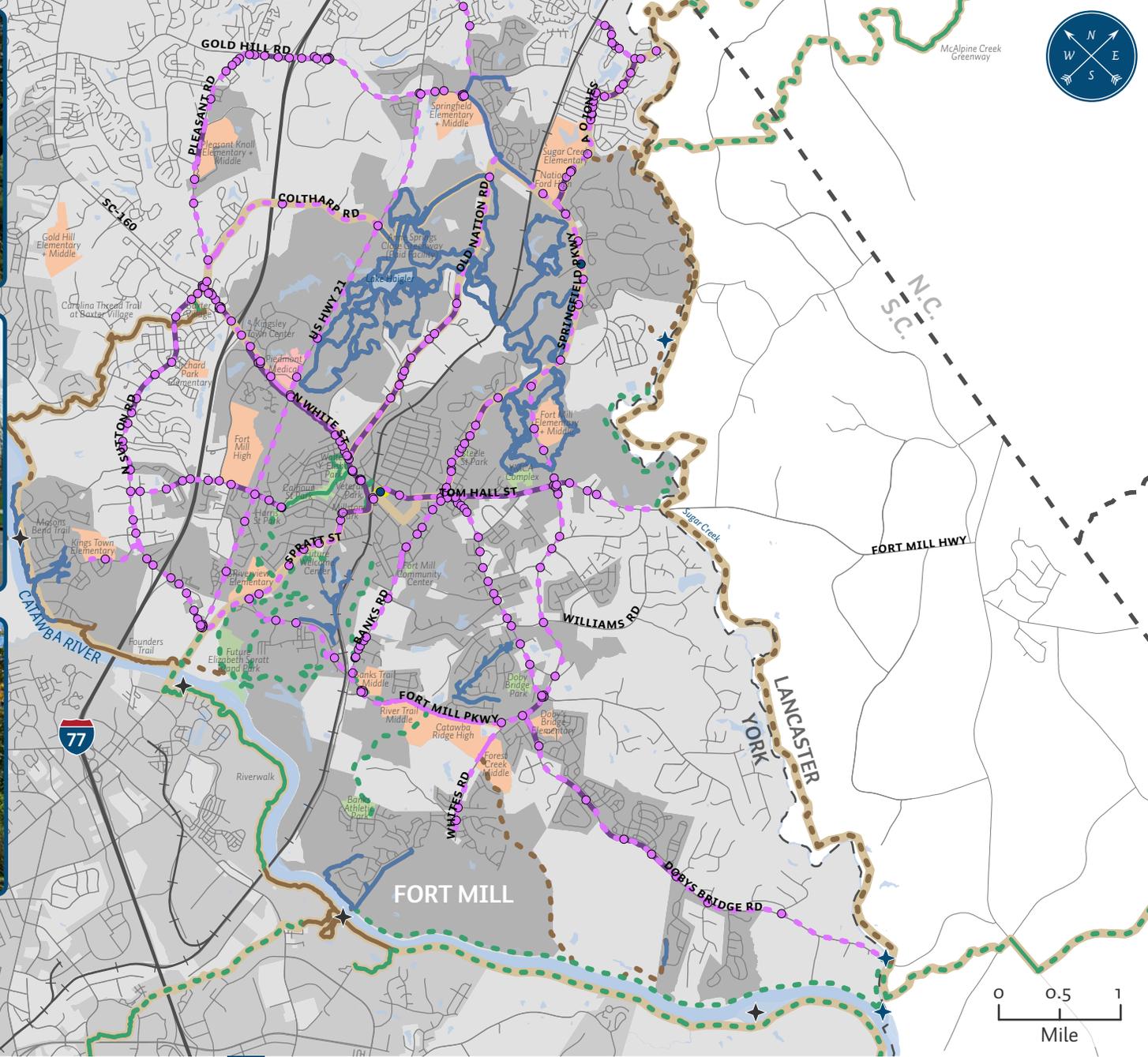
Example: High-Visibility Crosswalk



Example: Rectangular Rapid Flashing Beacon (RRFB)



Example: Existing Blueway Access at the Riverwalk (Rock Hill)



# FORT MILL TRAIL MASTER PLAN

Proposed Trail Network with Crossings + Blueways

## LEGEND

- ◆ Proposed Blueway Access
- ◆ Existing Blueway Access
- Proposed High-Visibility Crosswalk
- Proposed Mid-Block Crossing (RRFB)
- Road
- Railroad
- Private Facility
- Existing Sidewalk
- Potential CTT

- MULTI-USE PATH/TRAIL STATUS**
- Complete Paved Trail
  - Proposed Paved Trail
  - Complete Natural Surface Trail
  - Proposed Natural Surface Trail
  - Complete Sidepath
  - Proposed Sidepath

- Fort Mill School
- Fort Mill Park
- Hospital
- Downtown/Main St
- Water
- Fort Mill
- Other Jurisdiction
- York County
- County Boundary

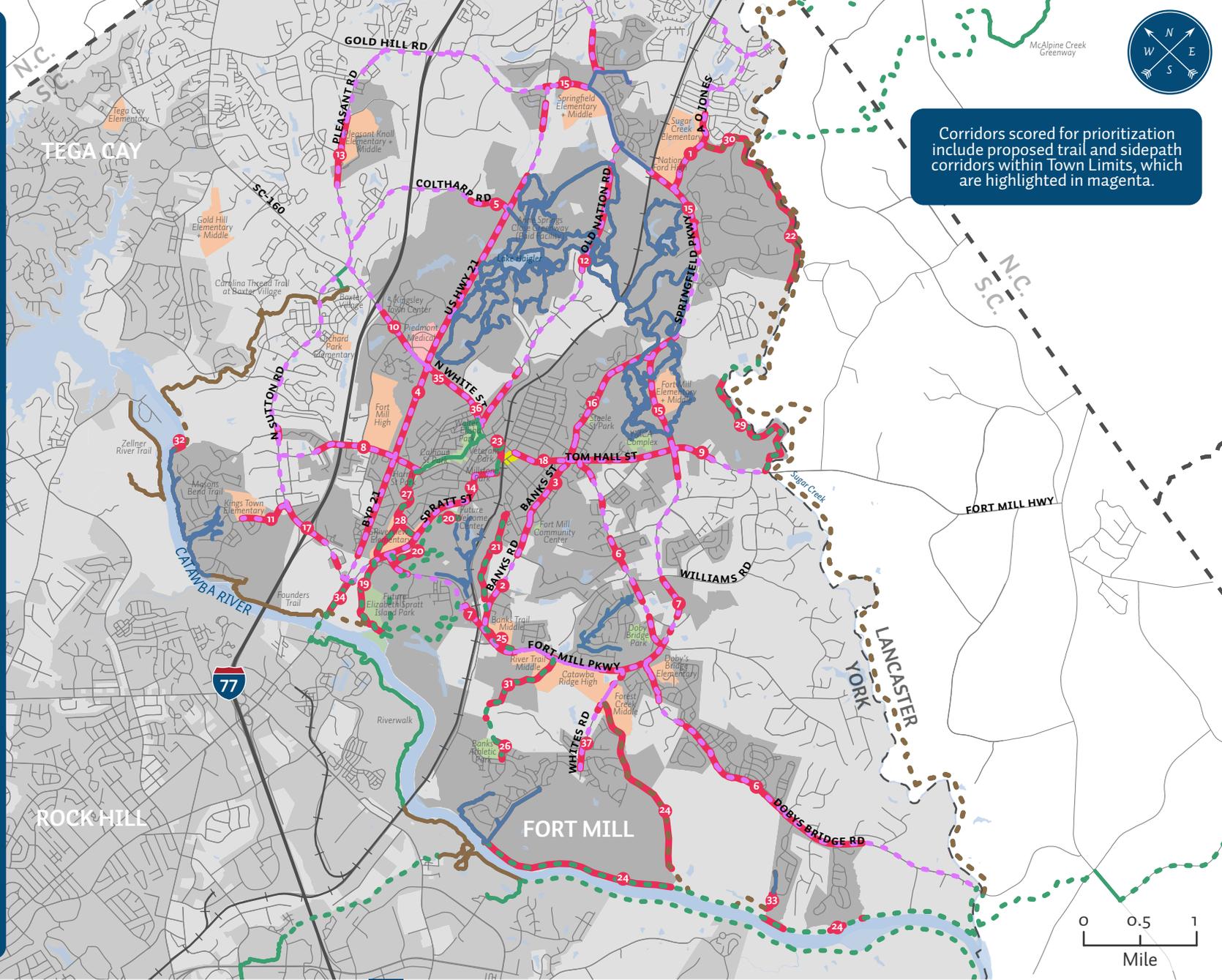
## PRIORITIZATION

Following the network recommendation phase, the project team reviewed each proposed project and scored them according to a set of four criteria. Based on guidance provided by Town staff, the project team scored proposed trail corridors that were within Town limits. Prioritization criteria were selected based upon both Town staff and Steering Committee feedback and emphasized improving trail connectivity between parks, schools, and activity centers within the Town. Community needs for specific projects were also identified and scored during this process. Based on guidance provided by Town staff, the project team scored proposed trail corridors that were within Town limits. Additional information on the criteria is included in the table below.

#	CRITERIA	DESCRIPTION	RANK	MEASUREMENT	POINTS
1	Community Needs	Trail corridor addresses the community needs or concerns for improved trail facilities.	High	Town received 5+ public comments identifying needs or concerns along the corridor.	10
			Medium	Town received 1-4 public comments identifying needs or concerns along the corridor.	5
			Low	Town received 0 public comments identifying needs or concerns along the corridor.	0
2	Corridor Connections: Schools	Trail corridor provides connections to schools.	High	Corridor is within ¼ mile of a school.	10
			Medium	Corridor is within ½ mile of a school.	5
			Low	Corridor is not near a school.	0
3	Corridor Connections: Parks	Trail corridor provides connections to parks (i.e., Town parks, Fort Mill Aquatic Center/YMCA, Fort Mill Community Center, and Anne Springs Close Greenway recreational areas).	High	Corridor is within ¼ mile of a park or recreation area.	10
			Medium	Corridor is within ½ mile of a park or recreation area.	5
			Low	Corridor is not near a park or recreation area.	0
4	Corridor Connections: Activity Centers	Trail corridor provides connections to activity centers (i.e., Downtown, Kingsley Town Center, Springfield Town Center, and Future Harris Teeter Shopping Center).	High	Corridor is within ¼ mile of a major activity center.	10
			Medium	Corridor is within ½ mile of a major activity center.	5
			Low	Corridor is not near a major activity center.	0

**Trail Corridors Scored for Prioritization:**

1. A O Jones Blvd Sidepath
2. Banks Rd Sidepath
3. Banks St Sidepath
4. Byp + Hwy 21 Sidepath
5. Coltharp Rd Sidepath
6. Dobys Bridge Rd Sidepath
7. Fort Mill Pkwy Sidepath
8. Harris Rd Sidepath
9. Hwy 160 Sidepath
10. Hwy 160 W Sidepath
11. Masons Bend Dr Sidepath
12. Old Nation Rd Sidepath
13. Pleasant Rd Sidepath
14. Spratt St Sidepath
15. Springfield Pkwy Sidepath
16. Steele St Sidepath
17. Sutton Rd S Sidepath
18. Tom Hall St Sidepath
19. Trail A
20. Trail B
21. Trail C
22. Trail D
23. Trail E
24. Trail F
25. Trail G
26. Trail H
27. Trail I
28. Trail J
29. Trail K
30. Trail L
31. Trail M
32. Trail N
33. Trail O
34. Trail P
35. White St Sidepath
36. White St N Sidepath
37. Whites Rd Sidepath



Corridors scored for prioritization include proposed trail and sidepath corridors within Town Limits, which are highlighted in magenta.

# FORT MILL TRAIL MASTER PLAN

## Proposed Trail Network with Highlighted Trail Corridors Scored for Prioritization

**LEGEND**

- Road
- ⊕ Railroad
- Private Facility
- Trail Corridors Scored for Prioritization
- Complete Paved Trail
- ⋯ Proposed Paved Trail
- Complete Natural Surface Trail
- ⋯ Proposed Natural Surface Trail
- Complete Sidepath
- ⋯ Proposed Sidepath
- Fort Mill School
- Fort Mill Park
- Hospital
- Downtown/Main St
- Water
- Fort Mill
- Other Jurisdiction
- York County
- County Boundary

The following table includes the complete list of sidepath and paved trail projects recommended in this plan. The table summarizes the project recommendations, overall prioritization score, and rankings.

RANK	PROJECT DESCRIPTION	RECOMMENDATION	TOTAL SCORE	PROJECT MAP ID#
1 (TIE)	Byp + Hwy 21	Sidepath	40	4
1 (TIE)	Dobys Bridge Rd	Sidepath	40	6
1 (TIE)	Fort Mill Pkwy	Sidepath	40	7
1 (TIE)	Spratt St	Sidepath	40	14
1 (TIE)	Springfield Pkwy	Sidepath	40	15
2 (TIE)	Hwy 160 W	Sidepath	35	10
2 (TIE)	Old Nation Rd	Sidepath	35	12
2 (TIE)	Tom Hall St	Sidepath	35	18
2 (TIE)	Whites Rd	Sidepath	35	37
3 (TIE)	Banks Rd	Sidepath	30	2
3 (TIE)	Steele St	Sidepath	30	16
3 (TIE)	Sutton Rd S	Sidepath	30	17
3 (TIE)	Trail B (Future Park Trail)	Paved Trail	30	20
3 (TIE)	Trail E (Farmers Market Connector)	Paved Trail	30	23
3 (TIE)	Trail I (Harris St Park Trail)	Paved Trail	30	27
3 (TIE)	Trail M (Banks Athletic Park to Catawba Ridge - River Trail Schools Connector)	Paved Trail	30	31
3 (TIE)	White St	Sidepath	30	35
4 (TIE)	A O Jones Blvd	Sidepath	25	1

RANK	PROJECT DESCRIPTION	RECOMMENDATION	TOTAL SCORE	PROJECT MAP ID#
4 (TIE)	Banks St	Sidepath	25	3
4 (TIE)	Harris Rd	Sidepath	25	8
4 (TIE)	Hwy 160	Sidepath	25	9
4 (TIE)	Trail F (Catawba River Trail)	Paved Trail	25	24
4 (TIE)	Trail K (Sugar Creek Corridor)	Paved Trail	25	29
4 (TIE)	Trail P (Bye 21 Trail - Connector to Rock Hill Riverwalk)	Paved Trail	25	34
4 (TIE)	White St N	Sidepath	25	36
5 (TIE)	Coltharp Rd	Sidepath	20	5
5 (TIE)	Masons Bend Dr	Sidepath	20	11
5 (TIE)	Pleasant Rd	Sidepath	20	13
5 (TIE)	Trail G (Banks School Connector)	Paved Trail	20	25
5 (TIE)	Trail H (Banks St Athletic Park Connector)	Paved Trail	20	26
5 (TIE)	Trail J (Riverview Elementary School Connector)	Paved Trail	20	28
5 (TIE)	Trail L (Old Nation Ford Greenway)	Paved Trail	20	30
6 (TIE)	Trail C (South of Downtown Trail)	Paved Trail	15	21
7 (TIE)	Trail A (Future Park Trail Connector)	Paved Trail	10	19
7 (TIE)	Trail N (Mason's Bend CTT Gap)	Paved Trail	10	32
8	Trail D (Foxwood Trail)	Paved Trail	5	22
9	Trail O (Massey Trail Connector)	Paved Trail	0	33

## PRIORITY PROJECTS

From the list of scored projects, four were selected by Town staff for further study within this plan:

- ▶ Harris Road/Street Sidepath
- ▶ Harris Street Park Trail
- ▶ Old Nation Road Sidepath
- ▶ Tom Hall Street Sidepath

The four priority projects are highlighted in the map to the right and fast facts are provided below. The following pages contain cut sheets that provide a summary of existing conditions, project snapshot information, planning level cost estimates, and prioritization scores for each project. Cut sheets also include a project map with detailed design considerations along the project corridors.



### Harris Road/ Street Sidepath

**Location:** Harris Rd/Harris St between N Sutton Rd and Yorktowne St

**Total Length:** 1.36 miles

**Facility Type:** Sidepath



### Harris Street Park Trail

**Location:** Harris St Park at Harris St south to Riverview Elementary School

**Total Length:** 0.58 miles

**Facility Type:** Paved Trail



### Old Nation Road Sidepath

**Location:** Old Nation Rd between Springfield Pkwy and N White St

**Total Length:** 2.88 miles

**Facility Type:** Sidepath



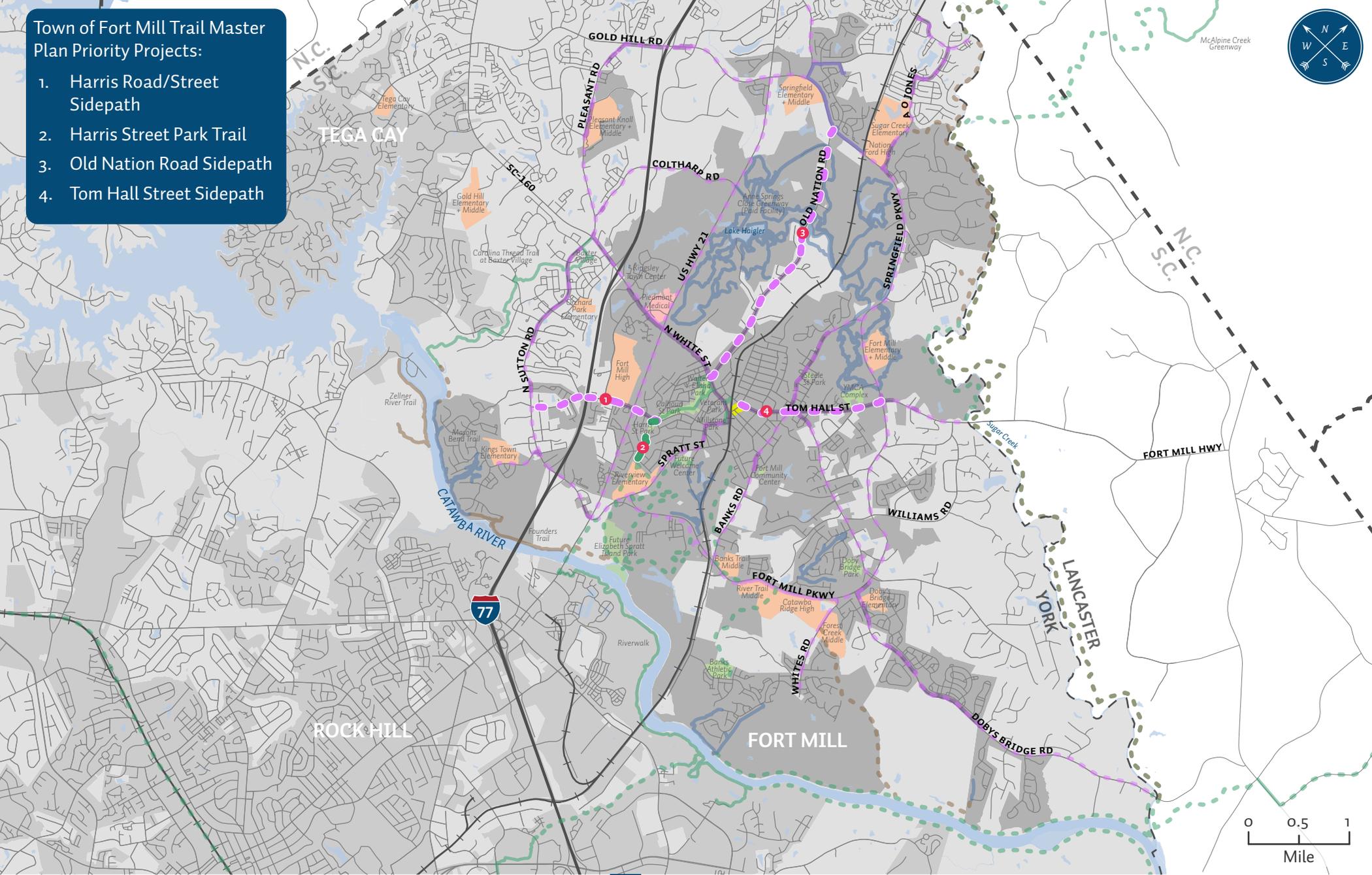
### Tom Hall Street Sidepath

**Location:** Tom Hall St between Spings St and Springfield Pkwy

**Total Length:** 1.45 miles

**Facility Type:** Sidepath

- Town of Fort Mill Trail Master Plan Priority Projects:**
1. Harris Road/Street Sidewalk
  2. Harris Street Park Trail
  3. Old Nation Road Sidewalk
  4. Tom Hall Street Sidewalk

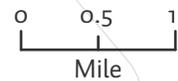


# FORT MILL TRAIL MASTER PLAN

## Priority Projects

**LEGEND**

- ★ Priority Project
  - Road
  - Railroad
  - Private Facility
  - Existing Sidewalk
- MULTI-USE PATH/TRAIL STATUS**
- Complete Paved Trail
  - Proposed Paved Trail
  - Complete Natural Surface Trail
  - Proposed Natural Surface Trail
  - Complete Sidewalk
  - Proposed Sidewalk
- Fort Mill School
  - Fort Mill Park
  - Hospital
  - Downtown/Main St
  - Water
  - Fort Mill
  - Other Jurisdiction
  - York County
  - County Boundary



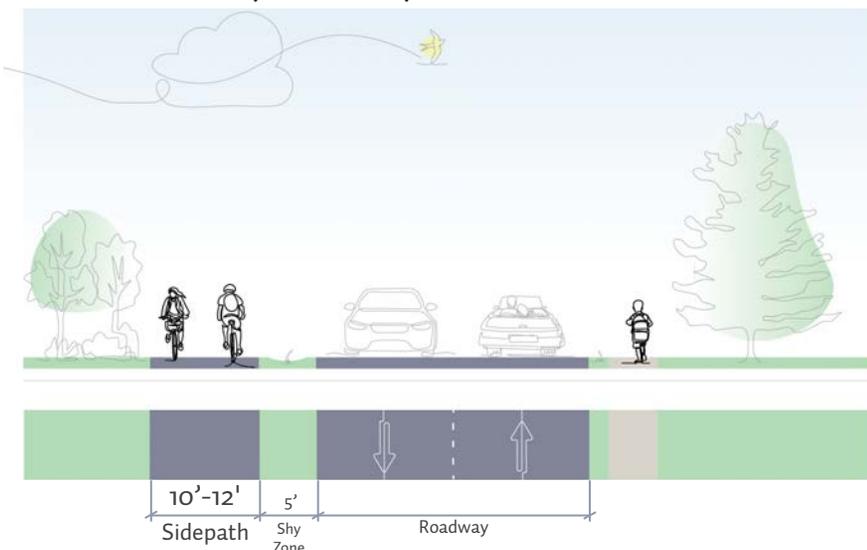
# Harris Road/Street Sidepath

This project runs along Harris Rd and Harris St and contains segments of existing sidewalk on the northern side. A new sidepath is proposed on the north side of this corridor and will connect N Sutton Rd to Fort Mill High School, Harris Street Park, and Calhoun Street Park. The project will intersect Bypass 21 which contains an existing crosswalk on the north side, as well as I-77, which Harris Rd runs underneath.

## EXISTING CONDITIONS



## TYPICAL SECTION (PROPOSED)



## PROJECT SNAPSHOT

**Location:** Harris Rd/Harris St between N Sutton Rd and Yorktowne St

**Total Length:** 1.36 miles

**Facility Type:** Sidepath (Asphalt)

### Crossing Treatments:

- ▶ 10 high-visibility crosswalks along Harris Rd/St
- ▶ 1 RRFB at Harris St Park

### Implementation Partners:

Town of Fort Mill, RFATS, SCDOT District 4, and York County

### Planning Level Cost Estimate(s):

Asphalt	Concrete
\$2,347,000	\$2,813,000

The planning level cost to construct the facility is provided in 2023 dollars and includes a 40% contingency. Facility cost estimates were based on the following factors: project type, project length, facility width, construction market region, and existing roadway characteristics. The estimate does not include costs of right-of-way acquisition and design.

## PRIORITIZATION RANKING

Community Needs: 5  
 Corridor Connections (Schools): 10  
 Corridor Connections (Parks): 10  
 Corridor Connections (Activity Centers): 0

**Total: 25/40**



## DESIGN CONSIDERATIONS

Ensure safe transitions for bicyclists and pedestrians from the proposed Harris Rd/Harris St sidepath to the proposed sidepaths along N Sutton Rd and Hwy-21.

Right-of-way constraints due to existing parking and the retaining wall in front of Riverview Presbyterian Church may pose a challenge to sidepath development in this area.

The right-of-way along the Harris Rd underpass as the roadway crosses I-77 is constrained, which may pose a challenge to sidepath development in this area.

Ensure safe transitions for bicyclists and pedestrians from the proposed Harris Rd/Harris St sidepath to Fort Mill High School.

Ensure safe transitions for bicyclists and pedestrians from the proposed Harris St sidepath to the proposed Harris St Park Trail. Install a Rectangular Rapid Flash Beacon (RRFB) and high-visibility crosswalk (ladder, zebra, or continental markings) at this mid-block crossing.

Environmental constraints along the corridor include the crossings of Jones Branch (between Sam Smith Rd and I-77) and Dye Branch (near Harris St Park). Both may pose a challenge to sidepath development.

Mitigate safety conflicts at street intersections and driveways along Harris Rd/Harris St at Walden Park Dr, Hwy-21, Munn Rd E, Quincy St, Sam Smith Rd, Spring Blossom Trail, Kidd Rd, 1st St N, and Sutton Rd by incorporating the following design strategies for sidepath crossings:

- ▶ Where possible reduce driveway frequencies and widths, especially in front of commercial establishments near the 1400 block of Harris Rd.
- ▶ Design or improve existing intersections to reduce driver speeds and heighten awareness of sidepath users.
- ▶ Encourage lower speeds at sidepath approaches by use of signage and traffic calming measures.
- ▶ Provide clear assignment of right-of-way with signs and markings. Consider using an R10-15 Right Turn Yield to Pedestrians sign at street crossings with right turn interactions.

Existing utilities along Harris Rd/Harris St may need to be relocated to accommodate the sidepath in several areas along the corridor.

Existing sidewalks along Harris Rd/Harris St are proposed to be widened to a sidepath from the Fort Mill High School entrance to Harris St Park and from Sam Smith Rd to Quail Ridge Rd.



# FORT MILL TRAIL MASTER PLAN

## Priority Project #1: Harris Road/Street Sidepath

### LEGEND

- Road
- ++ Railroad
- Existing Sidewalk
- Sidepath Priority Project

- #### MULTI-USE PATH/TRAIL STATUS
- Complete Paved Trail
  - Priority Project Sidepath
  - Proposed Sidepath

- Fort Mill School
- Fort Mill Park
- Water
- Fort Mill
- York County
- County Boundary

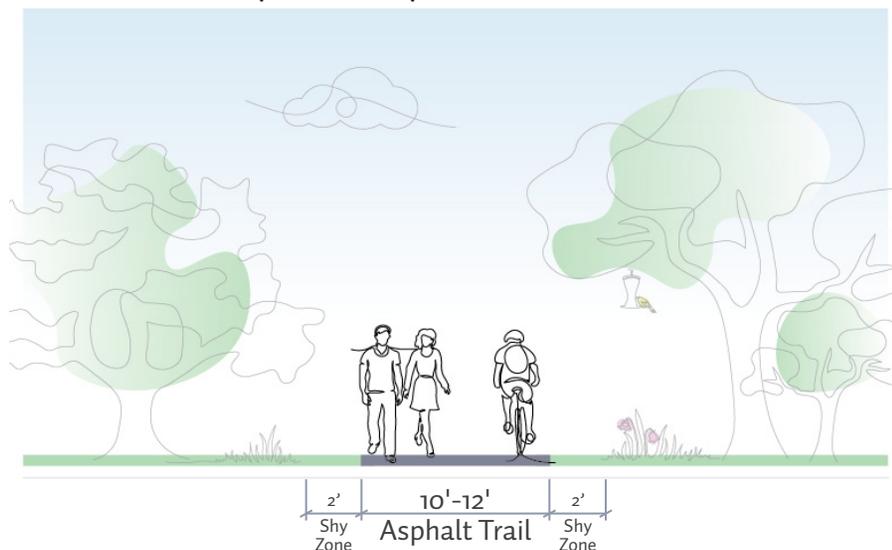
# Harris Street Park Trail

The proposed trail will run through Harris Street Park and over Dye Branch until it reaches the northern point of the Riverview Elementary School parcel. This greenway will support a recreational link between Harris Street Park, surrounding residential areas, and Riverview Elementary School. The Town of Fort Mill owns the parcels that the proposed trail segment runs through which will help streamline project implementation.

## EXISTING CONDITIONS



## TYPICAL SECTION (PROPOSED)



## PROJECT SNAPSHOT

**Location:** Harris St Park at Harris St south to Riverview Elementary School

**Total Length:** 0.58 miles

**Facility Type:** Paved Trail (Asphalt)

**Crossing Treatments:** 1 RRFB at Harris Street Park

**Implementation Partners:** Town of Fort Mill, RFATS, and SCDOT District 4

## Planning Level Cost Estimate:

Asphalt	
\$1,814,000	
Two proposed trail connectors would tie the Harris St Park Trail to Spratt St. Estimates for asphalt pavement are provided below.	
Connector 1	Connector 2
\$508,000	\$2,761,000

The planning level cost to construct the facility is provided in 2023 dollars and includes a 40% contingency. Facility cost estimates were based on the following factors: project type, project length, facility width, construction market region, and existing roadway characteristics. The estimate does not include costs of right-of-way acquisition and design.

## PRIORITIZATION RANKING

Community Needs: 10  
 Corridor Connections (Schools): 10  
 Corridor Connections (Parks): 10  
 Corridor Connections (Activity Centers): 0

**Total: 30/40**



## DESIGN CONSIDERATIONS

At the entrance of Harris St Park, existing signage and landscaping may need to be relocated or removed to accommodate the proposed trail. In addition, existing drainage, utilities, and the Dye Branch pose constraints to the trail alignment in this area. Consider utilizing the existing sidewalk corridor and route along the edge of the parking lot to avoid drainage and utility constraints.

Right-of-way constraints between the existing baseball field and Dye Branch may pose a challenge to trail development in this area. An alignment east of the baseball field may pose similar challenges due to existing utilities and topography constraints. The Town of Fort Mill owns the adjacent parcel west of the park and could consider crossing Dye Branch and routing west of the stream in the area. However, this alternative alignment may require additional stream crossings and increase project costs.

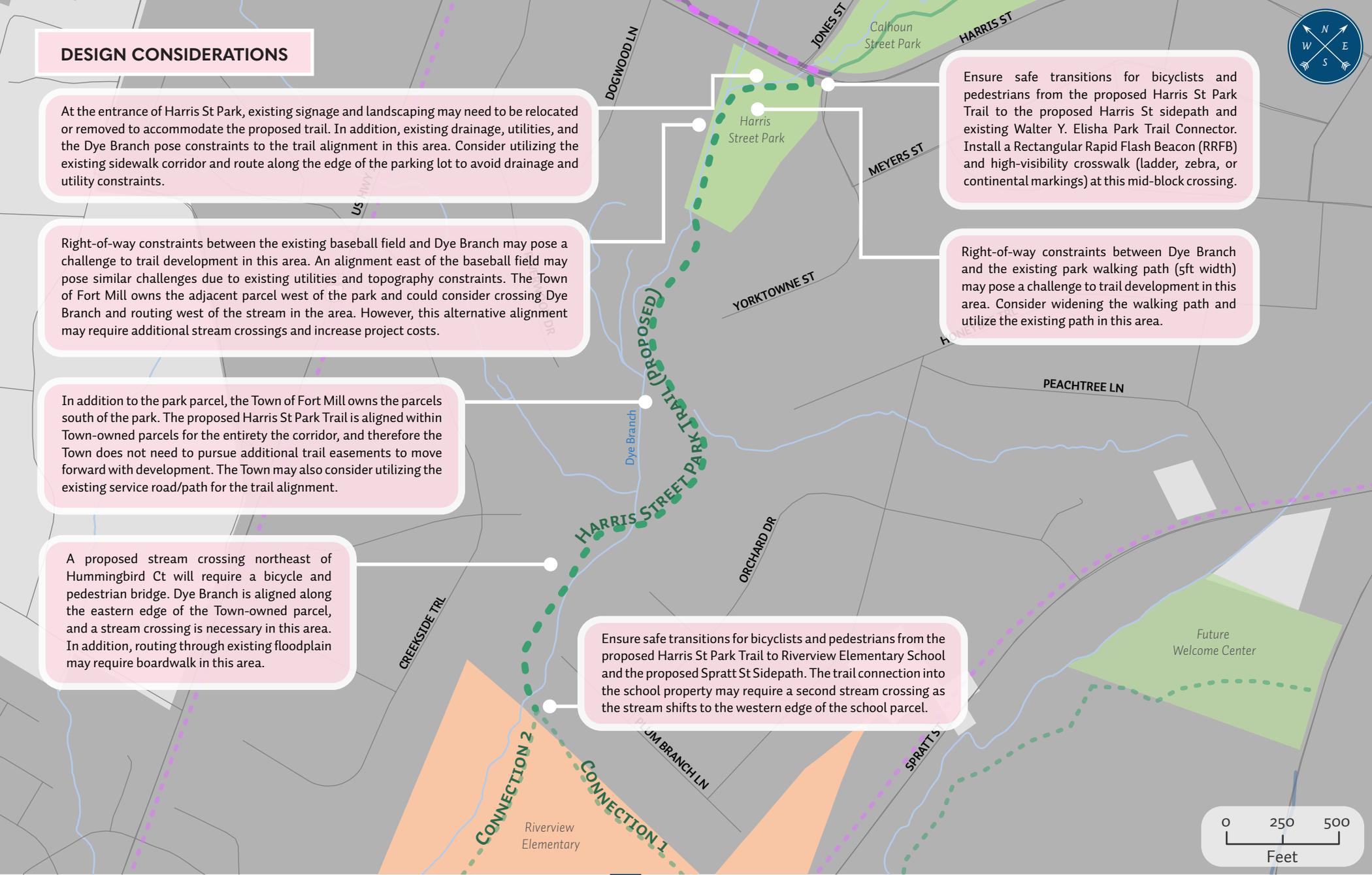
In addition to the park parcel, the Town of Fort Mill owns the parcels south of the park. The proposed Harris St Park Trail is aligned within Town-owned parcels for the entirety the corridor, and therefore the Town does not need to pursue additional trail easements to move forward with development. The Town may also consider utilizing the existing service road/path for the trail alignment.

A proposed stream crossing northeast of Hummingbird Ct will require a bicycle and pedestrian bridge. Dye Branch is aligned along the eastern edge of the Town-owned parcel, and a stream crossing is necessary in this area. In addition, routing through existing floodplain may require boardwalk in this area.

Ensure safe transitions for bicyclists and pedestrians from the proposed Harris St Park Trail to Riverview Elementary School and the proposed Spratt St Sidepath. The trail connection into the school property may require a second stream crossing as the stream shifts to the western edge of the school parcel.

Ensure safe transitions for bicyclists and pedestrians from the proposed Harris St Park Trail to the proposed Harris St sidepath and existing Walter Y. Elisha Park Trail Connector. Install a Rectangular Rapid Flash Beacon (RRFB) and high-visibility crosswalk (ladder, zebra, or continental markings) at this mid-block crossing.

Right-of-way constraints between Dye Branch and the existing park walking path (5ft width) may pose a challenge to trail development in this area. Consider widening the walking path and utilize the existing path in this area.



# FORT MILL TRAIL MASTER PLAN

## Priority Project #2: Harris Street Park Trail

### LEGEND

- Road
- ++ Railroad
- Paved Trail Priority Project
- Complete Paved Trail
- - - Proposed Paved Trail
- Complete Sidepath
- - - Proposed Sidepath
- Fort Mill School
- Fort Mill Park
- Water
- Fort Mill
- York County
- County Boundary

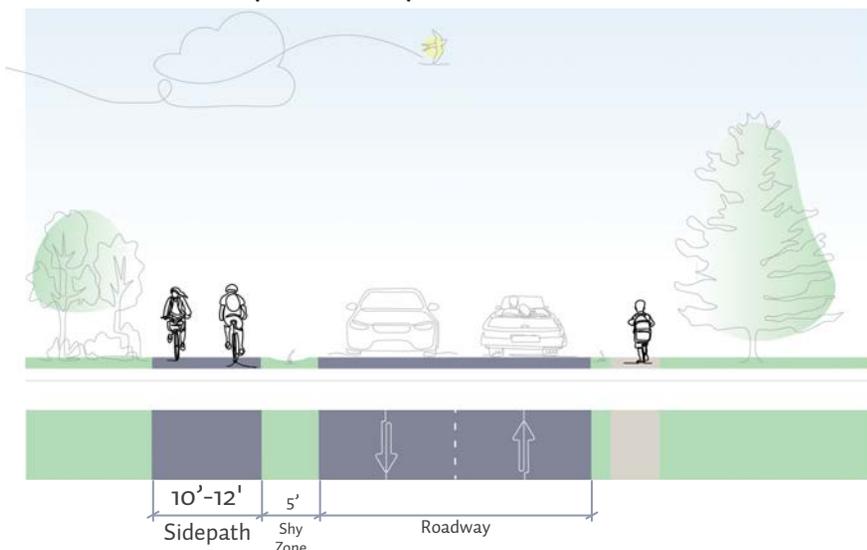
# Old Nation Road Sidepath

Old Nation Rd serves as a north-south transportation corridor within the Town. This road is also part of the potential Carolina Thread Trail corridor. While the road segment already contains existing sidewalk, the Town plans to prioritize and install a new sidepath on the west side of the road to link Nation Ford High School, Anne Springs Close Greenway, and Walter Y. Elisha Park.

## EXISTING CONDITIONS



## TYPICAL SECTION (PROPOSED)



## PROJECT SNAPSHOT

**Location:** Old Nation Rd between Springfield Pkwy and N White St

**Total Length:**  
2.88 miles

**Facility Type:**  
Sidepath (Asphalt)

**Crossing Treatments:**  
9 high-visibility crosswalks along Old Nation Rd

**Implementation Partners:**  
Town of Fort Mill, RFATS, SCDOT District 4, Carolina Thread Trail, and York County

The planning level cost to construct the facility is provided in 2023 dollars and includes a 40% contingency. Facility cost estimates were based on the following factors: project type, project length, facility width, construction market region, and existing roadway characteristics. The estimate does not include costs of right-of-way acquisition and design.

## PRIORITIZATION RANKING

- Community Needs: 10
- Corridor Connections (Schools): 10
- Corridor Connections (Parks): 10
- Corridor Connections (Activity Centers): 5

**Total: 35/40**

## Planning Level Cost Estimate(s):

Asphalt	Concrete
\$4,602,000	\$4,972,000

## DESIGN CONSIDERATIONS

Mitigate safety conflicts at street intersections and driveways along Old Nation Rd at N. White St, Watford St, Walker St, Cantrell Ave, Wayside Dr, Elm St, Ritch St, Dairy Barn Ln, and Springfield Pkwy (Hwy 460) by incorporating the following design strategies for sidepath crossings:

- ▶ Where possible reduce driveway frequencies and provide clear signage and markings, especially along the residential area of Old Nation Rd between the 1100 block and 1600 block.
- ▶ Design or improve existing intersections to reduce driver speeds and heighten awareness of sidepath users.
- ▶ Encourage lower speeds at sidepath approaches by use of signage and traffic calming measures.
- ▶ Provide clear assignment of right-of-way with signs and markings. Consider using an R10-15 Right Turn Yield to Pedestrians sign at street crossings with right turn interactions.

Existing sidewalks along Old Nation Rd may be widened to accommodate the proposed sidepath. The sidewalk on the westside of Old Nation Rd span from N. White St to the 150 block and along the eastside from N. White St to the 1600 block. Consider a routing alternative along the eastside of Old Nation Rd to reduce potential conflicts with existing utilities and drainage in this area.

Ensure safe transitions for bicyclists and pedestrians from the proposed Old Nation Rd sidepath to the proposed sidepaths along N. White St and Springfield Pkwy and to Walter Y. Elisha Park and existing park paths.

Ensure safe transitions for bicyclists and pedestrians from the proposed Old Nation Rd sidepath to the Anne Springs Close Greenway (Dairy Barn Ln Entrance).

The existing bridge over the stream may require retrofitting the bridge to accommodate the sidepath or a bicycle and pedestrian bridge adjacent to the existing bridge.

Right-of-way constraints, such as heavy foliage and trees along the westside of Old Nation Rd from the 190 to 1100 block and 1900 block may pose challenges for sidepath development. Existing drainage and utilities along Old Nation Rd at the 1100 block, 1300 block, 1600 block, 1900 block, and 2200 block may also pose challenges to sidepath development.

# FORT MILL TRAIL MASTER PLAN

## Priority Project #3: Old Nation Road Sidepath

### LEGEND

- Road
- ++ Railroad
- Private Facility
- Existing Sidewalk
- Sidepath Priority Project

- #### MULTI-USE PATH/TRAIL STATUS
- Complete Paved Trail
  - - - Proposed Paved Trail
  - Complete Natural Surface Trail
  - - - Proposed Natural Surface Trail
  - Complete Sidepath
  - - - Proposed Sidepath

- Fort Mill School
- Fort Mill Park
- Hospital
- Water
- Fort Mill
- York County
- County Boundary



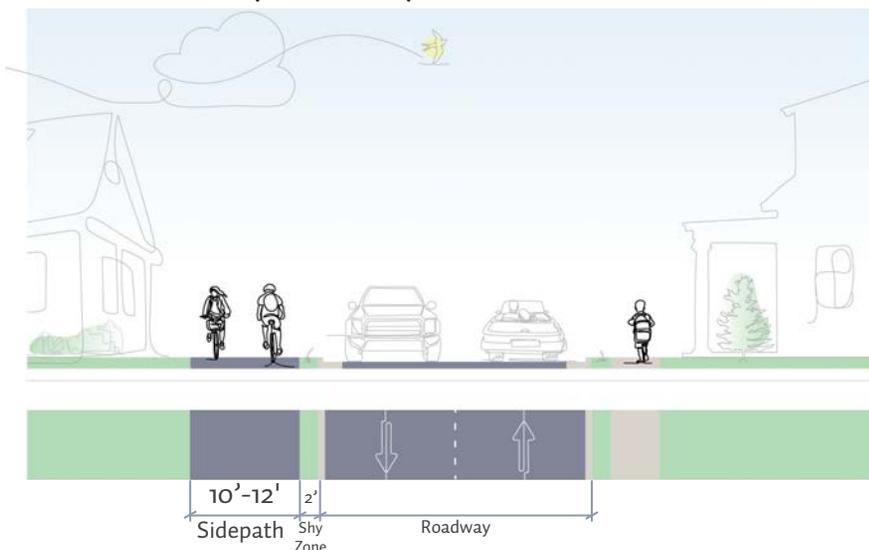
# Tom Hall Street Sidepath

Tom Hall St serves as the major east-west transportation corridor in Downtown Fort Mill. It links Downtown Fort Mill along Main St to several local businesses, churches, Town Hall, and the YMCA Complex. The project along this segment proposes to replace existing sidewalk on the northern side of the road with sidepath.

## EXISTING CONDITIONS



## TYPICAL SECTION (PROPOSED)



## PROJECT SNAPSHOT

**Location:** Tom Hall St between Springs St and Springfield Pkwy

**Total Length:**  
1.45 miles

**Facility Type:**  
Sidepath (Asphalt)

**Crossing Treatments:**  
7 high-visibility crosswalks along Tom Hall St

**Implementation Partners:**  
Town of Fort Mill, RFATS, Carolina Thread Trail, and SCDOT District 4

## Planning Level Cost Estimate(s):

Asphalt	Concrete
\$2,379,000	\$2,933,700

The planning level cost to construct the facility is provided in 2023 dollars and includes a 40% contingency. Facility cost estimates were based on the following factors: project type, project length, facility width, construction market region, and existing roadway characteristics. The estimate does not include costs of right-of-way acquisition and design.

## PRIORITIZATION RANKING

- Community Needs: 10
- Corridor Connections (Schools): 5
- Corridor Connections (Parks): 10
- Corridor Connections (Activity Centers): 10

**Total: 35/40**

## DESIGN CONSIDERATIONS

Ensure safe transitions for bicyclists and pedestrians from the proposed Tom Hall sidepath to Downtown; the proposed sidepaths along Steele St, Banks St, Dobys Bridge Rd, Springfield Pkwy, and Fort Mill Pkwy; and to the YMCA Complex.

Existing landscaping, brick bollards, and a brick retaining wall that line the streetscape in front of Hardee's may need to be relocated or removed to accommodate the proposed sidepath in this area.

Right-of-way constraints, narrow building setbacks, and utilities in from of 1873 Brewing, the adjacent shopping center, and homes between Steele St and Joe Louis St may pose a challenge to sidepath development in this area.

Existing drainage, utilities, landscaping, brick retaining walls, and established trees may pose a challenge to sidepath development along Tom Hall St in front of the Bank of America, Post Office, Unity Presbyterian Church, Wells Fargo, and Reach for the Stars Learning Center.

Mitigate safety conflicts at street intersections and driveways along Tom Hall St at Unity St, Morrow Bradford Dr, Steele St, Joe Louis St, Sanders St Ext, Bozeman Dr, Ashbrooke Dr, and Springfield Pkwy/Fort Mill Pkwy by incorporating the following design strategies for sidepath crossings:

- ▶ Where possible reduce driveway frequencies and widths, especially in front of commercial establishments along Tom Hall St.
- ▶ Design or improve existing intersections to reduce driver speeds and heighten awareness of sidepath users.
- ▶ Encourage lower speeds at sidepath approaches by use of signage and traffic calming measures.
- ▶ Provide clear assignment of right-of-way with signs and markings. Consider using an R10-15 Right Turn Yield to Pedestrians sign at street crossings with right turn interactions.

Minor topography constraints south of Sanders St and north of Bozeman Dr, which include existing slopes, brick retaining wall, and guard rails may pose a challenge to sidepath development in these areas.

An existing school bus shelter, utilities, and landscaping are present near the intersection of Bozeman Dr. The proposed sidepath may need to be routed west of the bus shelter to mitigate conflicts with path users and students utilizing the shelter.

# FORT MILL TRAIL MASTER PLAN

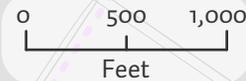
## Priority Project #4: Tom Hall Street Sidepath

### LEGEND

- Road
- ++ Railroad
- Private Facility
- Existing Sidewalk
- Sidepath Priority Project

- MULTI-USE PATH/TRAIL STATUS**
- Complete Paved Trail
  - ... Proposed Paved Trail
  - Complete Sidepath
  - ... Proposed Sidepath

- Fort Mill School
- Fort Mill Park
- Downtown/Main St
- Water
- Fort Mill
- York County
- County Boundary



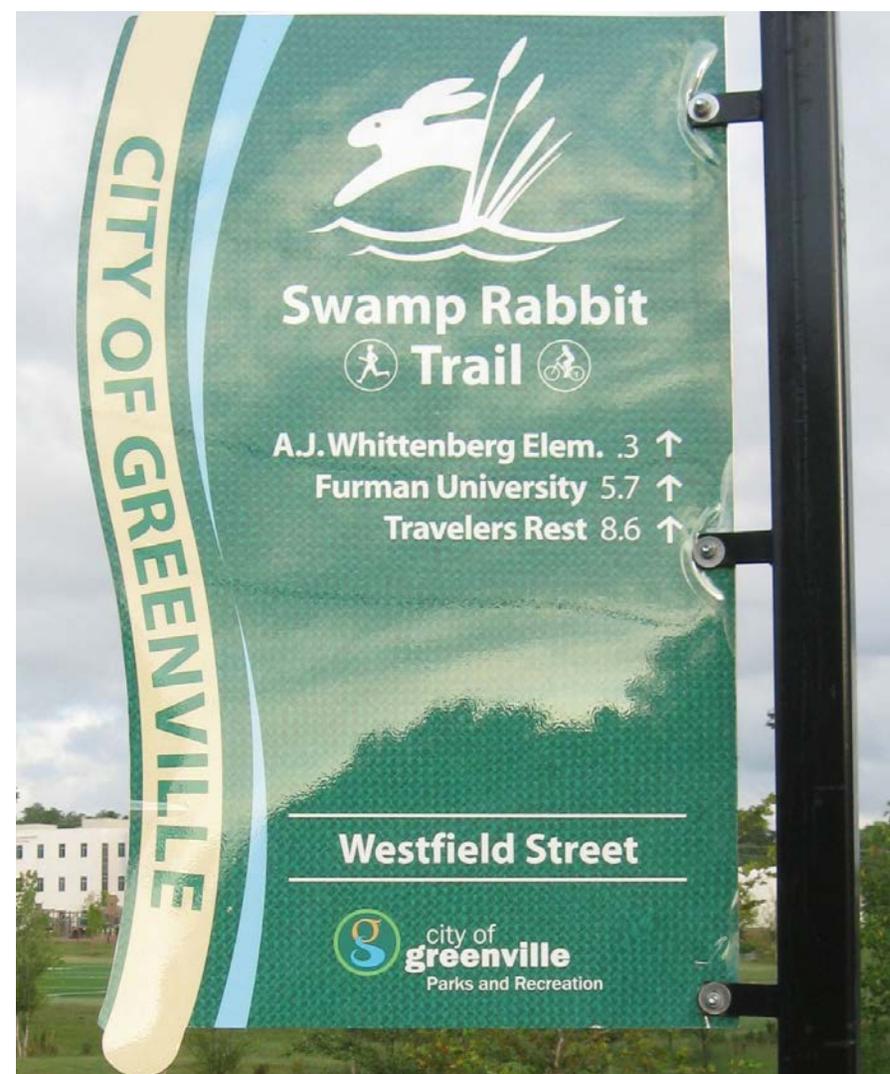
## Trail Amenities

In tandem with infrastructure and policy recommendations, trail amenities improve user safety and experience and enhance recreational and multi-modal accommodations in a community. A summary of recommended trail amenities are provided below and on subsequent pages.

### Wayfinding

Wayfinding consists of comprehensive signage, mapping, and marking systems that inform and educate users as they make their way to, from, and along a greenway or trail. A cohesive system across the network will enhance access, provide a greater sense of security and comfort, promote desired user behaviors, improve awareness of nearby trail networks and key destinations, and reinforce the brand and/or identity of the facility. The following principles should guide the implementation of the wayfinding system:

- ▶ **Consistency** - User experience should feel consistent and continuous across the entire network.
- ▶ **Connectivity** - A primary function of wayfinding is to connect users to destinations and other routes. It should clearly communicate current locations, access points, adjacent streets, distances, directions, destinations, estimated travel times, and historical/cultural/ environmental information where applicable.
- ▶ **Identity** - A strong wayfinding identity will make the network more recognizable and memorable to visitors and residents alike. Custom designs and graphics should be used to create a unique identity which reflects the goals of the Fort Mill community.
- ▶ **Predictability** - Apply wayfinding in a predictable manner (including sign placement, design, and content) to allow users to quickly understand the information being presented. For users, this builds trust, increases comfort, reduces stress, and provides a welcoming and low-stress experience as they navigate the trail network.
- ▶ **Simplicity** - Present information in a clear, logical, universal way to reach the widest possible demographic in a timely manner.



Swamp Rabbit Trail Wayfinding Signage - Greenville, SC  
Credit: John Foxe



Greenway Tunnel Lighting, Bolin Creek Greenway - Chapel Hill, NC

## Lighting

Well-placed and properly maintained lighting can improve visibility, increase overall trail network access, and give users a greater sense of security. The following guidance is recommended if lighting is implemented along Fort Mill's trail network:

- ▶ Meet the American Association of State Highway and Transportation Officials' (AASHTO) Guide for the Development of Bicycle Facilities requirements for shared-use paths.
- ▶ Light only what is needed along paved trails and comply with dark-sky requirements to help minimize light pollution, which impacts people, animals, and the environment.
- ▶ Be of appropriate scale and spacing to ensure adequate coverage.
- ▶ Be placed where recommended for safety at tunnels and overpasses; trailheads; bridges; gathering places; along streets; crosswalks; where a trail crosses another path or sidewalk; and on signage.

A variety of lighting types are available including wired, battery-powered, and solar-powered each of which offers unique advantages or disadvantages regarding cost, maintenance burden, and environmental impacts.

Use of colored and/or dynamic lighting schemes in select areas (such as tunnels and bridges) can enhance the user experience, contribute to the overall brand/identity of the trail network, and raise awareness of the facility to the traveling public.

## Trailheads

Trailheads provide public access points to trails and greenways. They are typically placed in locations where users begin or end their journeys and where they get oriented to the greenway or trail network. While there are minor access points along paved trails such as road crossings, within neighborhoods, or where two facilities intersect, a trailhead tends to be a developed site, purposefully designed to provide amenities to paved trail users such as parking, signage, information kiosks, restrooms, drinking fountains, bike racks, bike repair stations, seating, public art, landscaping, and trash receptacles.

Guidance for trailhead placement includes:

- ▶ Endpoints are natural places to locate trailheads, but any place where a large volume of users is expected should be considered as a possible trailhead location.
- ▶ Utilize areas where amenities already exist, such as parks.
- ▶ Consider placing a trailhead where paved trail facilities intersect.
- ▶ Consider placing a trailhead within residential neighborhoods. Trailheads that are located within neighborhoods should be designed to be compatible with their surrounding uses.
- ▶ Consult with the community and seek public input on locations. Residents may have insights or preferences for areas that best meet trail user needs.

At all paved trail access points, including trailheads, enhance user safety by implementing access management tools. Bollards, gates, fences, landscaping, and signage can prevent motorized vehicles from accessing the facility. These barriers should be accessible for persons with disabilities to ensure that users of all ages and abilities can access the facility. Barriers should also allow emergency or maintenance vehicles to access the greenway or trail.



**Carolina Thread Trail Trailhead**  
Credit: Gaston Outside



**Bicycle + Pedestrian Counter**  
Credit: Streetsblog USA

## Data Collection

Bicycle and pedestrian count data are an essential tool to justify investments in active transportation infrastructure (i.e., greenways and trails) and communicate needs with the public, elected officials, and other stakeholders. Collecting this data provides insights into temporal user volume trends (time of day and seasonal), user type trends (biking vs. walking), and user volume trends by geographic location (which sections are most frequently used). This information can also help identify potential areas of need as municipalities plan their future pedestrian and bicycling infrastructure projects.

A variety of counting technologies and products are available depending on the specific application and budget. These range from inductive loop detectors, pneumatic tube detectors, and passive infrared detectors among others.

Mobile counters provide the flexibility to collect data in one location before moving to another collection location and are typically battery-powered. Fixed counters are used at locations where long-term data collection is desired and may be wired or battery-powered. Some blend in with their surroundings and others utilize real-time display totems to present daily and yearly counts and engage directly with those users being counted.

Depending on the specific product, count data may be retrieved manually from the counter or may streamline the process via wireless transmission, reducing trips to the field. Online, easy-to-use data platforms are also offered to analyze and visualize the data. Features include dashboards and interfaces to provide access to count data for the development of custom websites and mobile applications. The emerging use of “Big Data” crowd-sourced from mobile phone users, via services such as Streetlight and Strava, may also be an option for collecting user count data.



 **DOWNTOWN**  
FORT MILL, SC



# 05

## POLICIES + PROGRAMS

### OVERVIEW

In addition to the recommended trail infrastructure improvements in this plan, the project team put together a comprehensive list of potential policies and programs for the Town to consider implementing alongside network recommendations. If implemented, the policies and programs recommended in this chapter would establish a structure to enhance trail development practices within the Town. Also, these recommendations may help to improve safety conditions associated with both natural surface and paved trails, as well as educate and encourage users on how to use the facilities safely.

The following sections are covered in this chapter:

- Policy Recommendations
- Program Recommendations
  - » Educational Programs
  - » Encouragement Programs
  - » Enforcement Programs
  - » Project Based Programs
  - » Evaluation Programs

## RECOMMENDED POLICIES

In tandem with program and infrastructure recommendations, trail-friendly policies improve bicyclist and pedestrian safety and enhance multi-modal accommodations in town ordinances and codes. The Town's zoning ordinance was reviewed to understand how it influences trail planning in the community. A summary of recommended policies for the Town of Fort Mill are provided in the table below.

POLICY RECOMMENDATION	DESCRIPTION
Establish dedicated funding in the Capital Improvements Plan (CIP) for trails and multi-use paths	The Town of Fort Mill should consider a dedicated funding allocation for trail and multi-use path improvement programs and facilities in the Town's budget to implement recommendations in this plan and to leverage other state and federal funding for these facilities.
Establish a trail facility maintenance policy and operations plan	The Town of Fort Mill should consider establishing a trail facility maintenance plan that includes debris and snow removal policies and schedules for trail clean-up coordination, routine and special maintenance of crossing signals and trail signage, and re-striping and resurfacing schedules for paved multi-use paths along Fort Mill-maintained roadways.
Reduce Town-wide speed limit to 25 mph	The Town of Fort Mill should consider lowering the Town-wide speed limit from 30 mph to 25 mph to improve the safety of bicyclists and pedestrians crossing roadways to connect to trail segments. Reducing the speed limit to 25 mph significantly lowers the risk of injury for bicyclists and pedestrians in crashes with motorists.
Create standard guidelines for trail design within the Town's policies and guidance documents	The Town of Fort Mill should consider creating a standard set of design standards for trail and multi-use path facilities to ensure consistent design within the community. At a minimum, facility design standards should include typical facility sections, minimum and preferred widths of facilities, intersection treatments, and connectivity requirements.
Developers should dedicate right-of-way and construction for trails within new developments and redevelopment projects	If trails or multi-use paths are included in a locally adopted plan, developers should be required to fund and construct the facility in part or full. Enforcing developer-built trails is one strategy that can be used to complete trail networks to supplement existing and pursued trail funding. The facilities should be included in submitted site plans during the development review process.
Use native plants in landscaping along trails or multi-use paths	Roadside swales placed between sidepaths and roads can be designed using native landscaping to control stormwater runoff instead of the installation of traditional curbs and gutters. Additionally, never introduce non-native or invasive species that may disrupt the ecosystem by taking over native vegetation.

POLICY RECOMMENDATION	DESCRIPTION
Establish a trail classification system and accompanying trail use policies	Establishing a trail classification system will help the Town identify appropriate trail uses. The Town should set overall goals and policies for balancing and accommodating different types of trail uses within the Town's network. The Anne Springs Close Greenway has established permitted uses for each of its trails including hiking, horse-back riding, and bicycling. The Town could coordinate with the Anne Springs Close Greenway to identify criteria for assigning trail uses.
Adopt a Complete Streets Ordinance and/or Program	<p>The Town of Fort Mill should adopt a municipal Complete Streets Policy, similar to SCDOT's statewide policy, to guide multi-modal improvements for the local street network. The City of North Myrtle Beach is an example of a municipality that has adopted a similar policy. The City incorporated the ordinance into their Land Development Regulations. All streets in the City must be designed and operated to enable safe access for all users. The document states that pedestrians, bicyclists, motorists, and transit riders of all ages and abilities must be able to safely move along and across a "complete street."</p> <p>For more information related to the municipal complete streets ordinance for North Myrtle Beach follow the link: <a href="https://scdhec.gov/sites/default/files/docs/HomeAndEnvironment/Docs/ModelOrdinances/Complete%20Streets.pdf">https://scdhec.gov/sites/default/files/docs/HomeAndEnvironment/Docs/ModelOrdinances/Complete%20Streets.pdf</a></p>
Encourage conservation / open space subdivision design within the Town	<p>Conservation subdivisions (CSDs) attempt to preserve undivided, buildable tracts of land as communal open space for residents (Arendt et al.). In this design, approximately 50 to 70 percent of the buildable land is set aside as open space by grouping homes on the developed portions of the land. These ordinances may be suitable for connecting trails and promoting trails of longer lengths, rather than short segments that lead to dead ends. The site design for these subdivisions can include walking trails to give residents public access to the trails. In some cases, communities offer density bonuses if public access is granted to a development's walking trails.</p> <p>Follow the link to learn more information on CSDs: <a href="https://www.ncufc.org/uploads/Conservation_subdivision.pdf">https://www.ncufc.org/uploads/Conservation_subdivision.pdf</a></p>
Minimize the number of cul-de-sacs constructed and promote trail connections at existing cul-de-sacs	<p>Cul-de-sacs limit walkability due to their lack of connectivity. Constructing roadway connections to existing cul-de-sacs is also challenging, therefore trail connections can help address limitations in terms of pedestrian connectivity and walkability.</p> <p>An adopted code may mandate that paths be placed between any proposed trail or recreational facility within "x" feet from any point on the cul-de-sac or when cul-de-sacs are proposed, the Town could mandate that a minimum of "x" feet wide pedestrian or bicyclist pathway that connects to an existing street, trail, recreational area, commercial area, or school must be included.</p>

POLICY RECOMMENDATION	DESCRIPTION
Encourage land and easement acquisition along trail corridors to ensure permanent use as trails	A trail easement is a type of easement that allows the public to use a piece of private property for recreational purposes. Access may be limited to specific users (i.e., hikers or mountain bikers) and should be determined at the time the easement is established. The easement stays with the land so if the original landowner sells the property, the trail remains permanently protected. Working with landowners may be essential to acquiring property and constructing a continuous and comprehensive trail network. Standard trail easement widths vary between 20-30 ft with 30 ft as the preferred width, 20 ft constrained.
Support trail development in tandem with the implementation of riparian buffer restoration and mitigation projects	Should riparian buffer restoration projects or mitigation projects arise within the Town, consideration should be given for trail or greenway access adjacent to these project types. Planning for these facilities at the same time reduces costs for the trail, enhances the environment, expands recreational opportunities in the area, and provides scenic views for trail users.
Adopt outdoor lighting ordinances or codes	<p>Sec. 6. - Lighting Standards mentions minimizing light pollution within the Town, but it does not state a formalized code or ordinance related to dark skies. For this reason, the Town should adopt outdoor lighting ordinances or codes that minimize costs, promote energy conservation, support safe trail systems and healthy habitat environments for threatened and endangered species within the Town. A Model Lighting Ordinance exists to address the need for strong, consistent outdoor lighting regulation in North America. Several municipalities throughout the nation have adopted similar codes to control light pollution, including glare, light trespass, and skyglow.</p> <p>Visit the link to learn more about the Model Lighting Ordinance: <a href="https://www.darksky.org/our-work/lighting/public-policy/mlo/">https://www.darksky.org/our-work/lighting/public-policy/mlo/</a></p> <p>Rails-to-Trails also provides a toolbox on trail lighting design: <a href="https://www.railstotrails.org/build-trails/trail-building-toolbox/design/lighting/">https://www.railstotrails.org/build-trails/trail-building-toolbox/design/lighting/</a></p>
Establish trail wayfinding standards	The Town of Fort Mill should consider establishing town-wide trail wayfinding standards to foster consistent placement, use, and design of wayfinding signage (including route confirmation, directional, and kiosk signage). Wayfinding standards should complement the existing greenways, multi-use facilities, and park wayfinding standards to create a comprehensive multi-modal wayfinding system.



*Dark Sky Lighting* Credit: Jim Richardson

## RECOMMENDED PROGRAMS

Bicycle, pedestrian, and trail-specific programs encourage and strengthen the culture for trail or multi-use path use within a community. Investments in these programs, when coupled with infrastructure improvements, frequently lead to an increase in biking, walking, and hiking among residents and visitors. Administering a series of community programs can help to increase active transportation, and more specifically the use of trails and multi-use paths. A summary of programs and initiatives are provided below and are organized into the following program classifications that are tailored to specific needs of the community:

- ▶ Educational Programs
- ▶ Encouragement Programs
- ▶ Enforcement Programs
- ▶ Project Based Programs
- ▶ Evaluation Programs

### Educational Programs

Educational programs can help provide trail users with information related to how they can safely use facilities within their communities. Informational packets, brochures, in-person learning events, technical training, and e-learning courses may be strategies used to disseminate educational material throughout the community. Not only is it important to educate pedestrians and bicyclists, but also motorists who may share the road with these users. Educational information should be tailored appropriately to a wide range of age groups, cultures, and abilities. Materials may include information related to safe facility design (e.g., the importance of separated facilities from the roadway), how to use specific facilities, permitted uses along facilities (e.g., hiking, horse-riding, rollerblading, and bicycling), how to properly fit oneself for a helmet, and the health and wellness benefits associated with using trails.

#### *Safe Routes to School*

The Safe Routes to School (SRTS) Program aims to make it safer for children walking and riding a bicycle to school. The program facilitates the planning, development, and implementation of projects and activities to improve safety and reduce traffic, fuel consumption, and improve the air quality near schools. SCDOT receives Transportation Alternative Program (TAP) federal funds and select projects through a competitive process open to local governments and school systems. At the local level, SRTS practitioners lead education and encouragement programs with families and schools. They also support local policies to make walking and bicycling safer within their communities. Each district in South Carolina may establish a SRTS district coordinating committee or team.

<https://www.saferoutesinfo.org/>

### *SCDOT Safety Tips for Biking and Walking*

SCDOT released a list of key safety tips related to biking and walking. These tips are provided in the bulleted lists below and could be included in the educational materials at other community-led events. Visit the link below to learn more about each safety tip.

#### Bike Safety Tips:

- ▶ Obey traffic signs and signals
- ▶ Never ride against traffic
- ▶ Follow lane markings
- ▶ Don't pass on the right
- ▶ Scan the road behind you
- ▶ Keep both hands ready to brake
- ▶ Wear a helmet and never ride with headphones
- ▶ Dress appropriately
- ▶ Use hand signals
- ▶ Ride in the middle of the lane in slower traffic
- ▶ Choose the best way to turn left
- ▶ Make eye contact with drivers
- ▶ Look out for road hazards
- ▶ If you must ride at night, use lights
- ▶ Keep your bike good repair

#### Pedestrian Safety Tips:

- ▶ Walk on sidewalks
- ▶ Cross at marked crosswalks or intersections
- ▶ Look for traffic
- ▶ Sees and be seen
- ▶ Watch your kids
- ▶ Do not consume alcohol and walk
- ▶ Obey traffic signals

[https://www.scdot.org/projects/pdf/Bike\\_Ped/safety\\_tips.pdf](https://www.scdot.org/projects/pdf/Bike_Ped/safety_tips.pdf)

### *Environmental Education*

Active transportation facilities such as trails provide users with access to nature. Installing educational signage on the environmental benefits or existing conditions along a trail could serve as environmental education programming for a community. Installing tree identification markers is one way to promote environmental education along a trail. Trail cleanups and other volunteer events may also instill a sense of pride and support environmental stewardship for the land surrounding the trail. According to the United States Environmental Protection Agency (USEPA), the following components should be incorporated into environmental education programming:

- ▶ Awareness and sensitivity to the environment and environmental challenges
- ▶ Knowledge and understanding of the environment and environmental challenges
- ▶ Attitudes of concern for the environment and motivation to improve or maintain environmental quality
- ▶ Skills to identify and help resolve environmental challenges
- ▶ Participation in activities that lead to the resolution of environmental challenges

<https://www.epa.gov/education/what-environmental-education>



## Encouragement Programs

Inspiring residents to walk, bike, and hike within their community can be achieved through the implementation of encouragement programs. Programs should consider various ages (e.g., children vs elderly), abilities (e.g., disabled), and cultures within a community. They should also focus on differing users for the facilities such as recreation and transportation. Encouragement programs can take on many different shapes and forms so communities can get creative. For example, they can range anywhere from town-wide events to handheld maps and brochures. Encouragement programs may help attract users to trails for recreation purposes or to promote health, exercise, and well-being. Additional examples of encouragement programs that the Town can implement are provided below.

### *Trail Network Map and/or Mobile App*

Comprehensive network maps and mobile apps are developed to guide bicyclists and pedestrians along preferred routes with existing active transportation facilities or routes that are suitable for pedestrians and bicyclists of all skill levels. Hiking maps can also provide details related to levels of terrain difficulty, permitted uses along trails, and identifying reference points when in the woods. Maps and mobile apps are also helpful resources that assist new residents and visitors to safely navigate the multimodal transportation network via preferred routes.

The TrailLink App helps users locate nearby trails and offers access to 40,000 miles of offline trail maps that can be downloaded before leaving for a hiking trip. The app can also be used to log completed trails so they can be tracked for future reference. As the Town expands its trail network, it can submit trails and maps to the app.

The AllTrails App is another popular trail app that can help users identify nearby trails. Trails can be filtered by distance away, activity, difficulty, length, suitability, and more. Maps can be downloaded for offline use and enabling off-route notifications allows users to navigate their trail of interest. App users can save favorite trails and view records of completed trails through the app.

### *Wayfinding Signage*

Wayfinding systems consist of comprehensive signage and/ or pavement markings to guide bicyclists and pedestrians to their destinations along preferred routes. Signs are typically placed at decision points along the routes – typically at the intersection of two or more bikeways and trails and at other key locations leading to and along designated routes. There are three types of wayfinding signs: confirmation signs, turn signs, and decision signs. Pavement markings can also be installed to help reinforce routes and directional signage and to provide route branding benefits. Signage should be simple and easily understood while a trail user is in motion.

### *Bicycle Parking*

Bicycle parking is a critical strategy for promoting bicycling for transportation and recreation. Convenient, easily used, and secure bicycle parking encourages people to replace some of their car trips with bicycle trips and helps legitimize bicycling as a transportation mode by providing parking opportunities equal to motorized modes. Guidance on bike parking, including site planning for short and long-term parking, rack selection, and placement can be found in the Association of Pedestrian and Bicycle Professionals (APBP) Essentials of Bike Parking.

<https://www.apbp.org/assets/docs/EssentialsofBikeParking-FINA.pdf>

### *Bicycle Repair Stations*

Bicycle repair stations include all the tools necessary to perform basic bike repairs and maintenance, from changing a flat tire to adjusting brakes. The tools are securely attached to the stand with cables with tamper-proof fasteners, and an air pump can be attached to the base of the repair station. Bicycle repair stations should be placed in areas highly trafficked by bicyclists, such as parks and recreation centers, Downtown, trailheads, and employment and shopping centers.

### *Bike/Walk to School Day Events*

Bike/Walk to School Days are energizing events that remind adults and students alike of the health and environmental benefits of bicycling and walking to school. For many communities, these events lead to more bicycling and walking throughout the school year. These events often draw attention to safety concerns that need to be addressed and inspire children and families to walk or use a bike for the school commute more often. National Bike to School Day occurs every May as part of Bike Month, and National Walk to School Day occurs annually on the first Wednesday in October.

### *Bike/Walk to Work Day Events*

Bike to Work Day is an annual event that promotes bicycling as an option for commuting to work. This special day is held every May during Bike Month. Bike to Work Day events usually include pit stops along popular biking routes to provide commuters with free breakfast, prizes, and educational materials on active transportation. For many communities, these events lead to an increase in bicycling for transportation and recreation. Similar programs can be implemented for walking to work as well.

National Walk to Work Day is held on the first Friday of April. The US Department of Health and Human Services and the American Podiatric Medical Association started this event in 2004. On this day, commuters heading to work are encouraged to walk, rather than drive to work.

### *Themed Biking/Walking Events*

Biking and walking events encourage active transportation and present opportunities for residents to socialize over shared experiences. Local advocacy groups, municipalities, and/or businesses can promote biking and walking by hosting themed bike rides, walking tours, races, or festivals that encourage walking or bicycling. Events can be centered around holidays, local festivals, local historic and cultural events, or coincide with national events such as Earth Day and National Trails Day. For many communities, these events lead to regularly scheduled bike rides or walks within communities.

### *Prescribe-a-Trail/Trailrx/Walk with a Doc*

Prescribe-a-Trail, Trailrx, and Walk with a Doc are all free walking programs that encourage people to live healthier lifestyles through walking on local trails. Typically, healthcare providers and organizations lead these outings and provide the opportunity for attendees to ask the professionals medical questions. View the link below to identify local walks, start a walk, or donate to the Walk with a Doc program.

<https://walkwithadoc.org/>

### *Trail Scavenger Hunts/Geocaching*

Organized scavenger hunts hosted along trails can help attract both old and new users to generate activity along a particular trail. The scavenger hunts can be educational, competitive, or non-competitive. The Kalamazoo River Valley Trail is an example of a trail system that provides scavenger hunt downloads on their website that users can complete in their own time. Global Positioning System (GPS)-enabled devices like cell phones can also be used to find geocaches, which can be placed anywhere along or near trails. The Geocache app can be downloaded to search for nearby containers. The Anne Springs Close Greenway hosts a trails passport activity that promotes visitors to explore various points of interest offered at the greenway (follow the link below to learn more). These activities are great ways to engage young children on trails and promote family outings outdoors.

[https://www.ascgreenway.org/wp-content/uploads/2022/04/2022ASCG\\_TrailsPassport\\_web\\_small.pdf](https://www.ascgreenway.org/wp-content/uploads/2022/04/2022ASCG_TrailsPassport_web_small.pdf)

### *Greenway/Trail Ambassadors*

Local volunteers can act as tour guides or ambassadors along trails and greenways within their community. Catawba South Carolina Master Naturalists that meet at the Anne Springs Close Greenway may also serve as guides and provide educational information along group hikes, walks, or biking events. Guided tours can help greenway and trail users feel more comfortable while using a trail and informed about their surroundings.

### *Bicycle Friendly Community Designation*

The Bicycle Friendly Community program provides a roadmap to improving conditions for bicycling and guidance to help make a community's vision for a better, bikeable community a reality. The Bicycle Friendly Community program is sponsored by the League of American Bicyclists. Applications for the programs are accepted twice a year. The program provides a checklist for current and aspiring bicycle friendly communities that includes the following action items:

- ▶ Adopt the BFC Action Plan Charter
- ▶ Review BFC resources, facts and questions, and information online.
- ▶ Form an Active Transportation Advisory Committee
- ▶ Conduct a bike parking inventory or audit in your community
- ▶ Encourage local employers to apply for the Bicycle Friendly Business designation
- ▶ Host a League Cycling Instructor (LCI) seminar or sponsor one or more local residents, bike advocates, and/or municipal employees to attend an existing LCI seminar elsewhere.
- ▶ Create or improve the inventory of your community's bike infrastructure
- ▶ Conduct a bike count to start to benchmark and measure trip ridership in your community
- ▶ Send someone to attend the National Bike Summit
- ▶ Organize a local bicycling event or series of events

<https://www.bikeleague.org/community>

### *Walk Friendly Community Designation*

Walk Friendly Communities have shown a commitment to improving and sustaining walkability and pedestrian safety through comprehensive programs, plans, and policies. Communities apply to this program to receive recognition in the form of a Bronze, Silver, Gold, or Platinum designation. Before applying, the Town can take a self-assessment using the online assessment tool to collect all the Town's pedestrian-related data in one location which can help guide the discussion about areas for improvement. This data can also be used to apply for future grants. The Designation as a Walk Friendly Community sets the Town up as an example among peer cities and raises local awareness about the value of supporting a walkable environment.

<https://www.walkfriendly.org/>

### *Trail Town Program*

The Trail Town Program is an initiative of The Progress Fund, and it supports community development around the nation with outdoor tourism and trails. The five keys to a successful Trail Town include partnerships, assessments and research, connecting town to trail, business and real estate development, and marketing.

According to the program, thriving trail economies in The Trail Program:

- ▶ Create economic growth
- ▶ Grow local businesses and create jobs
- ▶ Compound the trail's economic potential
- ▶ Improve infrastructure in rural areas
- ▶ Protect the trails and surrounding nature

If Fort Mill participates in the program, the Town could help grow and fund business development within the community, as well as learn how to market to bring in more trail tourism.

<https://www.trailtowns.org/about-us/>

## Enforcement Programs

Although complete streets accommodate various users (e.g., pedestrians, bicyclists, and motorists), each must pay special attention to rights of each other. For example, a motorist must yield to a pedestrian at a marked crosswalk. Enforcement programs help to ensure that motorists, bicyclists, and pedestrians understand common laws and exist to educate all roadway users to mitigate potential conflict.

### *Trail Watch Program*

The Town of Fort Mill should establish a Trail Watch Program to increase ‘eyes on the trail’ and report fallen trees, vandalism, or needs for safety measures such as call boxes. Volunteers could support the Program and partner with the Town’s Police Department to patrol and maintain trails during specific times of the year.

### *South Carolina Code of Laws Concerning Bicycles*

The following link provides information pertaining to laws that affect bicyclists in South Carolina. This information can be used to guide an event related to bicyclist safety within the community.

[https://www.scdot.org/projects/pdf/Bike\\_Ped/bike\\_laws.pdf](https://www.scdot.org/projects/pdf/Bike_Ped/bike_laws.pdf)

### *South Carolina Code of Laws Concerning Pedestrians*

The following webpage provides information pertaining to laws that affect pedestrians in South Carolina: <https://scdps.sc.gov/pedestriansafety>

For more information on laws pertaining to pedestrians, please visit Code of Laws - Title 56 - Chapter 5 - Uniform Act Regulating Traffic On Highways, Article 25.

### *South Carolina’s Target Zero Project*

The South Carolina Department of Public Safety initiated the Target Zero Project to reduce the number of traffic fatalities to zero. To support this project, highway patrol troopers will raise awareness and ticket drivers on the three main violations that lead to deaths on the road which include speeding, drinking under the influence, and seatbelt use.

<https://scdps.sc.gov/tz>

## Project Based Programs

Project based programs can serve as live demonstrations that make projects easier for the community to visualize and see in action. These types of programs also help to publicize what future project facilities may look like within a community and provides a way for residents and visitors to test them out and familiarize themselves with the facility. Project based programs are often introduced to build excitement and consensus among the public for a specific project.

### *Pop-up Demonstrations / Pilot Programs*

Pop-up demonstrations or pilot projects for proposed multi-modal facilities, such as parklets, crosswalk installations, and pop-up bike lanes create an action-based approach to transforming the local bicycle and pedestrian network by using short-term, low-cost, and scalable interventions to propel long-term change. Demonstrations or pilot projects can be led by the Town or local advocates and residents to experiment with and gather input on potential street design improvements. The Tactical Urbanist's Guide provides more examples of potential pop-up events that the Town can pull from to showcase demonstration projects within the community.

<http://tacticalurbanismguide.com/about/>

### *Paint the Pavement Programs*

Paint the Pavement programs allow mural painting on public streets to encourage community building and public art to transform roadway public space into neighborhood assets. Street murals implemented as part of the program also serve as an important traffic calming solutions in residential neighborhoods and can provide on-street connections between trail corridors. Paint the Pavement programs are typically sponsored by governmental agencies and mural design and painting are led by local volunteers.

The City of Charlotte created its own Paint the Pavement program with a two-page guide and a comprehensive manual that describes how communities can install a project within the City's maintained right-of-way. Refer to the following link to learn more about how to initiate a similar program within your Town.

[https://charlottenc.gov/civinnovation/Placemaking\\_Program/placemaking/Documents/PaintthePavement\\_with%20Attachments.pdf](https://charlottenc.gov/civinnovation/Placemaking_Program/placemaking/Documents/PaintthePavement_with%20Attachments.pdf)

### *Trail Mural Program*

Murals and public art installations can enhance the trail user experience within a community. Art displayed along trails may celebrate local culture and history or serve as functional pieces like benches, tables, and signage. Installations may also be temporary or long-term.

The City of Greensboro in North Carolina created an interactive public art map to accompany public art installations that provide the name of artists and other interesting facts about each art installation. The installations are located along the Downtown Greenway which is a planned 4-mile walking and biking greenway that will loop around downtown Greensboro and will serve as a key economic development tool for the community. Installations along the greenway include neighborhood benches, cornerstones, stie furnishings, landscape design, bike racks, and other special features. Follow the link below to see examples displayed in Greensboro.

<https://downtowngreenway.org/explore/art/>

### *Adopt-a-Trail Segment*

Adopt-a-Trail is a volunteer-led program that allows members of the public to contribute to the preservation of trails within their communities. Adopt-a-trail programming could support maintenance requirements for the future trail network. Volunteers in the community could support these efforts and help reduce the maintenance burden from Town staff (e.g., trash pickup, remove fallen branches, inspect conditions of trail amenities). Town staff may act as supervisors on larger volunteer projects. The Town may require program commitment periods and identify long-term volunteer service individuals from local scouting groups, clubs, and businesses.

## Evaluation Programs

Evaluation programs are the final program category and can be used to help evaluate the efficiency and effectiveness of a specific project or element related to a trail project. These programs typically rely on surveying methodologies that include measurable and quantitative data to produce data-driven results. These results can be used to support future decision-making conversations on trail or multi-use path projects. Counters and audits are typically performed to understand the existing conditions of facilities, as well as identify gaps and opportunities for improvements within a network. Several examples of an evaluation program are described below.

### *Trail Count Program*

Trail count programs provide non-motorized travel information for trails, paths, and greenways. Count programs assist municipalities to understand existing trail traffic and plan for future non-motorized infrastructure needs. Agencies who show clear evidence of use are more likely to receive funding for future trail projects.

<https://www.railstotrails.org/build-trails/trail-building-toolbox/management-and-maintenance/trail-user-surveys-and-counting/>

### *Trail User Surveys*

The Rails-to-Trails Conservancy provides information related to trail user surveys. Collecting data from trail users can help a jurisdiction understand who uses a particular trail, where the user is from, how they are using the trail, how long the user is on the trail, and which trail sections are the most popular. These surveys can help jurisdictions learn more about maintenance improvement areas and gain a better idea of what new amenities can improve the trail-user experience. The data collected from these surveys can be used to support grant applications for the development of new trails or improvements to existing trails.

<https://www.railstotrails.org/build-trails/trail-building-toolbox/management-and-maintenance/trail-user-surveys-and-counting/>

### *Bike and Walk Audits*

Bicycling and walking audits are processes that involve the systematic gathering of data about environmental conditions that affect bicycling and walking. Audits are typically performed by personnel with experience in local bicycle and pedestrian infrastructure issues and focus on a school site, a corridor popular for bicycling and walking, or an intersection that residents find unsafe. Guidance on conducting bicycling and walking audits is provided by the Safe Routes to School Online Guide.

[http://guide.saferoutesinfo.org/engineering/walking\\_and\\_bicycling\\_audits.cfm](http://guide.saferoutesinfo.org/engineering/walking_and_bicycling_audits.cfm)

### *311 Service / See-Click-Fix*

311 service is a governmental program that allows citizens to report maintenance issues, such as street debris in bike lanes, potholes, damaged sidewalks, or malfunctioning traffic signals. Similarly, See-Click-Fix is an online platform and mobile app that allows residents to report maintenance issues via interactive map to precisely pinpoint and describe the issue. Municipalities can partner with See-Click-Fix as their local 311 provider.



**WALKING TRAIL**  
WALTER Y. ELISHA PARK



# 06

## IMPLEMENTATION

### OVERVIEW

Achieving the vision and goals outlined in this plan will require the commitment of Town officials and staff, the Rock Hill-Fort Mill Area Transportation Study (RFATS) Metropolitan Planning Organization (MPO), the South Carolina Department of Transportation (SCDOT), and the support and leadership from the community and other partner organizations. The adoption of this plan is the first step for improving trail connectivity in the Town of Fort Mill.

This chapter covers several implementation strategies to assist with moving the proposed trail network projects closer towards construction:

- Partners + Roles
- Action Plan
- Trail Maintenance

## PARTNER ROLES

Achieving success in the development of Fort Mill's trail network will rely on collaboration with community partners and stakeholders at the state, regional, and local levels. Implementation will require both individual and coordinated efforts by all project stakeholders. Key roles in the implementation of Fort Mill's trail network are outlined below.

The following partners are recommended to support implementation of the future trail network:

- ▶ Town of Fort Mill
- ▶ York County
- ▶ South Carolina Department of Transportation
- ▶ Rock Hill-Fort Mill Area Transportation Study (RFATS) Metropolitan Planning Organization (MPO)
- ▶ Carolina Thread Trails
- ▶ Nation Ford Land Trust
- ▶ Anne Springs Close Greenway
- ▶ Local Residents + Community Groups
- ▶ Private Sector Partners

### Town of Fort Mill

Municipal governments often lead or support the development of recreation and transportation projects within their respective jurisdiction. On projects they play a supporting role, municipal staff are the primary coordinators for community engagement, policy development, and maintenance. The Town of Fort Mill has an established Capital Improvement Programs (CIP) that identifies and prioritizes projects for funding, as well as some bicycle and pedestrian-friendly policies already in place. As the project leader, the Town of Fort Mill will be responsible for the development of all trail segments within their respective jurisdiction. The Town will be responsible for adopting the *Trail Master Plan* and amending other local plans and ordinances to incorporate study recommendations. The Town will also need to coordinate with York County and neighboring municipalities on developing the connections across jurisdictional boundaries.



### York County

York County leads development and provides services for municipalities and unincorporated areas within the county. Key departments involved in the development of Fort Mill's trail network include the Parks and Planning + Development Services Departments. The York County Parks Department plans and develops parks and trails and conserves natural areas to provide recreational access to all residents within the county. The York County Planning Department oversees all aspects of land development in the unincorporated areas of York County, including long-range planning, zoning, permitting, and environmental compliance. The department is responsible for developing and implementing plans and policies for land development; administering the building, planning and development, zoning, subdivision, floodplain damage prevention, and stormwater management and sediment control ordinances; issuing building and land disturbance permits; and enforcing all ordinances applicable to land usage. The County will be responsible for coordinating with partners on funding opportunities and leading the development of county trail connectors through land acquisition, design, construction, and maintenance.



## South Carolina Department of Transportation (SCDOT)

SCDOT allocates federal and state funding and establishes policies for transportation improvements in communities across South Carolina. SCDOT develops the State Transportation Improvement Program (STIP), which identifies projects that will receive funding during a 7-year period (current STIP is 2021–2027). The STIP is generally scheduled for updating every three years and is revised on a continual basis to reflect the latest program and project information. SCDOT policies, such as Complete Streets and Control of Access, provide guidance and oversight for permitting and implementing projects. The Complete Streets Policy requires SCDOT to consider and incorporate multimodal facilities in the design and improvement of the state’s transportation projects. The Control of Access Policy provides design guidance and defines permitted activities within the ROW for limited, partial, and full control access roadways. Since several recommended sidepaths are within SCDOT ROW across the Town, coordination with SCDOT is of critical importance. SCDOT Engineering District 4 oversees York County.



## Rock Hill-Fort Mill Area Transportation Study (RFATS) Metropolitan Planning Organization (MPO)

RFATS is the Rock Hill-Fort Mill Area Transportation Study which is the Metropolitan Planning Organization (MPO) for eastern York County as well as the panhandle of Lancaster County, South Carolina. RFATS coordinates comprehensive transportation planning activities within the urbanized area, in cooperation with SCDOT, the Federal Highway Administration (FHA), and the Federal Transit Administration (FTA). Its goal is to plan the most efficient, responsive, and cost-effective transportation system for the movement of people and goods in the urbanized area. The SCDOT Commission determines the funding priorities for the federal-aid program following each new federal highway bill and annual appropriations act. As a leader and supporting partner of this planning effort and project prioritization in the region, RFATS is responsible for coordinating with SCDOT to incorporate study recommendations into planning documents, assisting in facilitating project development and coordination between jurisdictions and non-profit partners along the trail network, and coordinating with SCDOT on STIP project development to incorporate viable trail segments into roadway improvements.



## Carolina Thread Trails

The Carolina Thread Trail has planned and constructed several miles of trails within and around the Town of Fort Mill so it serves a key role in advocating for project investment. This organization generates support for the project by providing funding, raising awareness amongst the public, advocating to elected officials to prioritize funding for trail development, and fostering collaboration amongst jurisdictional partners. The Carolina Thread Trail may be responsible for coordinating with regional agencies, York County, and municipalities on developing funding opportunities, project phasing, and building public support for the trail network. This organization should also provide guidance and technical assistance to the Town on trail design standards.



## Nation Ford Land Trust

Nation Ford Land Trust is a 501 (c)(3) organization dedicated to the preservation of open spaces, natural beauty, and the scenic heritage of the York County, South Carolina area. This conservation organization currently owns or holds conservation easements on a total of 15,512 acres in the region. Nation Ford Land Trust is responsible for The Founders Trail (currently closed) and Springfield Trail (temporarily closed) located in the Town of Fort Mill.



## Anne Springs Close Greenway

The Anne Springs Close Greenway (ASCG) offers several existing trails within the Fort Mill community. Although trail use requires paid passes, several of the existing trails will connect to newly proposed trails and sidepaths recommended in this plan. Coordination with ASCG will be required to help build public support for the future trails as well as future connection points to their existing trail system.



## Local Residents + Community Groups

Residents and community groups that promote greenway and trail system expansion serve a key role in advocating for infrastructure investment. Community members and groups generate support for projects by raising awareness within the public, advocating to elected officials to prioritize funding for greenways and trail amenities, and fostering collaboration amongst jurisdictional partners.



## Private Sector

Private entities along the proposed trail network will play an important role in trail development. Key private sector partners include the Southern Railway, Duke Energy, York Electric Cooperative, and developers. The Southern Railway runs north to south through the Town, creating barriers for active transportation connections. Duke Energy and York Electric Cooperative manage electric utilities and serve customers throughout the county. Developers may construct planned greenways, sidepaths, and sidewalks as a requirement to development in municipal and county limits, when specified in a jurisdiction's code of ordinances.



## ACTION PLAN

The following Action Plan outlines strategies to implement prioritized trails and sidepaths in Fort Mill based on a 10-year implementation plan. The Town should apply these action plan steps to the priority projects identified in this plan. Ideally, the identified priority projects will have a greater opportunity for implementation in the short-term. Timeframe durations for the steps are categorized into short-term, medium-term, long-term, and perpetual steps. These categories are described below.

STATUS DESCRIPTION	EXPLANATION
Short-Term	The action item could be initiated in the near future within 2-4 years.
Medium-Term	The action item could be initiated within the next 4-8 years.
Long-Term	The action item could be initiated within the next 8+ years.
Perpetual	The action item does not have an end date. It may be currently under progress and will continue to be implemented in perpetuity of this master plan.

TASK #	ACTION	Descripton	LEAD	PARTNERS	TIMEFRAME
1	Adopt the <i>Fort Mill Trail Master Plan</i> .	Plan adoption establishes the <i>Fort Mill Trail Master Plan</i> as the guiding planning document for trails and sidepaths.	Fort Mill Town Board	Town Departments, Steering Committee	Short-Term
2	Develop a work plan and designate Town staff to lead implementation of plan recommendations.	The Town Manager and Department Directors of Planning, Public Works, and Parks should identify their staff leads and duties for plan implementation. Staff leads should be selected to guide project development, complete streets, data collection, programs, and maintenance.	Town Manager, Town Department Directors	Town Staff	Short-Term
3	Provide staff resources and training related to sidepath and trail infrastructure and amenities.	Provide resources and funding for staff involved in plan implementation to attend webinars, trainings, and conferences on sidepath, greenway, and trail planning, design, and programming Invest in guidance materials available from FHWA, SCDOT, NACTO, AASHTO, Rails to Trails Conservancy, and American Trails.	Town Manager, Town Department Directors	Town Staff, Carolina Thread Trail, Nation Ford Land Trust, Anne Springs Close Greenway	Perpetual

TASK #	ACTION	Descriptor	LEAD	PARTNERS	TIMEFRAME
4	Designate an Advisory Committee to guide plan implementation.	Continue to convene the Parks & Recreation Subcommittee to guide Town staff in the implementation of recommended infrastructure, amenities, and policies. Parks & Recreation Subcommittee representatives should include of trail users of all ages and abilities.	Town Board	Town Departments, Advisory Committee, Steering Committee	Short-Term
5	Prioritize the development of a Comprehensive Bicycle and Pedestrian Plan for the Town of Fort Mill.	The Planning Department should prioritize the development of a comprehensive bicycle and pedestrian plan to establish a network of on- and off-road bicycle and pedestrian facilities that connect to and compliment the existing and proposed trail network.	Town Planning Department, Town Board	Town Departments, Advisory Committee, SCDOT, York County, RFATS	Short-Term
6	Amend the Town of Fort Mill Code of Ordinances.	Amend the Code of Ordinances to incorporate policy recommendations such as requiring developer-built trails, reducing the Town-wide speed limit, adopting a municipal Complete Streets Policy, and encouraging conservation subdivisions and cul-de-sac connectivity to ensure trails implementation occurs with future development.	Town Planning Department, Town Board	Town Departments	Short-Term
7	Ensure that trail infrastructure recommendations are incorporated into new developments.	The Planning Department should coordinate closely with developers during the development review process on inclusion of planned trails in new developments where required.	Town Planning Department	Town Board, Town Departments	Short-Term
8	Define standards for trail facilities in the Town of Fort Mill Code of Ordinances.	Amend the Code of Ordinances to define the recommended facilities included in the <b>Trail Master Plan</b> (i.e., paved trails, natural surface trails, and sidepaths) and include design standards as specified in the proposed typical sections.	Town Planning Department, Town Board	Town Departments	Short-Term

TASK #	ACTION	Descripton	LEAD	PARTNERS	TIMEFRAME
9	Develop a Landowner and Developer Outreach Program	In efforts to coordinate with developers and private landowners to construct planned trails and sidepaths, Town staff should develop a program with strategies to build relationships and maintain consistent contact with developers and willing landowners to work towards acquiring property/easements for trails and to ensure trails and sidepaths are constructed in new developments.	Town Planning and Parks Departments	Town Board, Advisory Committee, York County, Carolina Thread Trail, Nation Ford Land Trust, Anne Springs Close Greenway	Short-Term
10	Develop funding strategies for project implementation.	Review funding resources provided in Appendix D and coordinate with Town Staff, RFATS, York County, and SCDOT to determine funding strategies for plan recommendations.	Town Manager and Department Directors	Town Staff, Town Board, RFATS, SCDOT, York County	Perpetual
11	Conduct feasibility studies for priority trail projects.	Coordinate with SCDOT, regional partners, and adjacent municipalities to conduct feasibility studies for priority trail projects, such as Harris St Park Trail, Future Park Trail, Banks Athletic Park to Schools Trail Connector, Catawba River Trail, Sugar Creek Trail, and Byp-21 Trail-Connector to Rock Hill Riverwalk to determine corridor alignments with 30% design and detailed cost estimates.	Town Department Directors and Staff	RFATS, SCDOT Division 4, York County, Carolina Thread Trail	Short- to Medium-Term
12	Coordinate with SCDOT Engineering District 4 on future STIP projects to include sidepaths and intersection treatments in roadway projects.	Coordinate with SCDOT Engineering District 4 on future State Transportation Improvement Program (STIP) projects to incorporate sidepaths and intersection treatments in roadway enhancements.	Town Department Directors and Staff, SCDOT Division 4	RFATS	Perpetual

TASK #	ACTION	Descripton	LEAD	PARTNERS	TIMEFRAME
13	Develop a municipal Bicycle and Pedestrian Count Program.	Coordinate with SCDOT and RFATS to establish a municipal bicycle and pedestrian count program to collect usage rates of bicyclists and pedestrians along trails and sidepaths. A robust bicycle and pedestrian count program includes a mix of permanent counters and short duration counters. Counting provides information for multi-modal usage and patterns at and along intersections, paths and roadways. Agencies who show clear evidence of use are more likely to receive funding and support for multi-modal projects.	Town Planning and Parks & Rec Departments	SCDOT, RFATS, Advisory Committee, Carolina Thread Trail	Short- to Medium-Term
14	Develop a Trail System Branding Study to establish a cohesive brand for the network.	Develop a branding study to create cohesive brand elements such as logos, graphics, and color palette for wayfinding, lighting, furnishings, and other amenities to create a consistent, high-quality user experience along the trail system.	Town Planning, Parks & Recreation, and Public Works Departments	Town Manager, Town Board, Advisory Committee, Carolina Thread Trail, Nation Ford Land Trust, Anne Springs Close Greenway	Short- to Medium-Term
15	Develop a Maintenance + Operations Plan.	Develop a greenway, sidepath, and trail amenities maintenance plan to define maintenance responsibilities, budget, and staff resources required to maintain facilities.	Town Planning, Parks & Recreation, and Public Works Departments	Town Manager, Town Board, Advisory Committee	Short-Term
16	Identify and implement wayfinding standards to improve the overall trail wayfinding system.	Identify wayfinding standards to foster consistent placement, use, and design of wayfinding signage. Signage should clearly communicate current locations, access points, adjacent streets, distances, directions, destinations, and estimated travel times/ mileage.	Town Planning, Parks & Recreation, and Public Works Departments	Town Manager, Town Board, Advisory Committee, Carolina Thread Trail, Nation Ford Land Trust, Anne Springs Close Greenway	Short- to Medium-Term

TASK #	ACTION	Description	LEAD	PARTNERS	TIMEFRAME
17	Conduct a Lighting Study to determine lighting needs along trails.	Conduct a system lighting study to evaluate strategic locations for trail lighting to improve user safety and security.	Town Planning, Parks & Recreation, and Public Works Departments	Town Manager, Town Board, Advisory Committee, Carolina Thread Trail, Nation Ford Land Trust, Anne Springs Close Greenway	Long-Term
18	Create a dedicated webpage for the trail system on the Town of Fort Mill's website.	Create a dedicated webpage for Fort Mill's trail system that includes plan and development updates, and an interactive trail map that includes existing and planned facilities, trail access points, and closures. Continue to update the trail network maps as facilities are constructed.	Town Planning Department and Parks & Recreation Department	Town Manager, Advisory Committee	Perpetual
19	Evaluate performance of the trail network annually.	Develop performance measures and conduct annual reviews of the trail system to understand facility needs, funding and resource needs, and usage rates.	Town Planning Department and Parks & Recreation Department	Town Manager, Advisory Committee	Perpetual
20	Partner with the Fort Mill School District on Safe Routes to School Programming.	Coordinate with the Fort Mill School District to expand and develop Safe Routes to School programs, such as bike and walk to school events and bicycle and pedestrian safety education, and to fund investments in bicycle and pedestrian infrastructure, such as bike parking and trail and sidepath connections to schools.	Town Planning, Parks & Recreation, and Public Works Departments	Fort Mill School District, Advisory Committee	Perpetual

## Project Development Process

The *Fort Mill Trail Master Plan* provides an overall framework for development of trails and sidepaths; however, it is only the first step in a larger process. As a living document, recommendations and priorities outlined in the *Trail Master Plan* may evolve with changing development pressures, funding opportunities, and community growth trends. The Town may also need to conduct feasibility studies to understand the environmental conditions, routing challenges, and costs of priority corridors. This detailed analysis allows design and right-of-way acquisition to be finalized, which is followed by construction. Operational and programming plans are developed once the facility is constructed to determine maintenance and evaluation needs. The diagram below illustrates the typical planning, design, and construction process for trail facilities with key phases and individual tasks outlined.



## TRAIL MAINTENANCE

Facility maintenance is essential to the long-term viability of the network. Multi-use paths that are consistently maintained have lower costs over time and provide a safe and positive user experience than facilities that require major rehabilitation work from a lack of consistent maintenance. Good maintenance practices also prolong the useful life of the network, promote positive relationships with adjacent landowners, and create a sense of stewardship in the community.

SAMPLE MAINTENANCE TASK	TASK TYPE	RECOMMENDED FREQUENCY
Tree / Bush trimming Mowing Trail sweeping Signage / Map / Kiosk Updates / Replacement Trash removal / Litter clean-up Planting, pruning, landscaping Flooding repairs Repainting / Restriping Minor patching Minor bridge repairs Lighting replacement Bollard locks / Replacement Pest management	Routine	On-Going / Annually
Greenway and sidepath sealcoating	Minor Repairs	Every 5 Years
Resurfacing of paved trails: <ul style="list-style-type: none"> <li>Asphalt</li> <li>Concrete</li> <li>Boardwalk</li> </ul>	Major Reconstruction	Every 10-15 Years Every 20 Years 10 Years
Complete greenway and sidepath replacement, regrading, and resurfacing	Major Reconstruction	Every 20 Years

Source: *Best Practices in Trail Maintenance: A Manual by the Ohio River Greenway, Purdue University*

This plan recommends a comprehensive approach to maintenance with the development of a maintenance plan to prioritize funding and responsibilities amongst jurisdictions. The maintenance plan should be reviewed and updated annually, responding to lessons learned and changes in tasks, operational policies, standards, and maintenance goals. Key considerations for a greenway system maintenance plan include:

- ▶ Understanding the anticipated needs of the trail system and assessing the capacity of Town staff to meet those maintenance needs.
- ▶ Developing a facility inventory to understand the routine and substantial maintenance needs of signs, amenities, bridges, culverts, and pavement conditions.
- ▶ Estimation of baseline maintenance costs by determining necessary maintenance activities, such as mowing, edging, landscaping, trash removal, debris clearing, lighting, drainage, seasonal maintenance needs, sealcoating, repaving, patching, and bridge repair.
- ▶ Consideration of labor costs based on which maintenance activities can be completed in-house versus contracted out.
- ▶ Assessment of available technologies to collect data on facility conditions and facilitate maintenance functions.
- ▶ Developing methodology to prioritize annual maintenance needs based on facility conditions and available funding.
- ▶ Consideration of emergency services including designated ingress/ egress locations, mile-marker signage along the facility for location identification, and emergency notification systems.



 **SIDEPATH**  
SPRINGFIELD PARKWAY



# APPENDICES

**APPENDIX A:**  
DESIGN RESOURCES

**APPENDIX B:**  
DETAILED COST INFORMATION

**APPENDIX C:**  
ADDITIONAL COMMUNITY  
INVOLVEMENT INFORMATION

**APPENDIX D:**  
FUNDING RESOURCES

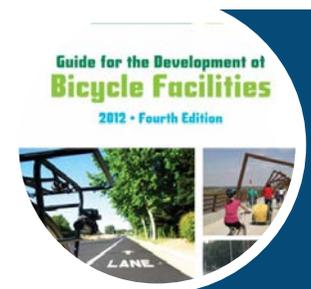
# APPENDIX A: DESIGN RESOURCES

## OVERVIEW

Below are several design resources that can be used to inform bicycle and pedestrian design decisions. Organizations such as Federal Highway Administration (FHWA), American Association of State Highway and Transportation Officials (AASHTO), National Association of City Transportation Officials (NACTO), and South Carolina Department of Transportation (SCDOT) offer general guidelines and project-specific tools to help professionals make design decisions. These guidelines promote flexibility to ensure context-sensitive applications.

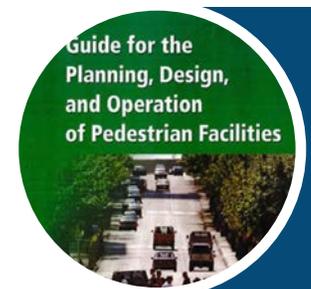
### AASHTO GUIDE FOR THE DEVELOPMENT OF BICYCLE FACILITIES

The AASHTO Guide for the Development of Bicycle Facilities is the authoritative national standard for bikeway design. The document provides guidance to designers and planners by referencing a recommended range of design values and describing alternative design approaches. The guide provides information on how to accommodate bicycle travel and operations in most environments. Sufficient flexibility is permitted to encourage designs that are sensitive to local context and incorporate the needs of bicyclists, pedestrians, and motorists.



### AASHTO GUIDE FOR THE PLANNING, DESIGN AND OPERATION OF PEDESTRIAN FACILITIES

The AASHTO Guide for the Planning, Design, and Operation of Pedestrian Facilities provides guidance for pedestrian facilities along streets and highways. The primary audiences for this manual are planners, roadway designers, and transportation engineers, whom make decisions on a daily basis that affect pedestrians. The guide focuses on identifying effective measures for accommodating pedestrians on public rights-of-way, and it recognizes the effect that land use planning and site design have on pedestrian mobility and addresses these topics as well.



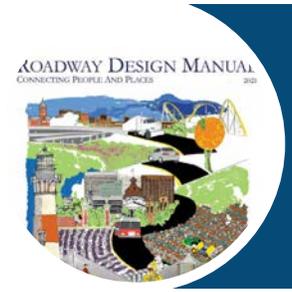
### MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES (MUTCD)

The Manual on Uniform Traffic Control Devices for Streets and Highways, or MUTCD defines the standards used by road managers nationwide to install and maintain traffic control devices on all public streets, highways, bikeways, and private roads open to public travel. The MUTCD is published by the Federal Highway Administration (FHWA) and is a compilation of national standards for all traffic control devices, including road markings, roadway signs, and traffic signals.



### SCDOT ROADWAY DESIGN GUIDE

The South Carolina Department of Transportation's (SCDOT) Roadway Design Manual defines standards for roadways owned and maintained by SCDOT, including typical sections for roadways. Typical sections establish design elements that emphasize safety, mobility, complete streets, and accessibility for multiple modes of travel. Typical sections also provide guidelines for comprehensive transportation planning, project planning, and project design activities.



### SCDOT COMPLETE STREETS IMPLEMENTATION GUIDANCE

The South Carolina Department of Transportation (SCDOT) Complete Streets Implementation Guide is designed to assist SCDOT staff engineers, project managers and designers in implementing the Complete Streets Policy. This document provides comprehensive guidance for incorporating a complete streets approach into SCDOT's planning, programming, design, and maintenance processes.



### PENNSYLVANIA TRAIL DESIGN & DEVELOPMENT PRINCIPLES: GUIDELINE FOR SUSTAINABLE, NON-MOTORIZED TRAILS

This document compiles the best practices and guidelines for the planning, design, construction, and management of trails. Techniques are presented for developing trails that create desirable and enjoyable experiences for trail users. These techniques employ sustainable design elements and construction practices that allow the trail to make use of natural systems so that the trail remains both physically and environmentally sustainable.



### NACTO URBAN STREETS DESIGN GUIDE

The Urban Street Design Guide charts the principles and practices of the nation's foremost engineers, planners, and designers working in cities today. A blueprint for designing 21st century streets, the guide unveils the toolbox and the tactics cities use to make streets safer, more livable, and more economically vibrant. The Guide outlines both a clear vision for complete streets and a basic road map for how to bring them to fruition.



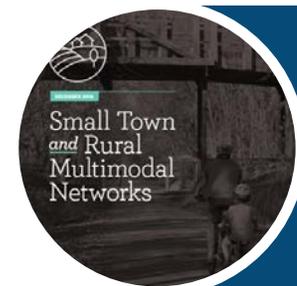
### **NACTO URBAN STREET STORMWATER GUIDE**

The Urban Street Stormwater Guide advances the discussion about how to design and construct sustainable streets. The guide provides cities with national best practices for sustainable stormwater management in the public right-of-way, including core principles about the purpose of streets, strategies for building inter-departmental partnerships around sustainable infrastructure, technical design details for siting and building bioretention facilities, and a visual language for communicating the benefits of such projects. The guide sheds light on effective policy and programmatic approaches to starting and scaling up green infrastructure, provides insight on innovative street design strategies, and proposes a framework for measuring performance of streets comprehensively.



### **FHWA SMALL TOWN & RURAL MULTIMODAL NETWORKS**

The Federal Highway Administration (FHWA) Small Town and Rural Multimodal Networks applies existing national design guidelines in a rural setting and highlights small town and rural case studies. It addresses challenges that are specific to rural areas and focuses on opportunities to make improvements despite the geographic, fiscal, and other challenges that many rural communities face. It also includes several design concepts applicable to National Scenic and Historic Trails.



### **BEST PRACTICES IN TRAIL MAINTENANCE: A MANUAL BY THE OHIO RIVER GREENWAY**

This manual is intended for practical use by trail maintenance managers. It will also be useful for policy makers who are tasked with anticipating and planning for maintenance budget and personnel needs. The recommendations included in this manual were chosen, in part, to facilitate widespread adoption by other trail operators. They are cost-effective solutions that require minimal technological or financial commitments. This manual also provides guidance for trail construction and design where a maintenance issue can best be resolved by constructing new trail segments.



### **FHWA ACHIEVING MULTIMODAL NETWORKS: APPLYING DESIGN FLEXIBILITY AND REDUCING CONFLICTS**

This publication is resource for practitioners seeking to build multimodal transportation networks. It highlights ways that planners and designers can apply the design flexibility found in current national design guidance to address common roadway design challenges and barriers. It focuses on reducing multimodal conflicts and achieving connected networks so that walking and bicycling are safe, comfortable, and attractive options for people of all ages and abilities.



### **RAILS TO TRAILS CONSERVANCY (RTC) RAIL WITH TRAILS: BEST PRACTICES AND LESSONS LEARNED**

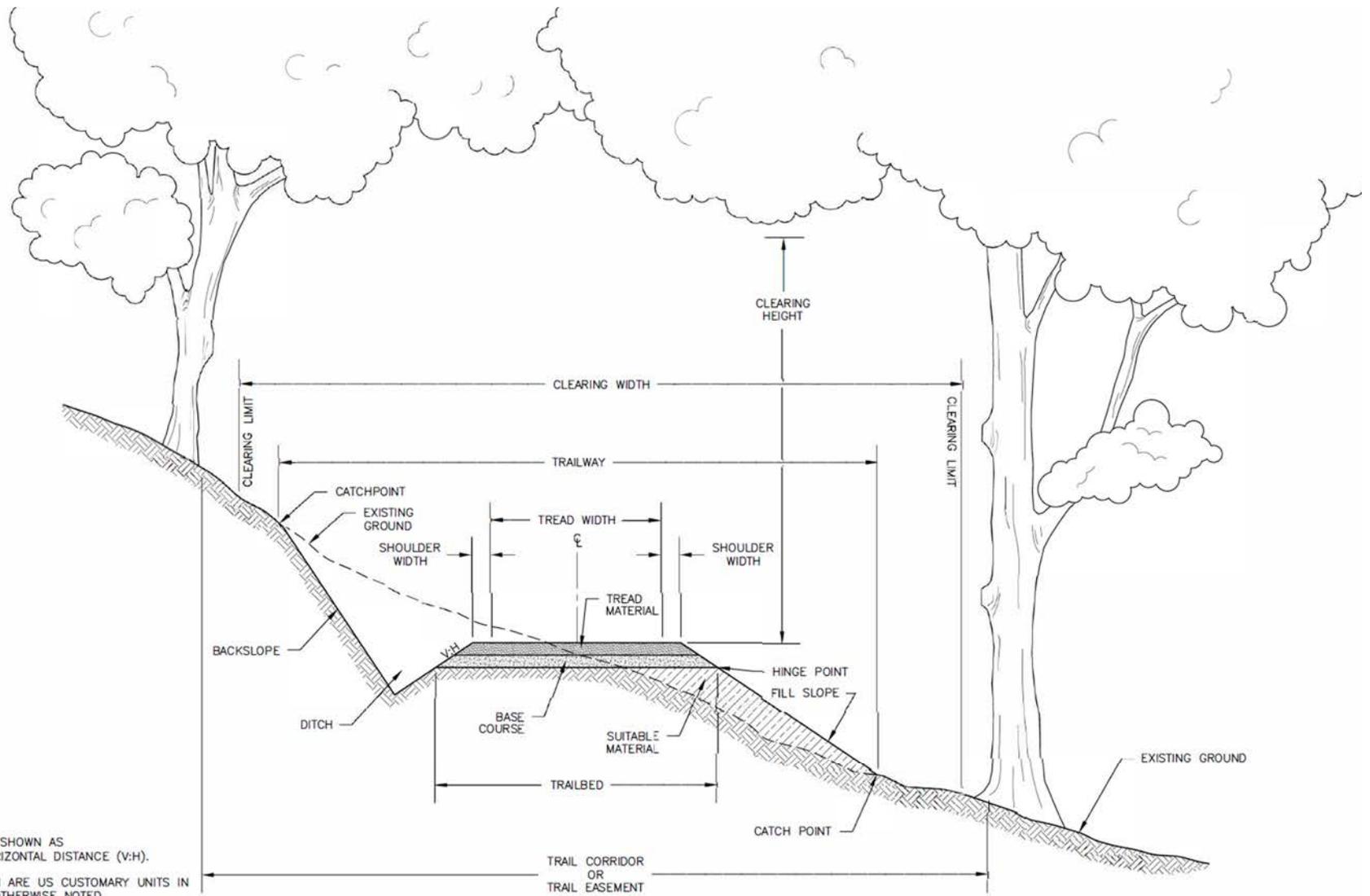
This updated USDOT Rails-with-Trails: Lessons Learned report documents how the state of the practice, perspectives, and context for rails-with-trails have evolved since the first report in 2002 and includes updated effective practices. Best practices are based on extensive research into existing and planned rails with-trails that involved interviews with railroad officials and trail managers; a literature review of previous rail-with-trail studies; a review of trail planning guidance documents; and input from various railroad and trail professionals.



### **ADA STANDARDS FOR ACCESSIBLE DESIGN**

This guide explains requirements in the current editions of the Americans with Disabilities Act (ADA) Standards issued by the Department of Justice (DOJ) and the Department of Transportation (DOT). It provides the scoping and technical requirements for new construction and alterations resulting from the adoption of revised 2010 Standards in the final rules for Title II and Title III.

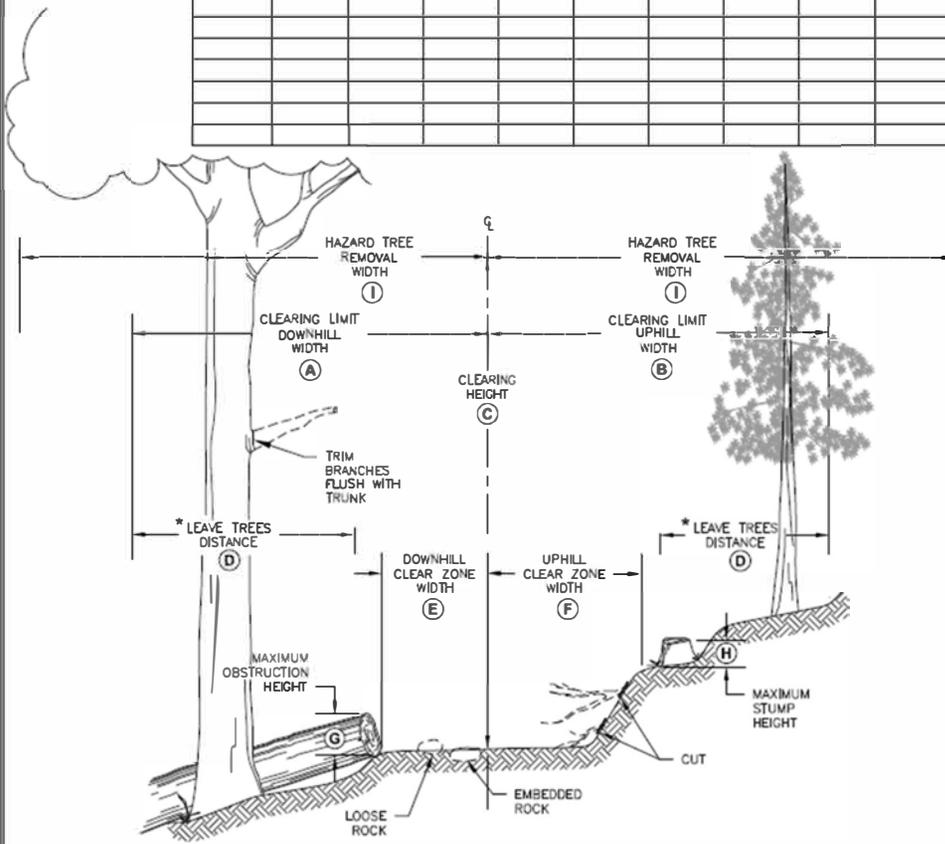




NOTES:  
 ALL SLOPES ARE SHOWN AS VERTICAL-TO-HORIZONTAL DISTANCE (V:H).  
 ALL UNITS SHOWN ARE US CUSTOMARY UNITS IN INCHES UNLESS OTHERWISE NOTED.

**CLEARING LIMITS - TREES AND LOGS**

TYPICAL ID	CLEARING METHOD	CLEARING LIMITS			* LEAVE TREES		CLEAR ZONE			STUMPS	HAZARD TREE	DISPOSAL METHOD	COMMENTS
		DOWNHILL WIDTH (A)	UPHILL WIDTH (B)	CLEARING HEIGHT (C)	DISTANCE (FEET) (D)	DIAMETER (INCHES)	DOWNHILL WIDTH (E)	UPHILL WIDTH (F)	OBSTRUCTION HEIGHT (G)	MAXIMUM HEIGHT (H)	REMOVAL WIDTH (I)		
	C												



**CLEARING METHOD**

CLEARING TYPE	CLEARING METHOD	COMMENTS
C1	NEW CONSTRUCTION	TREES, PRUNING, & BRUSH
C2	CLEARING LIMIT RESTORATION	TREES, PRUNING, LOGS, BRUSH & MAINTENANCE
C3	TRAIL OPENING	LOGGING OUT, LOOSE ROCK & ORAINAGE CLEARING
C4	HAZARD TREE REMOVAL	ALONG TRAIL CORRIDOR
C5	HAZARD TREE REMOVAL	INDIVIDUAL (AS MARKED)
C6	LOOSE ROCK & ROOT REMOVAL	
C7		

LEAVE TREES: LEAVE TREES SHOULD BE LIVE, SOUND & UNDATED WITH UNCOMPROMISED ROOT SYSTEMS.

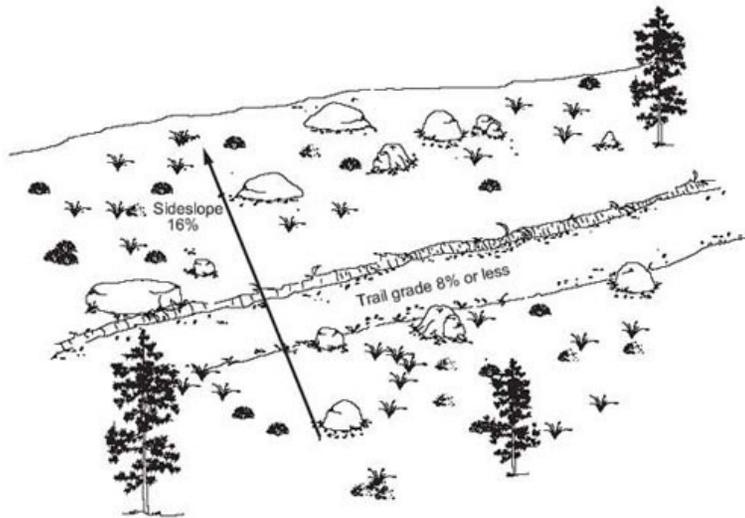
HAZARD TREES: HAZARD TREES ARE TREES THAT ARE STANDING OR LEANING DEAD TREES LARGER THAN 8 INCHES IN DIAMETER AND GREATER THAN 90 FEET IN HEIGHT.

**DISPOSAL METHOD**

DISPOSAL TYPE	DISPOSAL METHOD	COMMENTS
D1	LOP AND SCATTER OUTSIDE TRAILWAY	
D2	LOP AND SCATTER ON FILL SLOPE	
D3	PILE AND BURN	
D4	CHIP	
D5	HAUL TO DISPOSAL SITE	
D6		

## TRAIL ALIGNMENT

THE HALF RULE STATES THAT THE TRAIL GRADE SHOULD BE NO MORE THAN HALF THE SIDESLOPE GRADE (FIGURE BELOW). THIS RULE SHOULD BE THE FIRST INDICATOR FOR TRAIL ALIGNMENT. IF THE SITE HAS A 16% SIDESLOPE, TRAIL RUNNING GRADE SHOULD NOT EXCEED 8%, IF THE GRADE IS ANY STEEPER IT WILL BE A FALL LINE TRAIL. RUNNING GRADE SHOULDN'T EXCEED 12%.

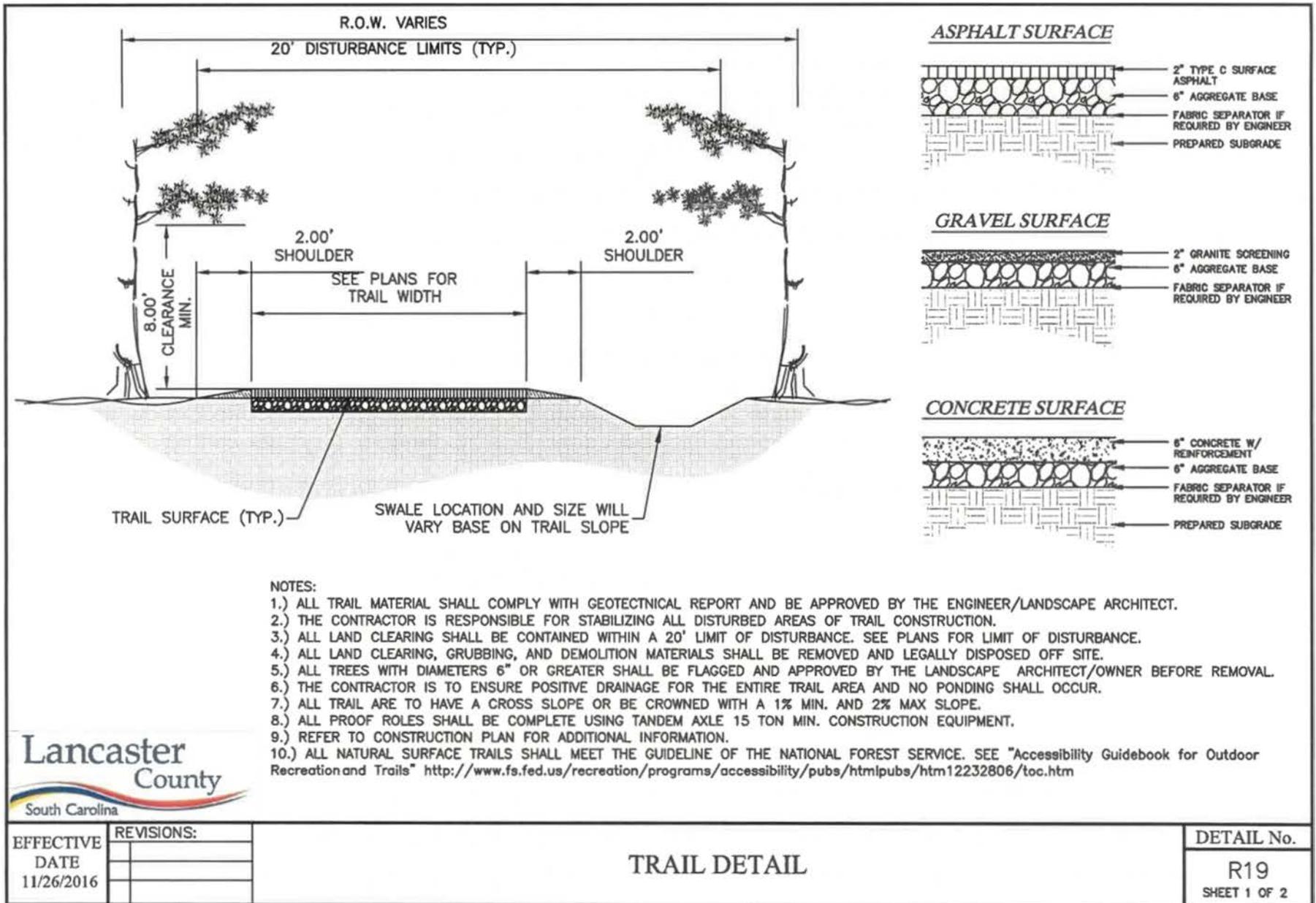


TRAIL SLOPE ALIGNMENT (TSA) WITH LOW TSA'S MORE DIRECTLY ASCEND SLOPES AND THEIR ADJACENT SIDE-SLOPES ARE RELATIVELY FLAT IN REFERENCE TO THE RUNNING GRADE. SUCH ALIGNMENTS ARE HIGHLY SUSCEPTIBLE TO DEGRADATION DUE TO TRAFFIC DISPLACES OR COMPACTS SOILS, INCISING THE TREAD, WHICH TRANSPORTS WHAT CONTRIBUTED TO EROSION IN SLOPING TERRAIN AND MUDDINESS IN FLAT TERRAIN.

Trail Slope Alignment (TSA)	Degradation Potential	Trail Profile
<b>Fall-aligned Trails</b>		
0-22°	Very High – tread drainage rarely possible; erosion, widening, & muddiness probable	
23-45°	High – tread drainage is often difficult; erosion, widening, & muddiness are likely	
<b>Side-hill Trails</b>		
46-68°	Low – tread drainage is possible; low potential for problems	
69-90°	Very Low – tread drainage is easy; very low potential for problems	

MARION & WHIMPY 2016 CITATION

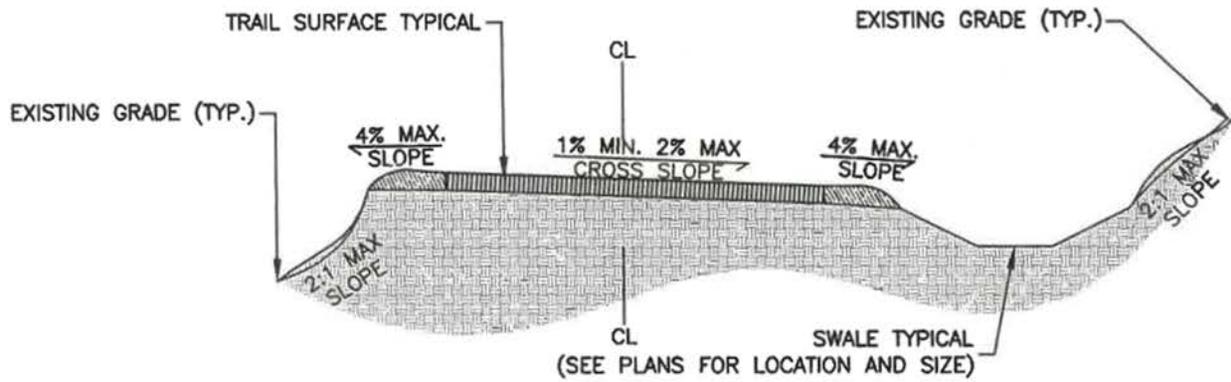




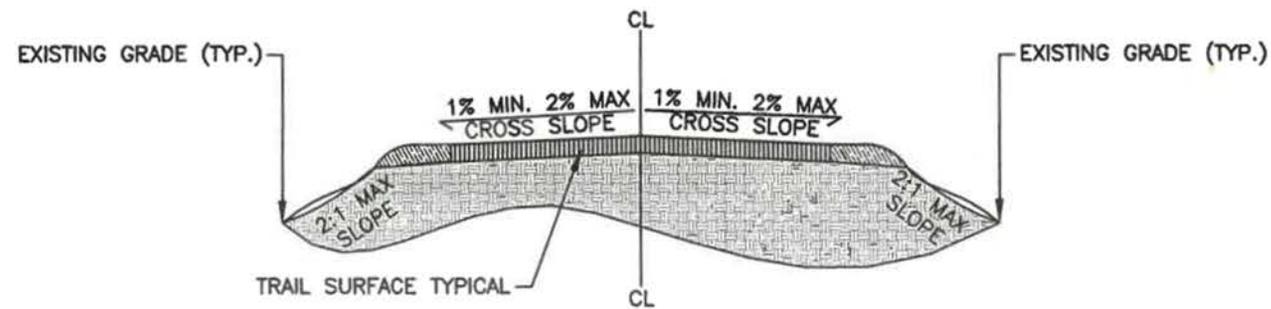
EFFECTIVE DATE 11/26/2016	REVISIONS:

**TRAIL DETAIL**

DETAIL No.
R19
SHEET 1 OF 2



**SUPER ELEVATED TRAIL**



**CROWNED TRAIL**

NOTES:  
 1.) SEE PLANS FOR DETERMINATION FOR TRAIL SLOPES.  
 2.) SIDE SLOPE USED TO TIE INTO EXISTING GRADE SHALL NOT EXCEED 2:1.

**TRAIL WIDTH REQUIREMENTS**

TRAIL TYPE	TRAIL WIDTH
SHARED USE TRAIL	10' MIN. - 12' PREFERRED (2' MIN. SHOULDER ON BOTH SIDES OF TRAIL)
FOOTPATH HIKING	3' MIN. - 6' PREFERRED
BOARDWALKS	8' MIN. (UP TO 14' IF EMERGENCY VEHICLES IS REQUIRED)
EQUESTRIAN	4' MIN. - 6' PREFERRED (CAN BE ADDED TO ONE SIDE OF SHARED USE TRAIL)

NOTE: IN HIGH USE ZONES THE COUNTY RESERVES THE RIGHT TO INCREASE THE WIDTH OF SHARED USE TRAILS UP TO 14'



EFFECTIVE DATE 11/26/2016	REVISIONS:
	1 6/11/2018

**TRAIL DETAIL**

DETAIL No. <b>R19</b> SHEET 2 OF 2
--

Accessibility Natural Surface Trail Standards

**7.4.1 Surface.** The trail tread surface, including resting intervals and passing spaces, shall be both firm and stable.

**7.4.2 Clear Tread Width.** The clear tread width of the trail shall be at least 36 inches (915 mm).  
 EXCEPTION: Where a condition for an exception prevents achieving the required width, the clear tread width may be reduced to 32 inches (815 mm) minimum. If the condition for an exception prevents achieving the reduced width of 32 inches, comply to the extent practicable.

**7.4.3 Slope.** Trail running slopes (grades) and cross slopes shall comply with sections 7.4.3.1 and 7.4.3.2.

**7.4.3.1 Running Slope (Grade).** The running slope (grade) of trail segments shall comply with this section and shall be consistent over the distances cited.

- Trail running slope (grade) of up to 1:20 (5 percent) is permitted for any distance.
- The running slope of any segment of a trail shall not be steeper than 1:8 (12 %).
- No more than 30 percent of the total trail length may exceed a running slope (grade) of 1:12 (8.33 %).
- Where the running slope (grade) of a segment of a trail is steeper than 1:20 (5 %), the maximum length of the segment shall be in accordance with the table below.

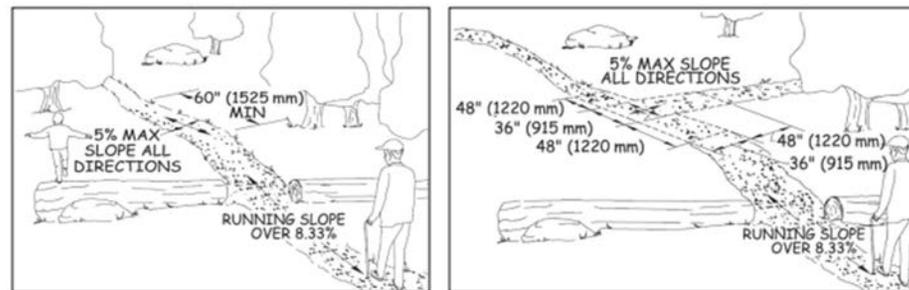
7.4.3.1, and a resting interval complying with 7.4.4 shall be provided at each end of the segment.

**Trail Running Slope (Grade) and Resting Intervals**

Running Slope of Trail Segment		Maximum Length of Segment Between Resting Intervals
Steeper Than	But Not Steeper Than	
1:20 (5 percent)	1:12 (8.33 percent)	200 feet (61 m)
1:12 (8.33 percent)	1:10 (10 percent)	30 feet (9 m)
1:10 (10 percent)	1:8 (12 percent)	10 feet (3050 mm)

**7.4.3.2 Cross Slope.** The cross slope shall not exceed 1:20 (5 percent). Where the surface is paved or is elevated above the natural ground, the cross slope shall not be steeper than 1:48 (2 percent).

**7.4.4 Resting Intervals.** Resting intervals shall comply with 7.4.4. Where the trail grade exceeds 1:20 (5 percent), resting intervals shall be provided as specified in Trail Running Slope and Resting interval table.



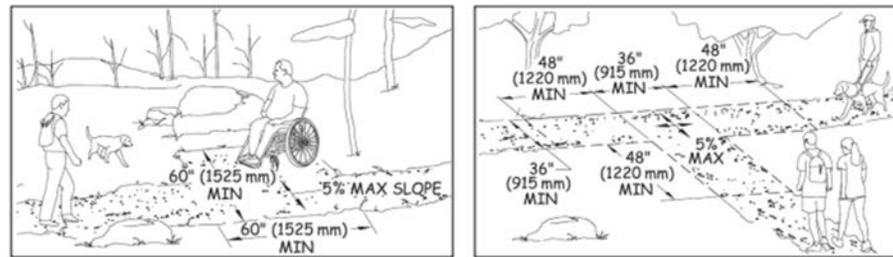
**7.4.4.1 Length.** The resting interval length shall be 60 inches (1525 mm) long minimum.

**7.4.4.2 Width.** Where resting intervals are provided within the trail tread, resting intervals shall be at least as wide as the widest segment of the trail tread leading to the resting interval. Where resting intervals are provided adjacent to the trail tread, the resting interval clear width shall be 36 inches (915 mm) minimum.

**7.4.4.3 Slope.** The slope of a resting interval shall not exceed 1:20 (5 percent) in any direction. Where the surface is paved or is elevated above the natural ground, the slope shall not be steeper than 1:48 (2 percent) in any direction.

**7.4.4.4 Turning Space.** Where resting intervals are provided adjacent to the trail tread, a turning space complying with ABAAS<sup>1</sup> section 304.3.2 shall be provided. Vertical alignment between the trail tread, turning space, and resting interval shall be nominally level. The trail tread, turning space, and resting interval may overlap.

**7.4.5 Passing Spaces.** Trails with a clear tread width less than 60 inches (1525 mm) shall provide passing spaces complying with 7.4.5 at intervals of 1000 feet (300 m) maximum. A passing space must also be provided at the end of any segment of trail that meets the requirements of 7.4, if the full length of the trail does not meet the requirements. Passing spaces and resting intervals may coincide or overlap.



**7.4.5.1 Size.** The passing space shall be either:

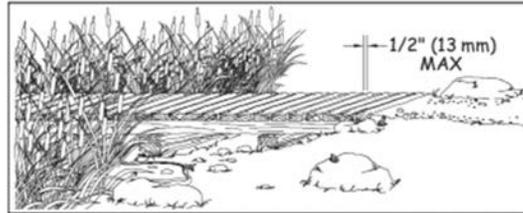
- A space 60 inches (1525 mm) minimum by 60 inches (1525 mm) minimum;
- The intersection of two trails providing a T-shaped space complying with ABAAS section 304.3.2<sup>2</sup> where the base and the arms of the T-shaped space extend 48 inches (1220 mm) minimum beyond the intersection. Vertical alignment at the intersection of the trails that form the T-shaped space shall be nominally level.

**7.4.5.2 Slope.** The cross slope of a passing space shall not exceed 1:20 (5 percent) in any direction.

**7.4.5.3 Non-complying Segment Ends.** Where a segment of the trail does not comply with 7.4, a passing space shall be located at the end of each adjacent trail segment that does comply with 7.4.

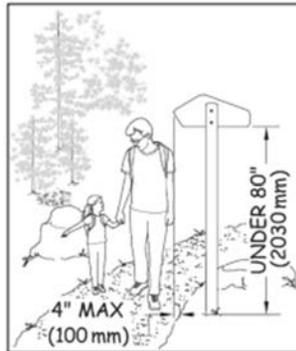
**7.4.6 Tread Obstacles.** Tread obstacles on trails shall not exceed 2 inches (50 mm) in height measured vertically to the highest point. Where the trail surface is paved or is elevated above the natural ground, tread obstacles shall not exceed ½ inch (13 mm) in height measured vertically to the highest point.

**7.4.7 Openings.** Openings in trail tread surfaces, trail resting spaces, and trail passing spaces shall be small enough to prevent passage of a 1/2 inch- (13 mm-) diameter sphere. Where possible, elongated openings should be placed perpendicular, or as close to perpendicular as possible, to the dominant direction of travel.



**Exception:** Where openings that do not permit the passage of a 1/2 inch (6.4 mm) sphere cannot be provided due to a condition for an exception, openings that do not permit passage of a 3/4 inch (19 mm) sphere shall be permitted.

**7.4.8 Protruding Objects.** Constructed features, including signs, shall not extend into the trail tread more than 4 inches (100 mm) between 27 inches (685 mm) and 80 inches (2030 mm) above the surface of the trail.



**7.4.9 Trail Facilities.** Where provided on trails, facilities shall comply with the applicable provisions of the FSORAG<sup>3</sup>. ORARs<sup>4</sup> are not required at or between facilities on trails.

**Exception:** When the surface of the required clear ground space for trail facilities is not paved or is not elevated above the natural ground, slopes not steeper than 1:20 (5 percent) shall be permitted where necessary for drainage.

**7.4.10 Trailheads.** Trailheads shall comply with 7.4.10.

**7.4.10.1 Outdoor Constructed Features.** Where provided within trailheads each outdoor constructed features such as parking spaces, toilets, or camp sites shall comply with the applicable portions of the FSORAG and ABAAS.

**7.4.10.2 Outdoor Recreation Access Routes (ORARs).** At least one outdoor recreation access route complying with FSORAG section 2.0 shall connect the following places at trailheads:

- Accessible parking spaces or other arrival point;
- Starting point of the trail; and
- Accessible outdoor constructed features, elements, spaces, and facilities within the trailhead.

**Exception 1.** In alterations to existing trailheads, where a condition for exception prohibits compliance with a technical provision, the ORAR shall comply with FSORAG 2.0 to maximum extent practicable.

**Exception 2.** Where elements, spaces, or outdoor constructed features are altered at trailheads but the circulation path is not altered, an outdoor recreation access route shall not be required.

**7.4.11 Trailhead Signs.** Where new trailhead information signs are provided at trailheads on newly constructed or altered trails, they shall comply with 7.4.11.

**7.4.11.1 Clear Space.** Trailhead signs shall be located centered at the back of a 30- by 48-inch (760- by 1,220-millimeter) minimum clear floor or ground space. The clear space shall not overlap the trail width but may overlap a resting space or passing space. The slope of the clear space shall not exceed 1:20 (5 percent) in any direction.

**7.4.11.2 Sign Contents.** Where new trail information signs are provided at trailheads on newly constructed or altered trails, regardless of whether the trail is accessible, the signs shall include at minimum the following information:

- Length of the trail or trail segment
- Surface type
- Typical and minimum tread width
- Typical and maximum running slope
- Typical and maximum cross slope
- A statement that the posted information reflects the condition of the trail when it was constructed or assessed, including the date of the construction or assessment

Where more extensive trail information is provided (e.g., an aerial map of the trail and related facilities), the location of specific trail features and obstacles that do not comply with the technical provisions in 7.4 should be identified and a profile of the trail grade should be included.

**7.4.11.3 Reach Ranges.** If materials need to be obtained from or manipulated on a sign or kiosk, the sign or kiosk shall be designed to meet the reach ranges in section 308 of the ABAAS.

**7.4.12 Gates and Barriers.** Where gates or barriers are constructed to control access to trails, gates and barriers shall comply with 7.4.12. 7.4.12.1 Clear Width. Gate openings and openings in barriers for hiker passage shall provide a clear width of 36 inches (915 mm), complying with ODAAG, section 1017.3 Clear Tread Width. 7.4.12.2 Gate Hardware. Gate hardware shall comply with operable controls requirements in ABAAS section 309.4 and 404.2.7.

<sup>1</sup> Architectural Barriers Act Accessibility Standards

<sup>2</sup> 304.3.2 T-Shaped Space. The turning space shall be a T-shaped space within a 60 inch (1525 mm) square minimum with arms and base 36 inches (915 mm) wide minimum. Each arm of the T shall be clear of obstructions 12 inches (305 mm) minimum in each direction and the base shall be clear of obstructions 24 inches (610 mm) minimum. The space shall be permitted to include knee and toe clearance complying with 306 only at the end of either the base or one arm.

<sup>3</sup> Forest Service Outdoor Recreation Accessibility Guidelines

<sup>4</sup> Outdoor Recreation Accessibility Routes

## RESOURCES

### **AASHTO GUIDE FOR THE DEVELOPMENT OF BICYCLE FACILITIES**

[https://nacto.org/wp-content/uploads/2015/04/AASHTO\\_Bicycle-Facilities-Guide\\_2012-toc.pdf](https://nacto.org/wp-content/uploads/2015/04/AASHTO_Bicycle-Facilities-Guide_2012-toc.pdf)

### **AASHTO GUIDE FOR THE PLANNING, DESIGN AND OPERATION OF PEDESTRIAN FACILITIES**

[https://onlinepubs.trb.org/onlinepubs/nchrp/docs/NCHRP20-07\(263\)\\_FR.pdf](https://onlinepubs.trb.org/onlinepubs/nchrp/docs/NCHRP20-07(263)_FR.pdf)

### **MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES (MUTCD)**

<https://mutcd.fhwa.dot.gov/>

### **SCDOT ROADWAY DESIGN MANUAL**

[https://www.scdot.org/business/pdf/roadway/2021\\_SCDOT\\_Roadway\\_Design\\_Manual.pdf](https://www.scdot.org/business/pdf/roadway/2021_SCDOT_Roadway_Design_Manual.pdf)

### **SCDOT COMPLETE STREETS IMPLEMENTATION GUIDANCE**

<http://info2.scdot.org/SCDOTPress/PublishingImages/DD%2028%20Complete%20Streets.pdf>

### **PENNSYLVANIA TRAIL DESIGN & DEVELOPMENT PRINCIPLES: GUIDELINE FOR SUSTAINABLE, NON-MOTORIZED TRAILS**

<https://www.americantrails.org/resources/pennsylvania-trail-design-development-principles>

### **NACTO URBAN STREETS DESIGN GUIDE**

<https://nacto.org/publication/urban-street-design-guide/>

### **NACTO URBAN STREET STORMWATER GUIDE**

<https://nacto.org/publication/urban-street-stormwater-guide/>

### **FHWA SMALL TOWN & RURAL MULTIMODAL NETWORKS**

[https://www.fhwa.dot.gov/environment/bicycle\\_pedestrian/publications/small\\_towns/](https://www.fhwa.dot.gov/environment/bicycle_pedestrian/publications/small_towns/)

### **BEST PRACTICES IN TRAIL MAINTENANCE: A MANUAL BY THE OHIO RIVER GREENWAY**

<https://www.railstotrails.org/resourcehandler.ashx?name=best-practices-in-trail-maintenance-a-manual-by-the-ohio-river-greenway&id=21221&fileName=Best%20Practices%20in%20Trail%20Maintenance.pdf>

### **FHWA ACHIEVING MULTIMODAL NETWORKS: APPLYING DESIGN FLEXIBILITY & REDUCING CONFLICTS**

[https://www.fhwa.dot.gov/environment/recreational\\_trails/publications/rwt2021/](https://www.fhwa.dot.gov/environment/recreational_trails/publications/rwt2021/)

### **RTC RAIL WITH TRAILS: BEST PRACTICES AND LESSONS LEARNED**

[https://www.fhwa.dot.gov/environment/recreational\\_trails/publications/rwt2021/](https://www.fhwa.dot.gov/environment/recreational_trails/publications/rwt2021/)

### **ADA STANDARDS FOR ACCESSIBLE DESIGN**

[https://www.ada.gov/2010ADAstandards\\_index.htm](https://www.ada.gov/2010ADAstandards_index.htm)

# APPENDIX B: DETAILED COST INFORMATION

## Asphalt Planning-level Estimates



Prepared By: JAP Date: 5/16/2023  
 Checked By: AJH Date: 5/16/2023  
 McAdams Project No: FTM-22001



Prepared By: JAP Date: 5/16/2023  
 Checked By: AJH Date: 5/16/2023  
 McAdams Project No: FTM-22001

### Fort Mill Trail Master Plan

Project Location: Fort Mill, SC  
 Project Description: Priority Projects  
 Client: Town of Fort Mill  
 Client Project No.

#### ENGINEER'S OPINION OF PROBABLE COST OF CONSTRUCTION - Master Plan

##### OVERALL SUMMARY

Segment	Segment Length (mi.)	Cost
1- HARRIS RD SIDEPATH	1.36	\$2,347,000
2- HARRIS ST PARK GREENWAY	0.58	\$1,814,000
3- OLD NATION RD SIDEPATH	2.88	\$4,602,000
4- TOM HALL ST SIDEPATH	1.45	\$2,379,000
RIVERVIEW ELEM CONNECTION -ALT1	0.25	\$508,000
RIVERVIEW ELEM CONNECTION -ALT2	0.40	\$2,761,000

### Fort Mill Trail Master Plan

Project Location: Fort Mill, SC  
 Project Description: Priority Projects  
 Client: Town of Fort Mill  
 Client Project No.

#### ENGINEER'S OPINION OF PROBABLE COST OF CONSTRUCTION - Master Plan

HARRIS RD SIDEPATH						
Section	Item Code	Item Description	Quantity	Unit	Unit Price	Cost
800	0000100000-N	MOBILIZATION	1	LS	\$ 80,000.00	\$ 80,000.00
801	0000400000-N	CONSTRUCTION SURVEYING	1	LS	\$ 25,000.00	\$ 25,000.00
SP	0241000000-E	GEOTEXTILE SEPARATOR FABRIC	8790	SY	\$ 5.00	\$ 43,950.00
520	1121000000-E	AGGREGATE BASE COURSE	3260	TON	\$ 45.00	\$ 146,700.00
610	1519000000-E	ASPHALT CONC SURFACE COURSE, TYPE S9.58	910	TON	\$ 150.00	\$ 136,500.00
620	1575000000-E	ASPHALT BINDER FOR PLANT MIX	55	TON	\$ 750.00	\$ 41,250.00
SP		EROSION CONTROL	1	LS	\$ 180,000.00	\$ 180,000.00
SP		TEMPORARY TRAFFIC CONTROL	1	LS	\$ 150,000.00	\$ 150,000.00
SP		DRIVEWAY CROSSING	11	EA	\$ 12,500.00	\$ 137,500.00
SP		MINOR ROAD CROSSING (UN-SIGNALIZED)	9	EA	\$ 15,000.00	\$ 135,000.00
SP		EXISTING SIGNALIZED INTERSECTION CROSSING	1	EA	\$ 30,000.00	\$ 30,000.00
SP		MID-BLOCK CROSSING (RRFB)	1	EA	\$ 30,000.00	\$ 30,000.00
SP		PREFABRICATED PEDESTRIAN BRIDGE	40	LF	\$ 3,500.00	\$ 140,000.00
SP		COMPREHENSIVE GRADING, HARRIS RD SIDEPATH	1	LS	\$ 400,000.00	\$ 400,000.00

**SUBTOTAL \$1,675,900.00**

**CONTINGENCY @ 40% \$670,360.00**

**CONSTRUCTION COST SAY \$2,347,000**

##### Notes:

1. Cost opinion does not include costs for easement or ROW acquisition.
2. Cost opinion does not include engineering, geotech, design survey, or construction administration.
3. Cost opinion does not include cost for private utility relocations.
4. Unit costs used in this cost opinion are representative of typical market costs as best known to the Consultant as of the date of this estimate, and do not account for inflationary cost escalation.
5. Quantities used in this cost opinion are approximations based on Master plan alignments by McAdams dated May 2023 and are subject to revision prior to bid.
6. The Engineer has no control over the cost of labor, materials, or equipment, or over the Contractor's methods of determining prices or over competitive bidding or market conditions. Opinions of probable costs, as provided here, are made on the basis of the Engineer's experience and qualifications and represent the Engineer's judgment as a design professional familiar with the construction industry. The Engineer cannot and does not guarantee that proposals, bids, or actual construction costs will not vary from opinions of probable cost prepared for the Owner.

### Fort Mill Trail Master Plan

Project Location: Fort Mill, SC  
 Project Description: Priority Projects  
 Client: Town of Fort Mill  
 Client Project No.

#### ENGINEER'S OPINION OF PROBABLE COST OF CONSTRUCTION - Master Plan

HARRIS ST PARK GREENWAY						
Section	Item Code	Item Description	Quantity	Unit	Unit Price	Cost
800	0000100000-N	MOBILIZATION	1	LS	\$ 70,000.00	\$ 70,000.00
801	0000400000-N	CONSTRUCTION SURVEYING	1	LS	\$ 15,000.00	\$ 15,000.00
SP	0241000000-E	GEOTEXTILE SEPARATOR FABRIC	2560	SY	\$ 5.00	\$ 12,800.00
520	1121000000-E	AGGREGATE BASE COURSE	1380	TON	\$ 45.00	\$ 62,100.00
610	1519000000-E	ASPHALT CONC SURFACE COURSE, TYPE 59.5B	270	TON	\$ 150.00	\$ 40,500.00
620	1575000000-E	ASPHALT BINDER FOR PLANT MIX	20	TON	\$ 750.00	\$ 15,000.00
848	2591000000-E	6" CONCRETE	1060	SY	\$ 80.00	\$ 84,800.00
SP		EROSION CONTROL	1	LS	\$ 80,000.00	\$ 80,000.00
SP		TEMPORARY TRAFFIC CONTROL	1	LS	\$ 15,000.00	\$ 15,000.00
SP		PREFABRICATED PEDESTRIAN BRIDGE	200	LF	\$ 3,500.00	\$ 700,000.00
SP		COMPREHENSIVE GRADING, HARRIS ST GREENWAY	1	LS	\$ 200,000.00	\$ 200,000.00

**SUBTOTAL \$1,295,200.00**

**CONTINGENCY @ 40% \$518,080.00**

**CONSTRUCTION COST SAY \$1,814,000**

**Notes:**

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### Fort Mill Trail Master Plan

Project Location: Fort Mill, SC  
 Project Description: Priority Projects  
 Client: Town of Fort Mill  
 Client Project No.

#### ENGINEER'S OPINION OF PROBABLE COST OF CONSTRUCTION - Master Plan

OLD NATION RD SIDEPATH						
Section	Item Code	Item Description	Quantity	Unit	Unit Price	Cost
800	0000100000-N	MOBILIZATION	1	LS	\$ 160,000.00	\$ 160,000.00
801	0000400000-N	CONSTRUCTION SURVEYING	1	LS	\$ 55,000.00	\$ 55,000.00
SP	0241000000-E	GEOTEXTILE SEPARATOR FABRIC	18590	SY	\$ 5.00	\$ 92,950.00
520	1121000000-E	AGGREGATE BASE COURSE	6600	TON	\$ 45.00	\$ 297,000.00
610	1519000000-E	ASPHALT CONC SURFACE COURSE, TYPE 59.5B	1930	TON	\$ 150.00	\$ 289,500.00
620	1575000000-E	ASPHALT BINDER FOR PLANT MIX	120	TON	\$ 750.00	\$ 90,000.00
SP		EROSION CONTROL	1	LS	\$ 390,000.00	\$ 390,000.00
SP		TEMPORARY TRAFFIC CONTROL	1	LS	\$ 200,000.00	\$ 200,000.00
SP		DRIVEWAY CROSSING	29	EA	\$ 12,500.00	\$ 362,500.00
SP		MINOR ROAD CROSSING (UN-SIGNALIZED)	8	EA	\$ 15,000.00	\$ 120,000.00
SP		PREFABRICATED PEDESTRIAN BRIDGE	180	LF	\$ 3,500.00	\$ 630,000.00
SP		COMPREHENSIVE GRADING, OLD NATION RD SIDEPATH	1	LS	\$ 600,000.00	\$ 600,000.00

**SUBTOTAL \$3,286,950.00**

**CONTINGENCY @ 40% \$1,314,780.00**

**CONSTRUCTION COST SAY \$4,602,000**

**Notes:**

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**Fort Mill Trail Master Plan**

Project Location: Fort Mill, SC  
 Project Description: Priority Projects  
 Client: Town of Fort Mill  
 Client Project No.

**ENGINEER'S OPINION OF PROBABLE COST OF CONSTRUCTION - Master Plan**

TOM HALL ST SIDEPATH						
Section	Item Code	Item Description	Quantity	Unit	Unit Price	Cost
800	0000100000-N	MOBILIZATION	1	LS	\$ 80,000.00	\$ 80,000.00
801	0000400000-N	CONSTRUCTION SURVEYING	1	LS	\$ 30,000.00	\$ 30,000.00
SP	0241000000-E	GEOTEXTILE SEPARATOR FABRIC	9420	SY	\$ 5.00	\$ 47,100.00
520	1121000000-E	AGGREGATE BASE COURSE	3490	TON	\$ 45.00	\$ 157,050.00
610	1519000000-E	ASPHALT CONC SURFACE COURSE, TYPE S9.5B	980	TON	\$ 150.00	\$ 147,000.00
620	1575000000-E	ASPHALT BINDER FOR PLANT MIX	60	TON	\$ 750.00	\$ 45,000.00
SP		EROSION CONTROL	1	LS	\$ 193,000.00	\$ 193,000.00
SP		TEMPORARY TRAFFIC CONTROL	1	LS	\$ 200,000.00	\$ 200,000.00
SP		DRIVEWAY CROSSING	16	EA	\$ 12,500.00	\$ 200,000.00
SP		MINOR ROAD CROSSING (UN-SIGNALIZED)	8	EA	\$ 15,000.00	\$ 120,000.00
SP		EXISTING SIGNALIZED INTERSECTION CROSSING	1	EA	\$ 30,000.00	\$ 30,000.00
SP		COMPREHENSIVE GRADING, TOM HALL ST SIDEPATH	1	LS	\$ 450,000.00	\$ 450,000.00

**SUBTOTAL \$1,699,150.00**  
**CONTINGENCY @ 40% \$679,660.00**  
**CONSTRUCTION COST SAY \$2,379,000**

**Notes:**

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**Fort Mill Trail Master Plan**

Project Location: Fort Mill, SC  
 Project Description: Priority Projects  
 Client: Town of Fort Mill  
 Client Project No.

**ENGINEER'S OPINION OF PROBABLE COST OF CONSTRUCTION - Master Plan**

RIVERVIEW ELEM CONN - ALT 1 (Segment east of the school)						
Section	Item Code	Item Description	Quantity	Unit	Unit Price	Cost
800	0000100000-N	MOBILIZATION	1	LS	\$ 20,000.00	\$ 20,000.00
801	0000400000-N	CONSTRUCTION SURVEYING	1	LS	\$ 5,000.00	\$ 5,000.00
SP	0241000000-E	GEOTEXTILE SEPARATOR FABRIC	1590	SY	\$ 5.00	\$ 7,950.00
520	1121000000-E	AGGREGATE BASE COURSE	620	TON	\$ 45.00	\$ 27,900.00
610	1519000000-E	ASPHALT CONC SURFACE COURSE, TYPE S9.5B	170	TON	\$ 150.00	\$ 25,500.00
620	1575000000-E	ASPHALT BINDER FOR PLANT MIX	15	TON	\$ 750.00	\$ 11,250.00
SP		EROSION CONTROL	1	LS	\$ 40,000.00	\$ 40,000.00
SP		COMPREHENSIVE GRADING, RIVERVIEW ELEMENTARY CONNECTION-ALT 1	1	LS	\$ 225,000.00	\$ 225,000.00

**SUBTOTAL \$362,600.00**  
**CONTINGENCY @ 40% \$145,040.00**  
**CONSTRUCTION COST SAY \$508,000**

**Notes:**

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**Fort Mill Trail Master Plan**

Project Location: Fort Mill, SC  
 Project Description: Priority Projects  
 Client: Town of Fort Mill  
 Client Project No.

**ENGINEER'S OPINION OF PROBABLE COST OF CONSTRUCTION - Master Plan**

RIVERVIEW ELEM CONN - ALT 2 (Segment west of the school)						
Section	Item Code	Item Description	Quantity	Unit	Unit Price	Cost
800	0000100000-N	MOBILIZATION	1	LS	\$ 100,000.00	\$ 100,000.00
801	0000400000-N	CONSTRUCTION SURVEYING	1	LS	\$ 10,000.00	\$ 10,000.00
SP	0241000000-E	GEOTEXTILE SEPARATOR FABRIC	1350	SY	\$ 5.00	\$ 6,750.00
520	1121000000-E	AGGREGATE BASE COURSE	530	TON	\$ 45.00	\$ 23,850.00
610	1519000000-E	ASPHALT CONC SURFACE COURSE, TYPE S9.58	140	TON	\$ 150.00	\$ 21,000.00
620	1575000000-E	ASPHALT BINDER FOR PLANT MIX	10	TON	\$ 750.00	\$ 7,500.00
SP		EROSION CONTROL	1	LS	\$ 53,000.00	\$ 53,000.00
SP		BOARDWALK (TIMBER PILES, CONCRETE DECK)	1000	LF	\$ 1,500.00	\$ 1,500,000.00
SP		COMPREHENSIVE GRADING, RIVERVIEW ELEMENTARY CONNECTION-ALT2	1	LS	\$ 250,000.00	\$ 250,000.00

**SUBTOTAL \$1,972,100.00**

**CONTINGENCY @ 40% \$788,840.00**

**CONSTRUCTION COST SAY \$2,761,000**

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# Concrete Sidewalk Planning-level Estimates



Prepared By: JAP Date: 5/16/2023  
 Checked By: AJH Date: 5/16/2023  
 McAdams Project No: FTM-22001



Prepared By: JAP Date: 5/16/2023  
 Checked By: AJH Date: 5/16/2023  
 McAdams Project No: FTM-22001

## Fort Mill Trail Master Plan

Project Location: Fort Mill, SC  
 Project Description: Priority Projects  
 Client: Town of Fort Mill  
 Client Project No.

### ENGINEER'S OPINION OF PROBABLE COST OF CONSTRUCTION - Master Plan

#### OVERALL SUMMARY

Segment	Segment Length (mi.)	Cost
1- HARRIS RD SIDEPATH	1.36	\$2,813,000
2- HARRIS ST PARK GREENWAY	0.58	\$1,814,000
3- OLD NATION RD SIDEPATH	2.88	\$4,972,000
4- TOM HALL ST SIDEPATH	1.45	\$2,933,700

## Fort Mill Trail Master Plan

Project Location: Fort Mill, SC  
 Project Description: Priority Projects  
 Client: Town of Fort Mill  
 Client Project No.

### ENGINEER'S OPINION OF PROBABLE COST OF CONSTRUCTION - Master Plan

HARRIS RD SIDEPATH						
Section	Item Code	Item Description	Quantity	Unit	Unit Price	Cost
800	0000100000-N	MOBILIZATION	1	LS	\$ 100,000.00	\$ 100,000.00
801	0000400000-N	CONSTRUCTION SURVEYING	1	LS	\$ 25,000.00	\$ 25,000.00
520	1121000000-E	AGGREGATE BASE COURSE	1600	TON	\$ 45.00	\$ 72,000.00
848	2591000000-E	6" CONCRETE SIDEPATH	7990	SY	\$ 80.00	\$ 639,200.00
SP		EROSION CONTROL	1	LS	\$ 180,000.00	\$ 180,000.00
SP		TEMPORARY TRAFFIC CONTROL	1	LS	\$ 150,000.00	\$ 150,000.00
SP		DRIVEWAY CROSSING	11	EA	\$ 12,500.00	\$ 137,500.00
SP		MINOR ROAD CROSSING (UN-SIGNALIZED)	9	EA	\$ 15,000.00	\$ 135,000.00
SP		EXISTING SIGNALIZED INTERSECTION CROSSING	1	EA	\$ 30,000.00	\$ 30,000.00
SP		PREFABRICATED PEDESTRIAN BRIDGE	40	LF	\$ 3,500.00	\$ 140,000.00
SP		COMPREHENSIVE GRADING, HARRIS RD SIDEPATH	1	LS	\$ 400,000.00	\$ 400,000.00

**SUBTOTAL \$2,008,700.00**

**CONTINGENCY @ 40% \$803,480.00**

**CONSTRUCTION COST SAY \$2,813,000**

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### Fort Mill Trail Master Plan

Project Location: Fort Mill, SC  
 Project Description: Priority Projects  
 Client: Town of Fort Mill  
 Client Project No.

#### ENGINEER'S OPINION OF PROBABLE COST OF CONSTRUCTION - Master Plan

HARRIS ST PARK GREENWAY						
Section	Item Code	Item Description	Quantity	Unit	Unit Price	Cost
800	0000100000-N	MOBILIZATION	1	LS	\$ 70,000.00	\$ 70,000.00
801	0000400000-N	CONSTRUCTION SURVEYING	1	LS	\$ 15,000.00	\$ 15,000.00
SP	0241000000-E	GEOTEXTILE SEPARATOR FABRIC	2560	SY	\$ 5.00	\$ 12,800.00
520	1121000000-E	AGGREGATE BASE COURSE	1380	TON	\$ 45.00	\$ 62,100.00
610	1519000000-E	ASPHALT CONC SURFACE COURSE, TYPE S9.5B	270	TON	\$ 150.00	\$ 40,500.00
620	1575000000-E	ASPHALT BINDER FOR PLANT MIX	20	TON	\$ 750.00	\$ 15,000.00
848	2591000000-E	6" CONCRETE	1060	SY	\$ 80.00	\$ 84,800.00
SP		EROSION CONTROL	1	LS	\$ 80,000.00	\$ 80,000.00
SP		TEMPORARY TRAFFIC CONTROL	1	LS	\$ 15,000.00	\$ 15,000.00
SP		PREFABRICATED PEDESTRIAN BRIDGE	200	LF	\$ 3,500.00	\$ 700,000.00
SP		COMPREHENSIVE GRADING, HARRIS ST GREENWAY	1	LS	\$ 200,000.00	\$ 200,000.00

**SUBTOTAL \$1,295,200.00**  
**CONTINGENCY @ 40% \$518,080.00**  
**CONSTRUCTION COST SAY \$1,814,000**

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### Fort Mill Trail Master Plan

Project Location: Fort Mill, SC  
 Project Description: Priority Projects  
 Client: Town of Fort Mill  
 Client Project No.

#### ENGINEER'S OPINION OF PROBABLE COST OF CONSTRUCTION - Master Plan

OLD NATION RD SIDEPATH						
Section	Item Code	Item Description	Quantity	Unit	Unit Price	Cost
800	0000100000-N	MOBILIZATION	1	LS	\$ 170,000.00	\$ 170,000.00
801	0000400000-N	CONSTRUCTION SURVEYING	1	LS	\$ 55,000.00	\$ 55,000.00
520	1121000000-E	AGGREGATE BASE COURSE	3370	TON	\$ 45.00	\$ 151,650.00
848	2591000000-E	6" CONCRETE SIDEPATH	16900	SY	\$ 80.00	\$ 1,352,000.00
SP		EROSION CONTROL	1	LS	\$ 390,000.00	\$ 390,000.00
SP		TEMPORARY TRAFFIC CONTROL	1	LS	\$ 200,000.00	\$ 200,000.00
SP		DRIVEWAY CROSSING	29	EA	\$ 12,500.00	\$ 362,500.00
SP		MINOR ROAD CROSSING (UN-SIGNALIZED)	8	EA	\$ 15,000.00	\$ 120,000.00
SP		OLD NATION RD (US-21 BUS) STEELE CREEK BRIDGE RETROFIT	1	LS	\$ 150,000.00	\$ 150,000.00
SP		COMPREHENSIVE GRADING, OLD NATION RD SIDEPATH	1	LS	\$ 600,000.00	\$ 600,000.00

**SUBTOTAL \$3,551,150.00**  
**CONTINGENCY @ 40% \$1,420,460.00**  
**CONSTRUCTION COST SAY \$4,972,000**

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**Fort Mill Trail Master Plan**

Project Location: Fort Mill, SC  
 Project Description: Priority Projects  
 Client: Town of Fort Mill  
 Client Project No.

**ENGINEER'S OPINION OF PROBABLE COST OF CONSTRUCTION - Master Plan**

TOM HALL ST SIDEPATH						
Section	Item Code	Item Description	Quantity	Unit	Unit Price	Cost
800	0000100000-N	MOBILIZATION	1	LS	\$ 110,000.00	\$ 110,000.00
801	0000400000-N	CONSTRUCTION SURVEYING	1	LS	\$ 30,000.00	\$ 30,000.00
520	1121000000-E	AGGREGATE BASE COURSE	1710	TON	\$ 45.00	\$ 76,950.00
848	2591000000-E	6" CONCRETE SIDEPATH	8570	SY	\$ 80.00	\$ 685,600.00
SP		EROSION CONTROL	1	LS	\$ 193,000.00	\$ 390,000.00
SP		TEMPORARY TRAFFIC CONTROL	1	LS	\$ 200,000.00	\$ 125,000.00
SP		DRIVEWAY CROSSING	16	EA	\$ 12,500.00	\$ 200,000.00
SP		MINOR ROAD CROSSING (UN-SIGNALIZED)	8	EA	\$ 15,000.00	\$ 120,000.00
SP		EXISTING SIGNALIZED INTERSECTION CROSSING	1	EA	\$ 30,000.00	\$ 30,000.00
SP		COMPREHENSIVE GRADING, TOM HALL ST SIDEPATH	1	LS	\$ 450,000.00	\$ 450,000.00

**SUBTOTAL \$2,095,550.00**

**CONTINGENCY @ 40% \$838,220.00**

**CONSTRUCTION COST SAY \$2,933,770**

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# APPENDIX C: ADDITIONAL COMMUNITY INVOLVEMENT INFORMATION

## Steering Committee Meeting #1



### AGENDA

- **Introductions**
- **Planning Process**
  - Project Overview
  - Project Schedule
  - Public Engagement Overview
- **Project Visioning**
  - Trail Master Plan Vision + Goals
- **Existing Conditions Review**
  - Demographics
  - Previous Plans Review
  - Mapping Analysis
- **Mapping Exercise**
  - Key Destinations
  - Network Opportunities + Challenges



Downtown Fort Mill



### PROJECT TEAM



**Kathryn Zeringue**  
Project Manager



**Andrew Hickling**  
Greenway Project Manager



**Haley Nafissi**  
Bicycle + Pedestrian Planner



**Christopher Normile**  
Bicycle + Pedestrian Planner



### TRAIL MASTER PLAN PURPOSE

#### Where do people want to go by foot or bike?

*Identify key locations that people want to walk or bike to via trails.*

#### How do we connect those places?

*Identify the facilities and standards to connect key locations.*

#### How do we build the network?

*Identify priority projects, costs, and funding opportunities.*

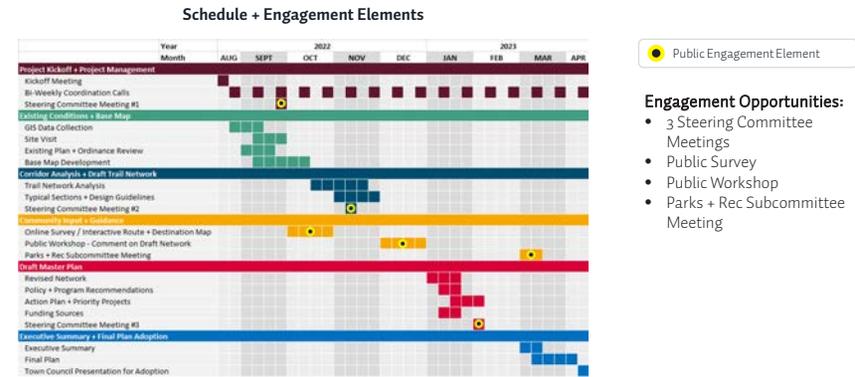
#### How do we build a trail-friendly community?

*Identify programs and policies to promote recreational opportunities and active transportation along trails.*



Trail Plan > **PROJECT SCHEDULE**

Trail Plan > **SCHEDULE + ENGAGEMENT**



- Engagement Opportunities:**
- 3 Steering Committee Meetings
  - Public Survey
  - Public Workshop
  - Parks + Rec Subcommittee Meeting

Trail Plan > **VISION + GOALS**

Trail Plan > **PUBLIC SURVEY**

**PROJECT VISIONING**

What does success look like for the Trail Master Plan?

*Share words or phrases that represent successful outcomes for the planning process and Fort Mill's future trail network.*



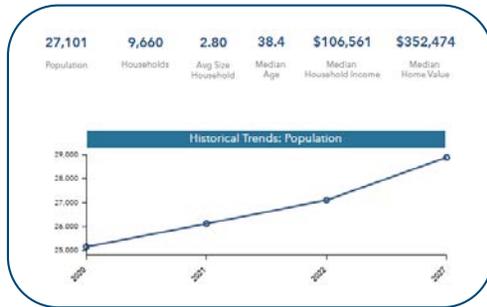
Founders Trail, Fort Mill

**DRAFT SURVEY REVIEW**



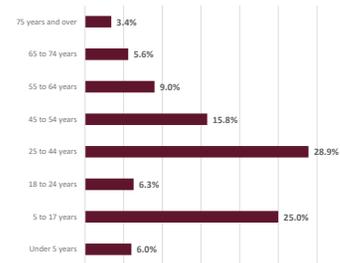
Harris St Park – Elisha Park Trail Connector

**TOWN OF FORT MILL - KEY INDICATORS**

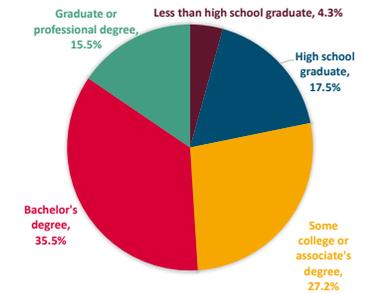


Source: The vintage of the data is 2022, ACS 2016-2020, 2027.

**AGE**

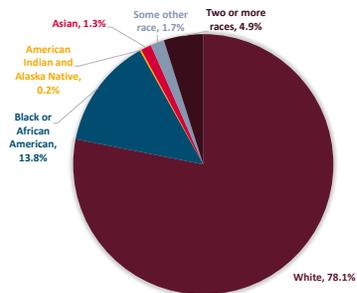


**EDUCATION**

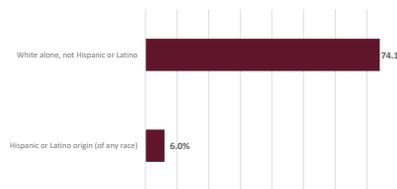


Source: ACS 2016-2020.

**RACE**

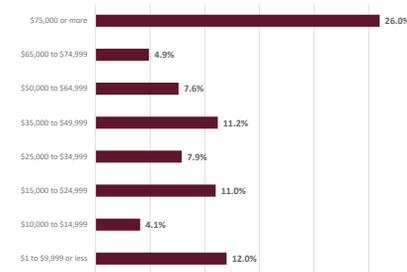


**ETHNICITY**



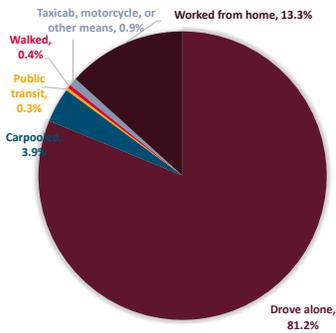
Source: ACS 2016-2020.

**HOUSEHOLD INCOME**

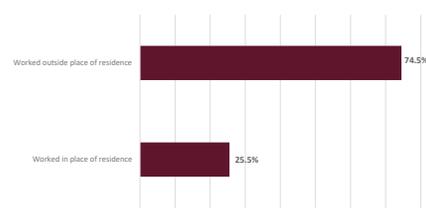


Source: ACS 2016-2020.

**COMMUTE MODE**

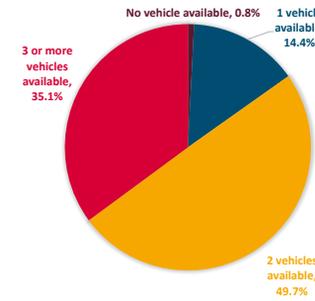


**LOCATION OF WORK**

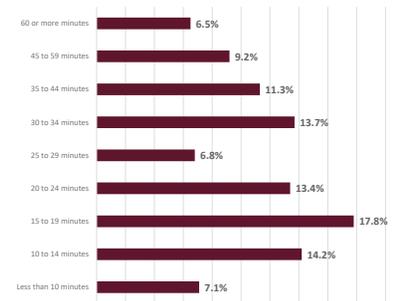


Source: ACS 2016-2020.

**VEHICLE ACCESS**



**COMMUTE TIME**



Source: ACS 2016-2020.

**LOCAL + REGIONAL PLANS**

- Carolina Thread Trail Master Plan for York County Communities, 2009
- RFATS Urbanized Area Transit Implementation Study, 2015
- Fort Mill Historic District Design Guidelines, 2016
- York Forward Comprehensive Plan, 2016
- RFATS Regional Bicycle + Pedestrian Connectivity Plan, 2016
- York County Multi-Jurisdictional Hazard Mitigation Plan, 2017
- RFATS 2045 Long Range Transportation Plan, 2017
- RFATS Collector Street Plan, 2017
- Catawba Regional Comprehensive Economic Development Strategy, 2020
- Town of Fort Mill 2040 Comprehensive Plan, 2020



Railroad crossing over Main St



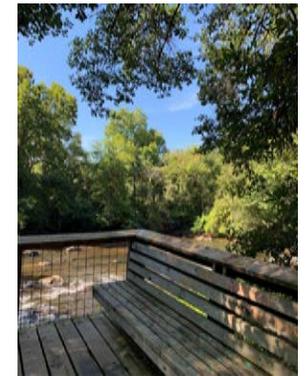
Piedmont Medical Center – Fort Mill



Anne Springs Close Greenway



Riverwalk, Rock Hill



Overlook on Founders Trail (Carolina Thread Trail)

Trail Plan > EXISTING CONDITIONS



Fort Mill High School



Kings Town Elementary School



Sidewalks, Steele St (near Steele St Park)



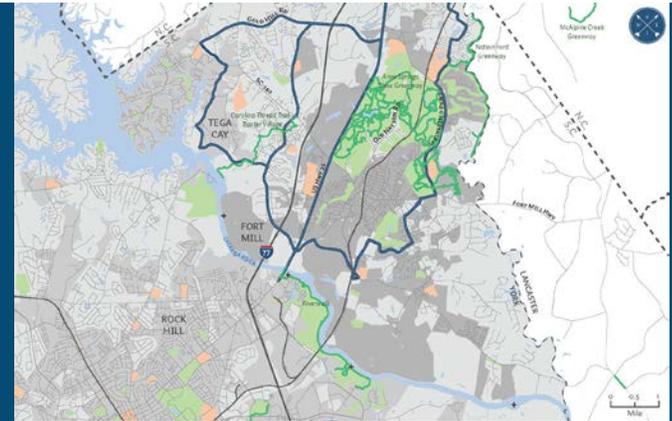
River Trail Elementary School



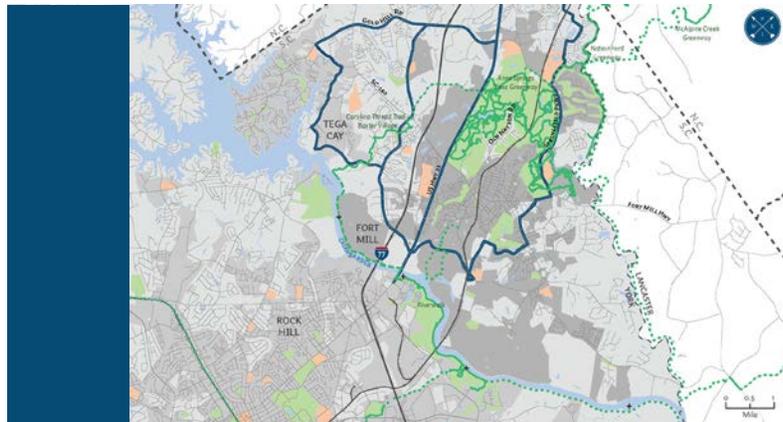
Crosswalk, Downtown Fort Mill



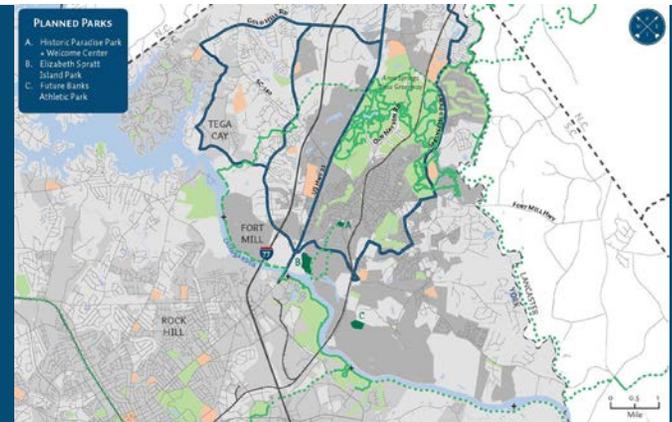
Existing Sidepath, Springfield Pkwy (Carolina Orchards)



**FORT MILL TRAIL MASTER PLAN**  
Existing Active Transportation Facilities

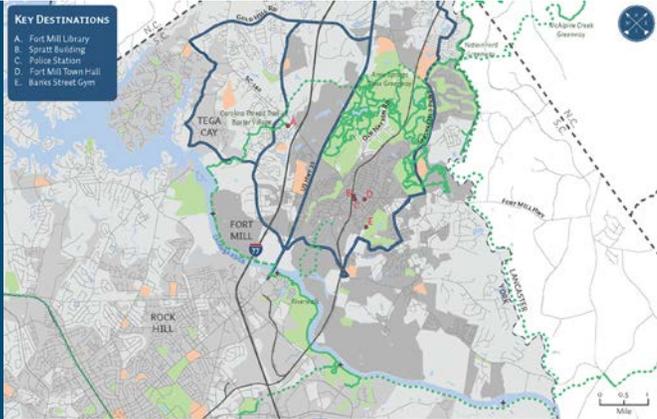


**FORT MILL TRAIL MASTER PLAN**  
Planned Active Transportation Facilities



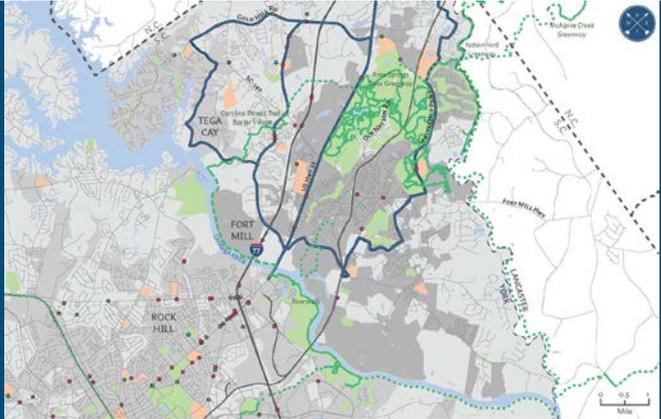
**FORT MILL TRAIL MASTER PLAN**  
Planned Parks

- KEY DESTINATIONS**
- A. Fort Mill Library
  - B. Spratt Building
  - C. Police Station
  - D. Fort Mill Town Hall
  - E. Banks Street Gym



**FORT MILL TRAIL MASTER PLAN**  
Key Destinations

- LEGEND**
- |                 |                       |                    |
|-----------------|-----------------------|--------------------|
| Blowway Access  | Greenway/Trail Status | School             |
| Key Destination | Complete              | Park               |
| Road            | In Progress           | Water              |
| Railroad        | Planned               | Fort Mill          |
| Bicycle Route   |                       | Other Jurisdiction |
|                 |                       | York County        |
|                 |                       | County Boundary    |



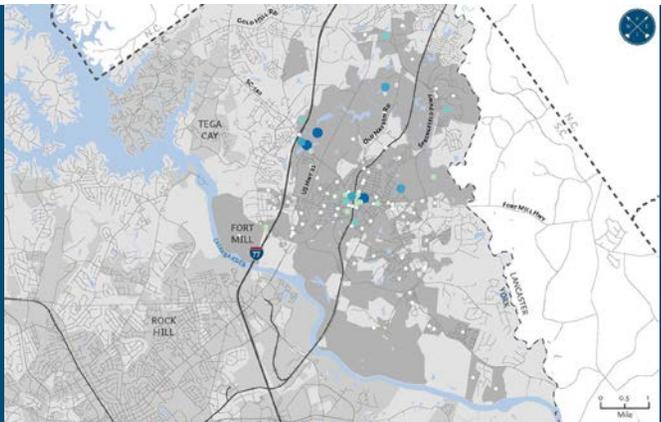
**FORT MILL TRAIL MASTER PLAN**  
Bicycle + Pedestrian Crashes (2016-2020)

- LEGEND**
- |                  |                       |                    |
|------------------|-----------------------|--------------------|
| Pedestrian Crash | Greenway/Trail Status | School             |
| Bicycle Crash    | Complete              | Park               |
| Road             | In Progress           | Water              |
| Railroad         | Planned               | Fort Mill          |
| Bicycle Route    |                       | Other Jurisdiction |
|                  |                       | York County        |
|                  |                       | County Boundary    |



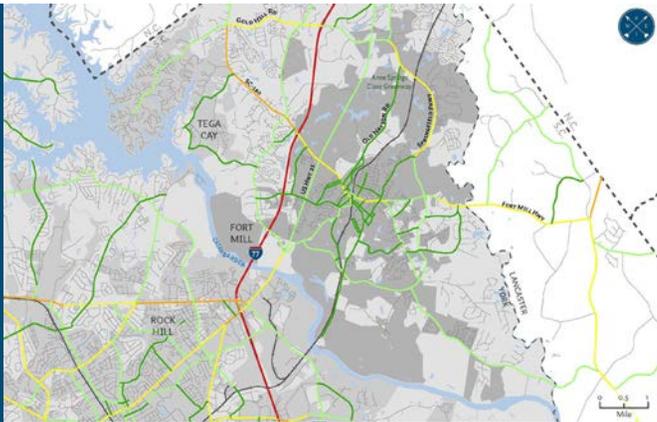
**FORT MILL TRAIL MASTER PLAN**  
Roadway Speed Limits

- LEGEND**
- |             |          |                    |
|-------------|----------|--------------------|
| Speed Limit | Road     | Water              |
| 20-25 mph   | Railroad | Fort Mill          |
| 30-35 mph   |          | Other Jurisdiction |
| 40-45 mph   |          | York County        |
| 50-55 mph   |          | County Boundary    |

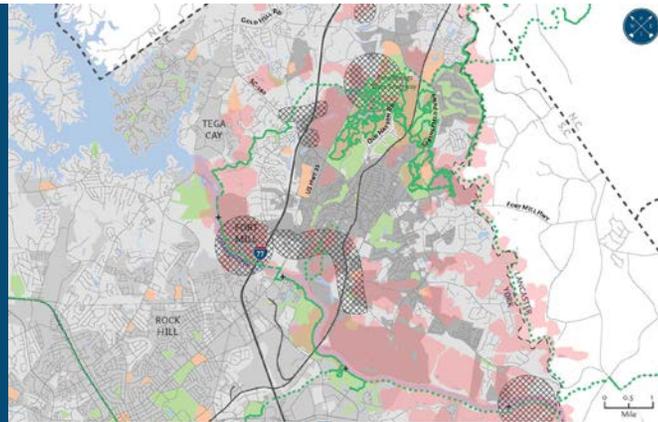


**FORT MILL TRAIL MASTER PLAN**  
Employment Density (2019 - All Jobs)

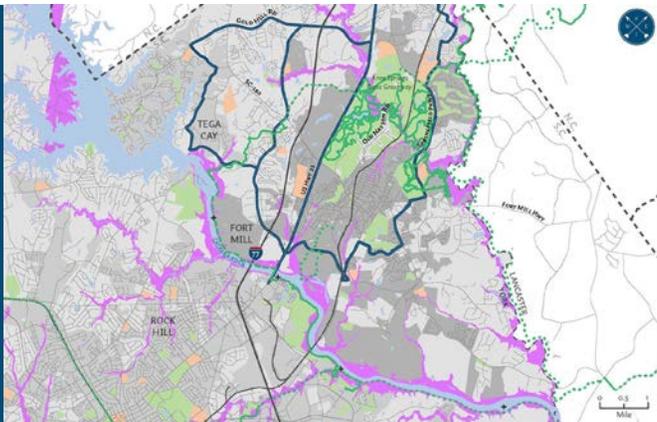
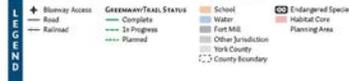
- LEGEND**
- |                  |          |                    |
|------------------|----------|--------------------|
| 1-50 Jobs        | Road     | Water              |
| 51-100 Jobs      | Railroad | Fort Mill          |
| 101-200 Jobs     |          | Other Jurisdiction |
| 201-500 Jobs     |          | York County        |
| 501-1,000 Jobs   |          | County Boundary    |
| 1,001-2,500 Jobs |          |                    |



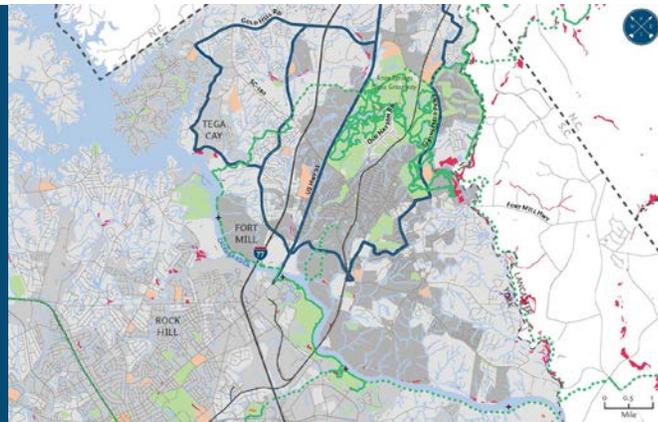
**FORT MILL TRAIL MASTER PLAN**  
Traffic Volumes (AADT)



**FORT MILL TRAIL MASTER PLAN**  
Endangered Species + Habitat Planning Areas

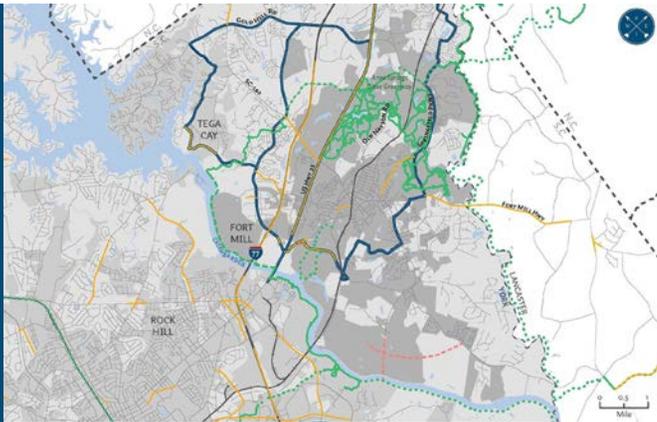


**FORT MILL TRAIL MASTER PLAN**  
Flooding



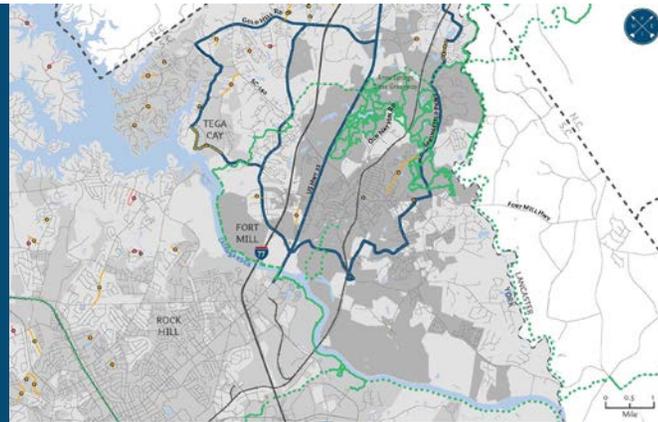
**FORT MILL TRAIL MASTER PLAN**  
Wetlands





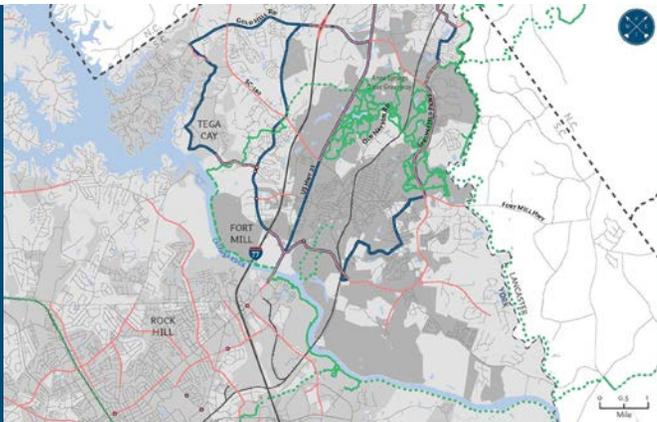
**FORT MILL TRAIL MASTER PLAN**  
 SCDOT Programmed Projects +  
 Planned Roadways

- LEGEND**
- |                      |               |                    |
|----------------------|---------------|--------------------|
| — Programmed Project | — Complete    | Water              |
| — Planned Road       | — In Progress | Fort Mill          |
| — Road               | — Planned     | Other Jurisdiction |
| — Railroad           |               | York County        |
| — Bicycle Route      |               | County Boundary    |



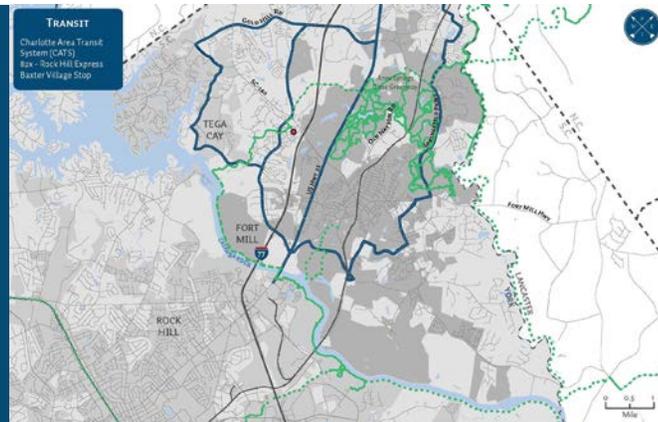
**FORT MILL TRAIL MASTER PLAN**  
 C-Fund + Critical Needs Program Projects

- LEGEND**
- |                  |               |                    |
|------------------|---------------|--------------------|
| ● Critical Needs | — Complete    | Water              |
| ● C-Fund         | — In Progress | Fort Mill          |
| — Critical Needs | — Planned     | Other Jurisdiction |
| — C-Fund         |               | York County        |
|                  |               | County Boundary    |



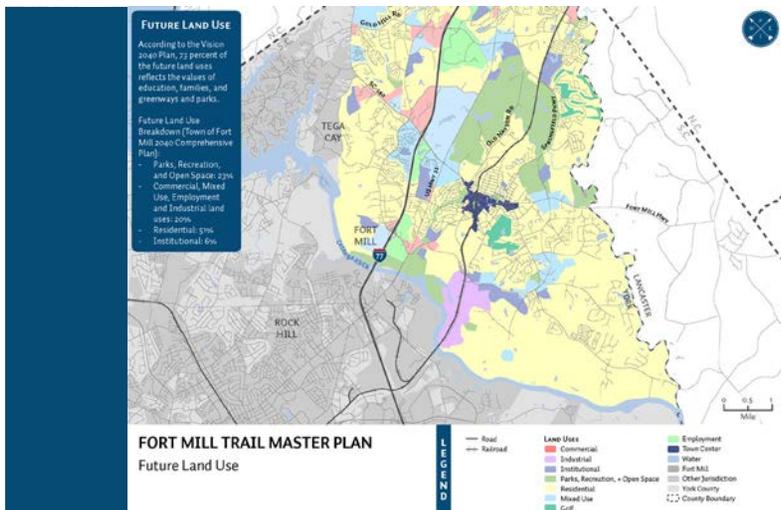
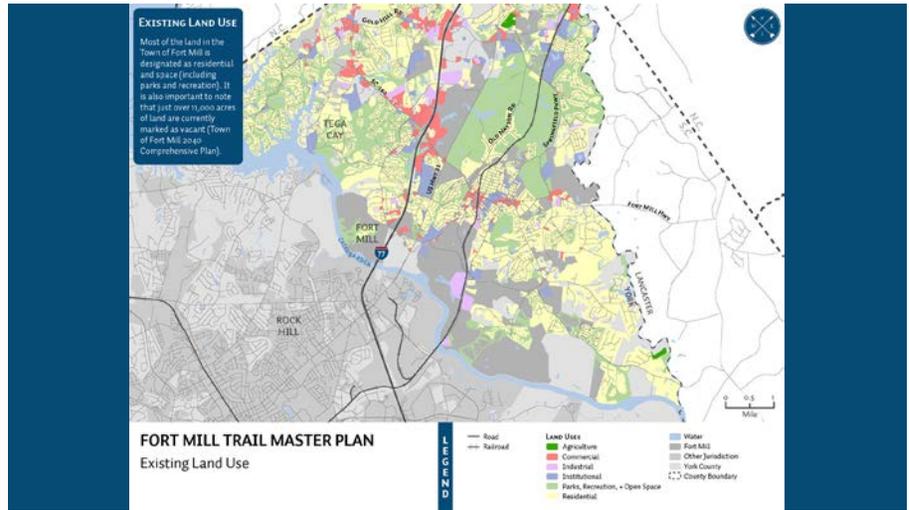
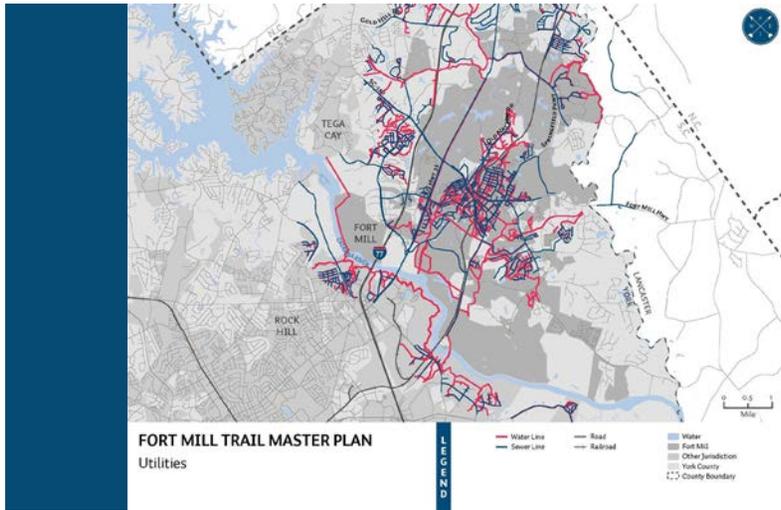
**FORT MILL TRAIL MASTER PLAN**  
 Pennies for Progress Projects

- LEGEND**
- |                        |               |                    |
|------------------------|---------------|--------------------|
| ● Intersection Project | — Complete    | Water              |
| — Roadway Project      | — In Progress | Fort Mill          |
| — Road                 | — Planned     | Other Jurisdiction |
| — Railroad             |               | York County        |
| — Bicycle Route        |               | County Boundary    |



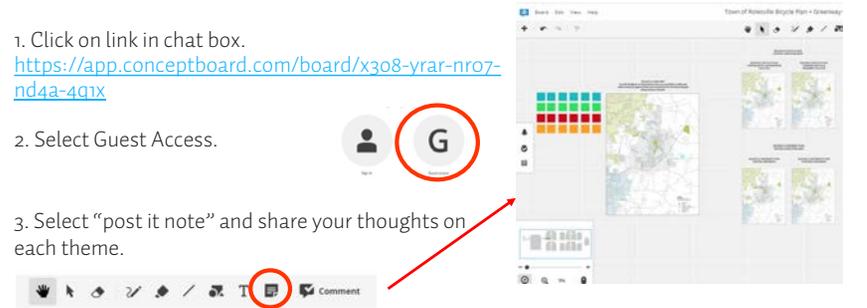
**FORT MILL TRAIL MASTER PLAN**  
 Transit Stop

- LEGEND**
- |                 |               |                    |
|-----------------|---------------|--------------------|
| ● Transit Stop  | — Complete    | Water              |
| — Road          | — In Progress | Fort Mill          |
| — Railroad      | — Planned     | Other Jurisdiction |
| — Bicycle Route |               | York County        |
|                 |               | County Boundary    |



### MAPPING EXERCISE – CONCEPTBOARD

1. Click on link in chat box.  
<https://app.conceptboard.com/board/x308-yrar-nro7-nd4a-4q1x>
2. Select Guest Access.
3. Select “post it note” and share your thoughts on each theme.



**NEXT STEPS**

- Public Survey Launch (October)
- Trail Network Analysis (October-November)
- Steering Committee Meeting #2 (November)
- Public Workshop (January)

**QUESTIONS?**

Kathryn Zeringue, Bicycle + Pedestrian Planner, McAdams  
[zeringue@mcadamsco.com](mailto:zeringue@mcadamsco.com)

Haley Nafissi, Bicycle + Pedestrian Planner, McAdams  
[nafissi@mcadamsco.com](mailto:nafissi@mcadamsco.com)



*Elisha Park Trail Loop*

# Steering Committee Meeting #2



## AGENDA

- Introductions
- Community Survey Review
- Opportunities + Constraints
- Draft Vision + Goals
- Facility Types Review
- Draft Network Recommendations
- Prioritization Criteria + Network Priorities
- Draft Program + Policy Recommendations
- Next Steps



Downtown Fort Mill



  
**NOV 1 – DEC 2, 2022**  
 SURVEY OPEN

  
**15 QUESTIONS +**  
 MAPPING EXERCISE

  
**1,244**  
 PARTICIPANTS

The project team distributed the public survey to help accomplish the following:

- > Introduce the project and gauge public support.
- > Solicit and compile public comment on destinations, opportunities and challenges, user preferences, and route preferences.
- > Fulfill requests for information.
- > Develop an email contact list for interested parties.

**SURVEY COMMENTS:**

- 

"As a physician that lives and works in this community, I would love to bike to work! Additional trail network/bike path along 160 and/or 21 connecting existing trails would be incredible for those that want to commute safely on two wheels. Thank you for considering this healthy option!"
- 

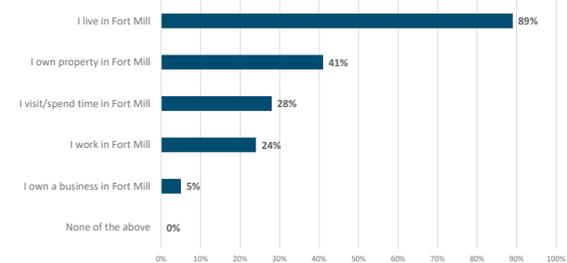
"As a runner and biker, I would love a safe route from where I live in Masons Bend off exit 83 to Downtown Fort Mill, Baxter Village, and to the Riverwalk. Being able to bike to things like Saturday morning coffee would be a game changer!"
- 

"Fort Mill has a strong special needs community. Please consider inclusion in all aspects of this venture."
- 

"I live on Dogwood lane and have been observing the new sewer line project progress. I sincerely hope that the 20-foot-wide path cleared through the woods is used for a trail once the pipe project is completed. I'm happy to see this trail plan is in the works. Trails are a great way to walk and explore without being near traffic, as we would be on sidewalks."

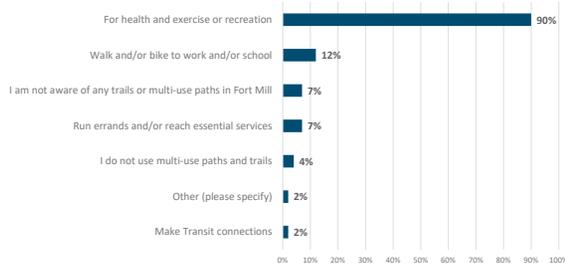
Question 1. What is your relationship to Fort Mill?

Select all that apply.

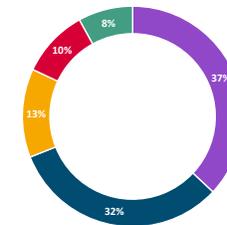


Question 2. How do you currently use trails or multi-use paths in Fort Mill?

Select all that apply.

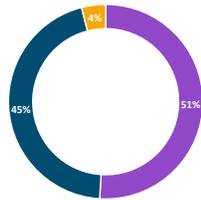


Question 3. How frequently do you use trails or multi-use paths in Fort Mill?



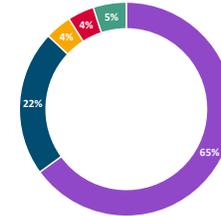
■ A few times a week 
 ■ A few times a month 
 ■ A few times a year 
 ■ Daily 
 ■ Never

Question 4. Have your trail habits changed during the COVID-19 pandemic?



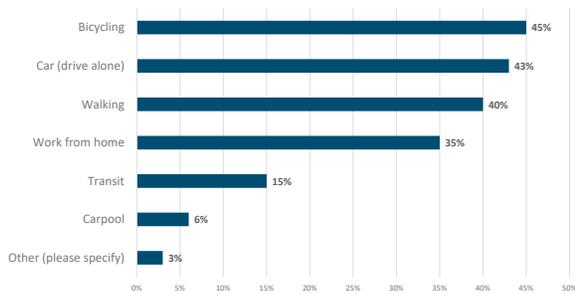
- I use trails about the same amount as I did before the pandemic.
- I use trails more than I did before the pandemic.
- I use trails less than I did before the pandemic.

Question 5. What is your primary mode of transportation for commuting to work and/or school?

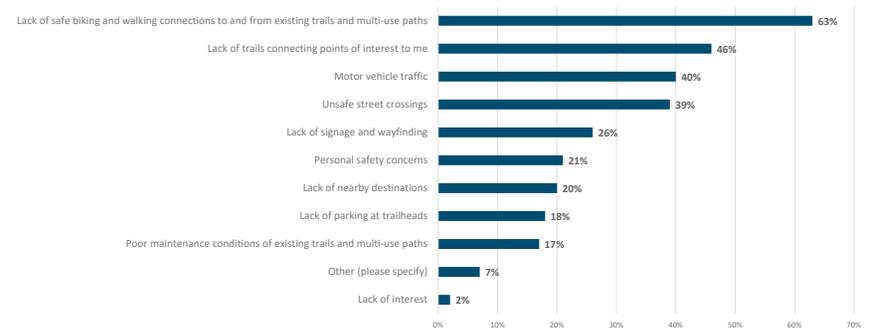


- Car (drive alone)
- Work from Home
- Bicycling
- Walking
- Other (please specify)

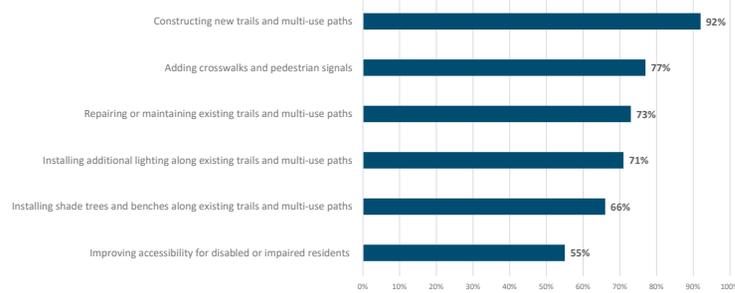
Question 6. Now consider your desired commute in the future. Which modes would you like to use? *Select all that apply.*



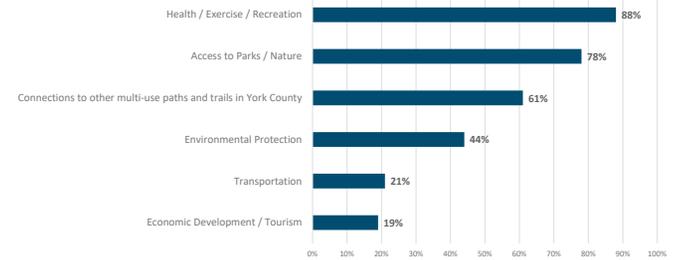
Question 7. What factors discourage you from using trails or multi-use paths in Fort Mill? *Select all that apply.*



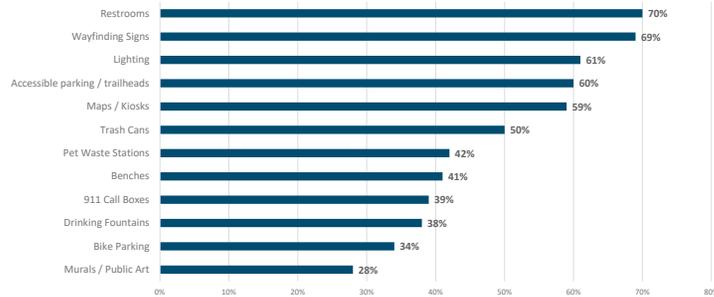
Question 8. Rank which infrastructure or program improvements would encourage you to use trails and multi-use paths more frequently in Fort Mill?  
Please rank your top 5 from 1 (most important) to 5 (least important).



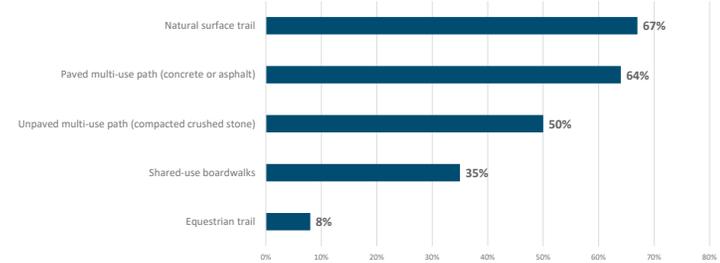
Question 9. Which factors of Fort Mill's trail network are most important to you?  
Select all that apply.



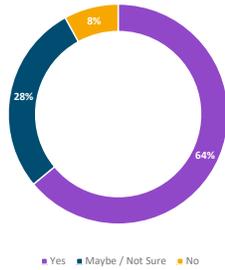
Question 10. What amenities are most important to you for trails and multi-use paths?  
Please rank your top 5 from 1 (most important) to 5 (least important).



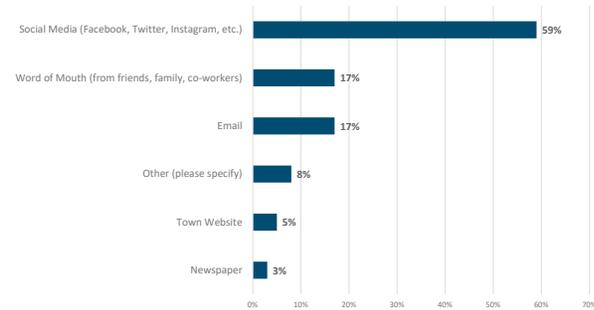
Question 11. Multi-use paths and trails can be constructed using several different materials.  
Select all that apply.



Question 12. Trail networks are often funded through bonds. Bonds are debt issued by local governments that are often used to finance large capital projects. If the opportunity presents itself, would you pay additional taxes to support a bond to expand the trail network?

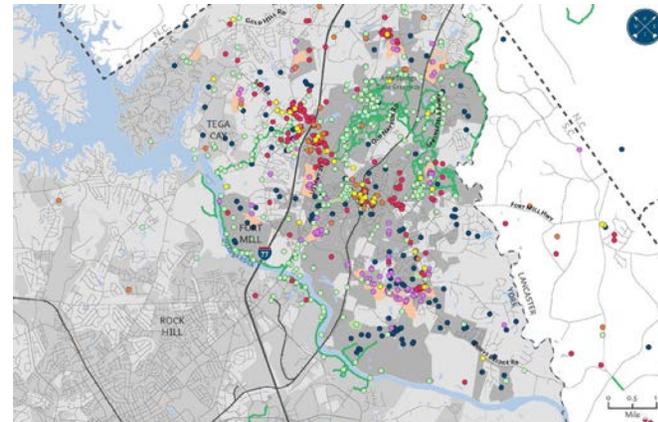


Question 13. How did you hear about the survey?  
*Please select all that apply.*



**INTERACTIVE MAP FINDINGS:**

- Future trails should support golf cart use and provide parking for them at trailheads.
- Respondents want crosswalks improved around the Town, especially at school entrances.
- Respondents want trails to connect to Downtown Fort Mill, Baxter Village, Kingsley Town Center, schools, parks, neighborhoods, the Catawba River, and the Anne Springs Close Greenway.
- Respondents support developer-built trails and sidewalks in neighborhoods.
- Respondents are unaware that other trails exist in the Town due to a lack of signage.
- There is support for regional connections outside of the Town (Connect to little sugar creek in Pineville).
- There are safety concerns associated with Sutton Road, Dam Road, and SC-160.
- Connections over I-77 are a priority to respondents.



**FORT MILL TRAIL MASTER PLAN**  
Preferred Destinations

**PREFERRED DESTINATIONS**

- School
- Recreation
- Shopping
- Home
- Work
- Other

**Multi-use Path/Trail**

- Road
- Railroad
- Water
- Fort Mill School
- Fort Mill Park
- Fort Mill
- Other Jurisdiction
- York County
- Clearly Boundary

**SCHOOL:**

- Fort Mill High
- Riverview Elm.
- Fort Mill Elm.
- Along Fort Mill Parkway

**RECREATION:**

- Baxter Village
- Anne Springs Close Greenway
- Downtown Fort Mill
- YMCA
- Catawba River/Riverwalk

**SHOPPING:**

- Baxter Village
- Kingsley Town Center
- Downtown Fort Mill

**HOME:**

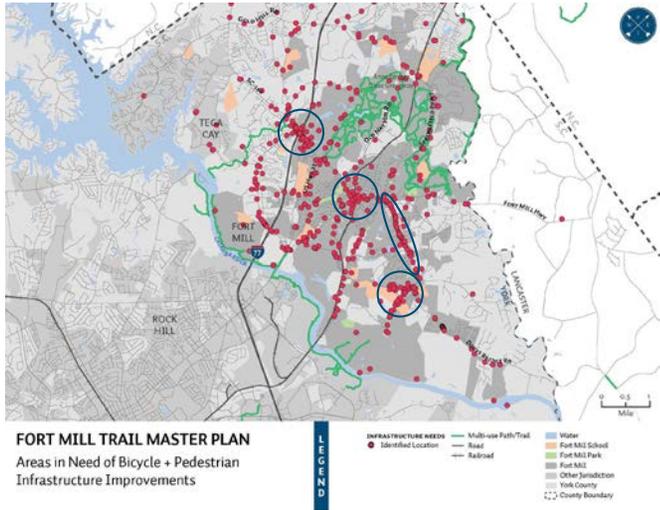
- Riverview area
- Pecan Ridge/Waterside neighborhoods (SC-158)

**WORK:**

- Downtown Fort Mill
- Kingsley Town Center
- Piedmont Medical Center

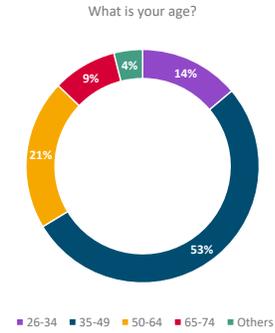
**OTHER:**

- West Town Market area (SC-160)
- Downtown Fort Mill

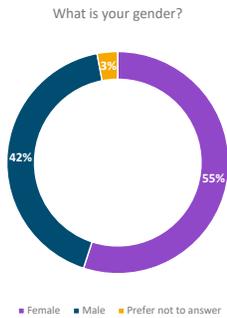


- Kingsley Town Center off SC-160
- Downtown Fort Mill along Main Street
- N Doby's Bridge Road between the Walmart and Doby Bridge Park
- Fort Mill Parkway area between Catawba Ridge Highschool and Doby's Bridge Elementary School

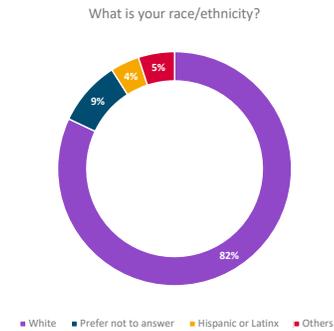
MCADAMS Fort Mill Trail Plan > Community Survey



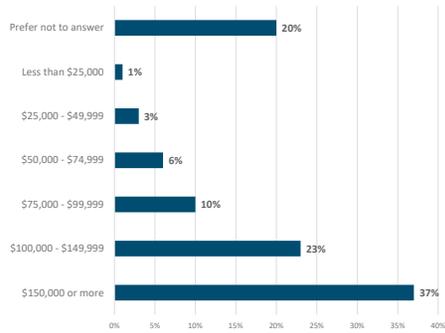
MCADAMS Fort Mill Trail Plan > Community Survey



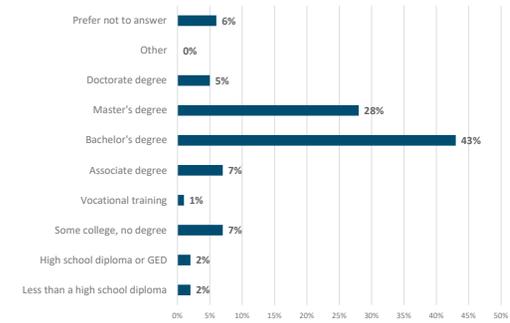
MCADAMS Fort Mill Trail Plan > Community Survey



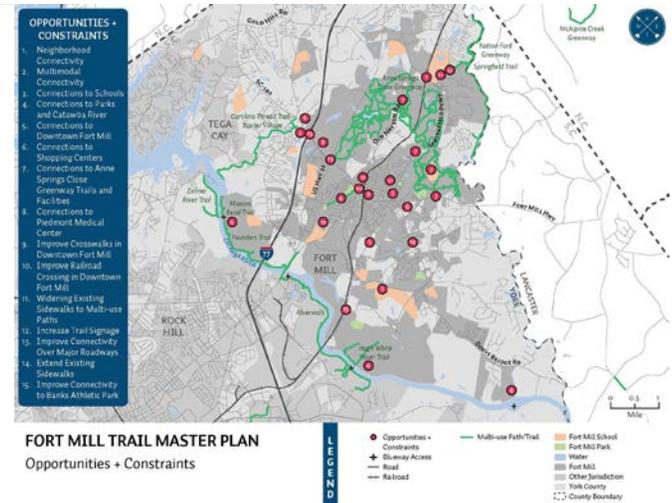
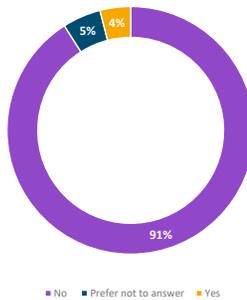
What is your annual household income?



What is the highest level of education you have completed?



Do you have a disability?



**DRAFT TRAIL MASTER PLAN VISION:**

“The Town of Fort Mill will be safe and accessible for community members of all ages and abilities to bike, walk, and hike along a comprehensive trail network that connects key destinations such as neighborhoods, parks, schools, commercial centers, and neighboring communities.”



Founders Trail, Fort Mill

**DRAFT TRAIL MASTER PLAN GOALS:**

**Accessibility + Connectivity** - Ensure that Fort Mill's trail network expands recreational access for residents and visitors and connects to key destinations within Town limits and neighboring jurisdictions.

**Safety** - Address safety needs of users of all ages and abilities in the development of Fort Mill's trail network and propose safety improvements at critical intersections and access points.

**Feasibility** - Prioritize the development of a comprehensive trail network that can be implemented and maintained with Town resources.

**Community** - Foster engagement and collaborate with community groups and residents to promote future development, use, and maintenance of Fort Mill's trail network.

**Paved Trails**

- 10' wide paved trails (multi-use paths, shared-use paths, greenways, etc.).
- Require least amount of long-term maintenance.
- Has greater eligibility from a wide variety of funding sources.
- Asphalt pavement is recommended based on site conditions, anticipated trail use, and cost considerations. Limited sections of concrete pavement may be required to accommodate site conditions, as necessary.
- Paved trail surfaces (asphalt or concrete) offer greater accessibility to accommodate users of all ages and abilities.
- Shoulders or shy zones of 2' or greater should be kept clear of any obstacles to ensure full trail width remains usable.



Swamp Rabbit Trail, Greenville, SC

**Paved Trails**



### Unpaved Trails

- 8-10' wide unpaved trails.
- Unpaved trails are typically constructed of compacted crushed stone.
- This trail type is a durable and affordable means of providing a natural surface for trail construction.
- Unpaved trails can accommodate most trail users (walkers, runners, bicyclists, wheelchair users, etc.) if crushed and compacted properly.



The Palmetto Trail, SC

### Unpaved Trails



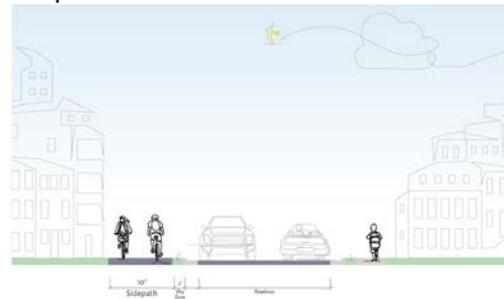
### Sidepaths

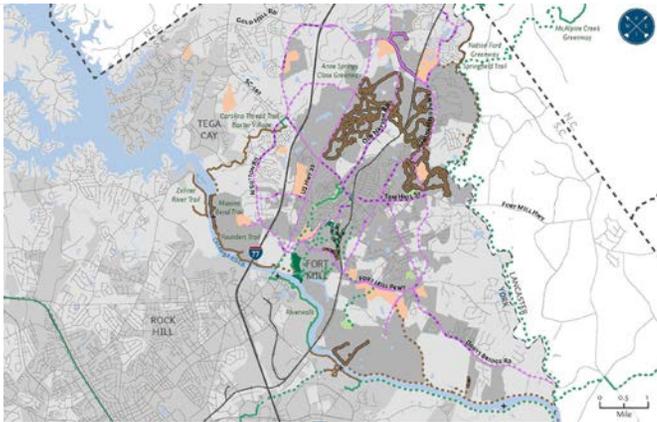
- 10' paved sidepaths
- Asphalt pavement is recommended based on site conditions, anticipated trail use, and cost considerations. Limited sections of concrete pavement may be required to accommodate site conditions, as necessary.
- A five-foot planting strip is provided to separate users from motorized traffic on the adjacent roadway. Planting strip width may be reduced in highly constrained areas where curb and gutter are present.



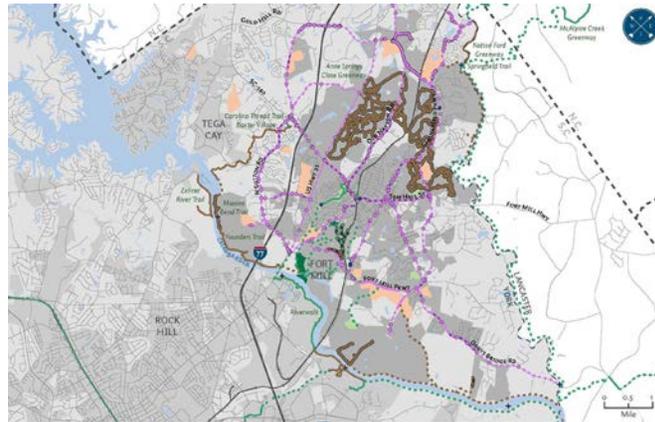
Sidepath, Chapel Hill, NC

### Sidepaths





**FORT MILL TRAIL MASTER PLAN**  
Draft Trail Network



**FORT MILL TRAIL MASTER PLAN**  
Draft Trail Network + Crossings



High-Visibility Crosswalk



RRFB Crossing

Trail Plan > Mapping Exercise

Trail Plan > Prioritization

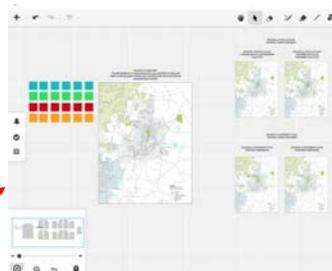
**DRAFT NETWORK MAPPING EXERCISE – CONCEPTBOARD**

1. Click on link in chat box.  
<https://app.conceptboard.com/board/ug3r-cefx-392y-4ck3-s9r6>

2. Select Guest Access.



3. Select “post it note” and share your thoughts on each theme.



**Trail Network Prioritization**

- Discuss evaluation criteria for ranking and scoring priority corridors
- Evaluation criteria guided by the plan goals include the following:
  - Accessibility + Connectivity
  - Safety
  - Feasibility
  - Community
- Other evaluation criteria to consider?



Trail Connector to Walter Y. Elisha Park

**DRAFT POLCIY RECOMMENDATIONS**

- Establish dedicated funding in the Capital Improvements Plan (CIP) for trails and multi-use paths
- Establish a trail facility maintenance policy and operations plan
- Reduce Town-wide speed limit to 25 mph
- Create standard guidelines for trail design within the Town’s policies and guidance documents
- Developers should dedicate right-of-way and construction for trails within new developments and redevelopment projects
- Use native plants in landscaping along trails or multi-use paths
- Adopt outdoor lighting ordinances or codes
- Adopt a Complete Streets Ordinance and/or Program
- Encourage conservation / open space subdivision design within the Town
- Minimize the number of cul-de-sacs constructed and promote trail connections at existing cul-de-sacs
- Encourage land and easement acquisition along trail corridors to ensure permanent use as trails
- Support trail development in tandem with the implementation of riparian buffer restoration and mitigation projects
- Establish a trail classification system and accompanying trail use policies
- Establish trail wayfinding standards

**PROGRAM RECOMMENDATIONS**

- **Educational Programs**
  - Safe Routes to School
  - SCDOT Safety Tips for Biking and Walking
  - Environmental Education
- **Encouragement Programs**
  - Trail Network Map and/or Mobile App
  - Wayfinding Signage
  - Bicycle Parking
  - Bicycle Repair Stations
  - Bike/Walk to School Day Events
  - Bike/Walk to Work Day Events
  - Theme Biking/Walking Events
  - Prescribe-a-Trail/TrailRX/Walk with a Doc
  - Trail Scavenger Hunts/Geocaching
  - Greenway/Trail Ambassadors
  - Bicycle Friendly Community Designation
  - Walk Friendly Community Designation
  - Trail Town Program



**PROGRAM RECOMMENDATIONS**

- **Enforcement Programs**
  - Trail Watch Program
  - South Carolina Code of Laws Concerning Bicycles
  - South Carolina Code of Laws Concerning Pedestrians
  - South Carolina’s Target Zero Project
- **Project Based Programs**
  - Pop-up Demonstrations/Pilot Programs
  - Paint the Pavement Programs
  - Trail Mural Programs
  - Adopt-a-Trail Segment
- **Evaluation Programs**
  - Trail Count Program
  - Trail User Surveys
  - Bike and Walk Audits
  - 311 Service/ See-Click-Fix



Painted sidewalk and crosswalk at 9th & Brevard  
Paint the Pavement Program, Charlotte, NC

**NEXT STEPS**

- Parks + Recreation Committee Meeting (Jan. 24)
- Public Workshop (Feb. 8)
- Trail Network Prioritization
- Draft Recommendations Refinement
- Steering Committee Meeting #3 – Draft Plan Review (Spring 2023)
- Final Plan + Plan Adoption (April-May 2023)



Anne Springs Close Greenway

### QUESTIONS?

Kathryn Zeringue, Bicycle + Pedestrian Planner, McAdams  
[zeringue@mcadamsco.com](mailto:zeringue@mcadamsco.com)

Haley Nafissi, Bicycle + Pedestrian Planner, McAdams  
[nafissi@mcadamsco.com](mailto:nafissi@mcadamsco.com)



*Walter Y. Elisha Park Trail Loop*

# Steering Committee Meeting #3

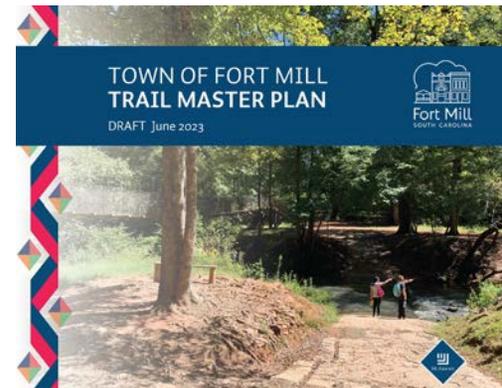


## AGENDA

- Trail Master Plan Tour:
  - Introduction
  - Existing Conditions
  - Community Engagement
  - Network Recommendations
  - Policy + Program Recommendations
  - Implementation
  - Appendices
- Next Steps for Adoption
- Thank You



Downtown Fort Mill



**ACKNOWLEDGMENTS**

**STEERING COMMITTEE**  
 Gary Brown, Town Manager, Town of Fort Mill  
 Chad Potts, Assistant Town Manager, Town of Fort Mill  
 Tom Norman, Mayor, Town of Fort Mill  
 President C. Karpman, Planning Director, Town of Fort Mill  
 Loren Sledge, Mayor, Town of Fort Mill  
 Lou Martin, Member of Fort Mill Economic Partners  
 John Gordon, Area Springs Club Chairman  
 Bob Barnard, Carolina Thread Trail  
 James Tappan, Planning Commission Member  
 Joe Norman & Tracy MGSchool District

Prepared for:  
 Town of Fort Mill, LLC  
 10000 Fort Mill Road, Suite 100  
 Fort Mill, SC 29504  
 Phone: 803.547.1234  
 Email: info@fortmillsc.gov

Prepared by:  
 MCLAREN

**EXECUTIVE SUMMARY**

<b>01</b> INTRODUCTION	<b>02</b> EXISTING CONDITIONS	<b>03</b> COMMUNITY ENGAGEMENT	<b>04</b> NETWORK RECOMMENDATIONS	<b>05</b> POLICY + PROGRAM RECOMMENDATIONS	<b>06</b> IMPLEMENTATION
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**APPENDICES**  
 APPENDIX A - Design Resources  
 APPENDIX B - Detailed Cost Information  
 APPENDIX C - Additional Community Development Information  
 APPENDIX D - Funding Resources

**CONTENTS**

**01 INTRODUCTION**

**OVERVIEW**

The goal of this Fort Mill Trail Master Plan is to enable the Town of Fort Mill to understand the extent of trails use and to plan for, protect from, and improve trail needs. From development through maintenance, and the maintenance required to meet those needs. This program covers new trail facilities, as well as development and ongoing maintenance of both and seeks to increase active transportation and outdoor recreation opportunities, connecting neighborhoods, commercial corridors, schools, and other popular destinations within and outside of the town limits. Recommendations for trail growth, responsibility and maintenance, and trail leadership have been used in the development of the trail network through the planning efforts.

The following actions are included within Chapter 1:

- Vision + Goals
- Study Area Profile
- Community Demographics
- Summary + Trail Network

**01 INTRODUCTION**

**Process - Schedule**

The recommended trail network for the Town of Fort Mill Trail Master Plan was developed and evaluated using an approach with consideration of the local, national, social, and economic environments. These environments are explored further in Chapter 1 Existing Conditions.

The Fort Mill Trail Master Plan begins in August 2021 and is concluded in August 2022. The study process was divided into the following four phases: Project Planning, Existing Conditions, Plan Recommendations, and Implementation + Final Plan. Key components for each phase are listed within the study process graphic below. Engagement efforts were integrated throughout the study process and included several meetings with a Steering Committee, one meeting with the City and Transition Subcommittees, and one meeting with the general public. In addition to these meetings, a community survey was launched in November 2021 to solicit feedback on trail priorities for the public.

**01 INTRODUCTION**

**VISION + GOALS**

The Steering Committee for the Fort Mill Trail Master Plan formed the vision and goals for the plan through a series of meetings with the Steering Committee Members. The goal of the Fort Mill Trail Master Plan is to create an accessible, comprehensive, safe, project feasibility, and community and served as a guide throughout the planning process. Community members also shared their goals during outreach events. The goals in this plan reflect the proposed network and recommendations of the plan. The community vision for the plan is presented below and the goals are described in the graphic to the right.

**Community Vision**

"The Town of Fort Mill will be safe and accessible for community members of all ages and abilities to bike, walk, and hike along a comprehensive trail network that connects key destinations such as neighborhoods, parks, schools, commercial centers, and neighboring communities."

**Community Goals**

**01 INTRODUCTION**

**STUDY AREA PROFILE**

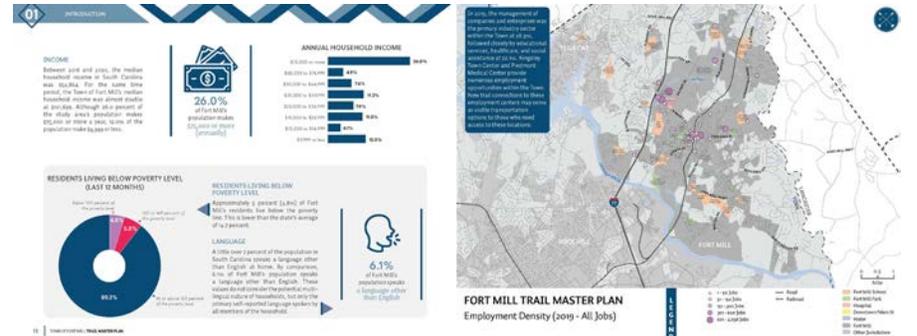
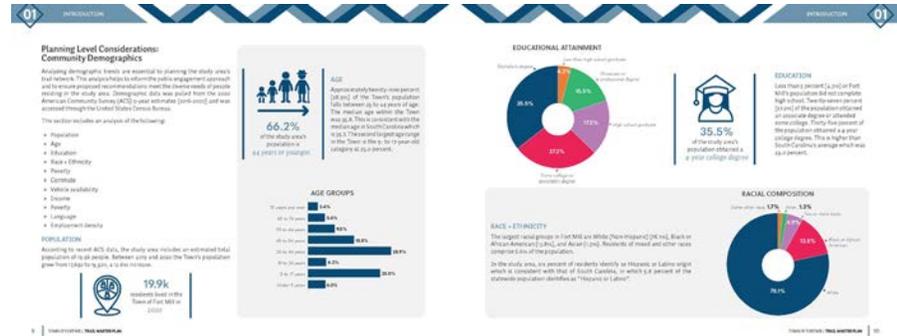
The Town of Fort Mill is in York County, South Carolina, just north of the Catawba River. Trails have always been important to the community since the town was the Carolina Nautilus combined to be located in nearby villages for trading purposes. The Town was officially established in 1820 and the name originated from Fort Mifflin located on the banks of the Catawba River. The town is located in the heart of the Carolina Thread Trail, and the Fort Mill Gateway Trail corridor.

**Anna Springs Close Greenway**  
 Fort Mill's proximity to the Catawba River provides a far-reaching water and greenway that connects the town to the Carolina Thread Trail and the Carolina Thread Trail. The Anna Springs Close Greenway, located north of downtown, provides some of the important and greenway facilities in the town. The Anna Springs Close Greenway also provides several outdoor activities for trail users, including hiking, biking, and canoeing. The trail is a part of the South Carolina Trail network and is a key component of the town's trail system.

**Carolina Thread Trail**  
 The Carolina Thread Trail is a long-distance trail that runs through York County and into the town of Fort Mill. The trail is a key component of the town's trail system and provides a safe and accessible route for trail users. The trail is a part of the South Carolina Trail network and is a key component of the town's trail system.

**Fort Mill Gateway Trail**  
 The Fort Mill Gateway Trail is a new trail that is being developed in Fort Mill. The trail is a key component of the town's trail system and provides a safe and accessible route for trail users. The trail is a part of the South Carolina Trail network and is a key component of the town's trail system.

**01 INTRODUCTION**



# Trail Plan > Existing Conditions

**EXISTING FACILITIES**

Following data collection, the report team conducted a series of field trips to gather additional data for the Trail Master Plan. The team conducted a series of field trips to gather additional data for the Trail Master Plan. The team conducted a series of field trips to gather additional data for the Trail Master Plan.

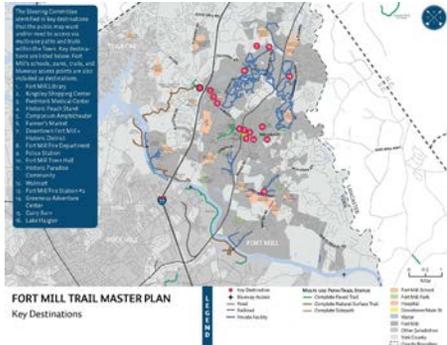
This section contains information that covers the following topics within each consideration focus area:

**Human Environment Considerations**

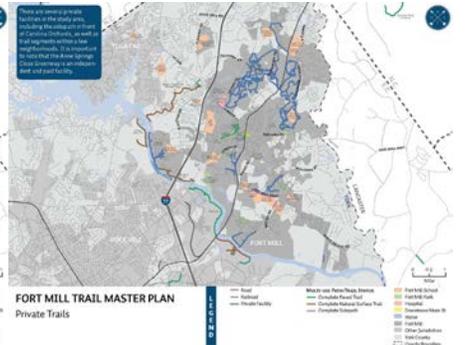
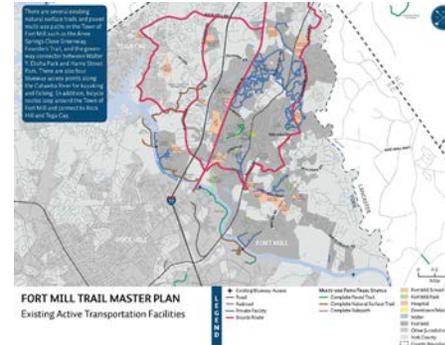
- Key Destinations
- Traffic Volume
- Special Land
- Utilities
- Transportation
- Existing - Future Land Use
- SCDP Programmed Tracts - Planned Business
- City of Fort Mill - Critical Needs Program Projects
- Planned for Progress Projects
- Private - Industrial Districts
- Planned Active Transportation Facilities
- Private Trails
- Planned Trail Segments
- Planned Parks

**Natural Environment Considerations**

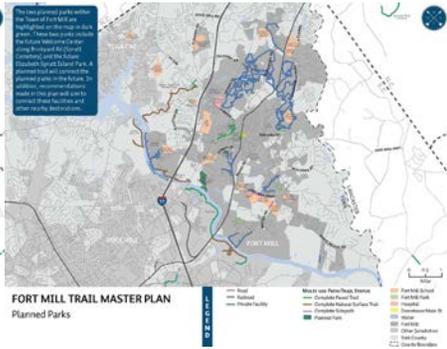
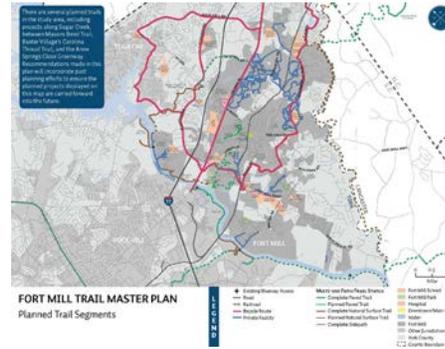
- Integrated System - Habitat Planning Area
- Flooding
- Wetlands



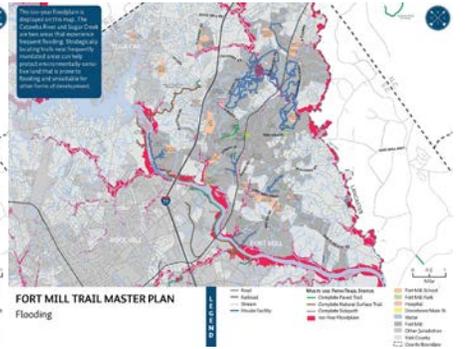
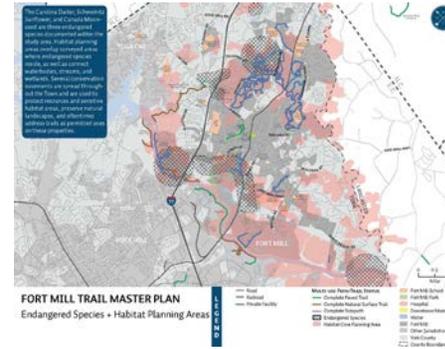
# Trail Plan > Existing Conditions



# Trail Plan > Existing Conditions



# Trail Plan > Existing Conditions





# Trail Plan > Previous Plan Review



## PREVIOUS PLANNING EFFORTS

Fort Mill, the County and various other planning organizations have been instrumental in planning for the past decade. The Area Strategic Council, Planning and Zoning Commission, and the Fort Mill Board of Commissioners have been instrumental in planning for the past decade. The following table provides a summary of the previous planning efforts and the recommendations that have been made to improve the trail system.

- Carolina Street Trail Master Plan for the County Commission, 2010
- AISC's Midland Area Transit Implementation Study, 2010
- Fort Mill Station Design Commission, 2010
- York Forward Comprehensive Plan, 2010
- HNTB Regional Bicycle and Pedestrian Connectivity Plan, 2010
- York County Applied Land Use and Planning Department, 2010
- HNTB Long Range Transportation Plan, 2010
- HNTB Connector Study, 2010
- Columbia Region Comprehensive Economic Development Strategy, 2010
- Town of Fort Mill 2010 Comprehensive Plan



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TOWN OF FORT MILL TRAIL MASTER PLAN | 49



# Trail Plan > Policy Review



## POLICY REVIEW

Existing local and state policies were reviewed to better understand how they influence trail design and development for active transportation facilities in the Town of Fort Mill. The following table provides a summary of key policies from SCDFP, York County, and the Town of Fort Mill that may guide or influence the development of the trail system in the Town.

- Town of Fort Mill Code of Ordinances
- Town of Fort Mill Zoning Ordinance
- York County Code of Ordinances
- SCDFP Strategic Design Manual, 2010

The Town of Fort Mill has adopted a policy that states the general development of land within the Town of Fort Mill shall be consistent with the Comprehensive Zoning Ordinance. The following table provides a summary of key policies from the Town of Fort Mill that may guide or influence the development of the trail system in the Town.

- York County Code of Ordinances
- SCDFP Strategic Design Manual, 2010

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TOWN OF FORT MILL TRAIL MASTER PLAN | 50



# Trail Plan > Opportunities + Constraints



## OPPORTUNITIES + CONSTRAINTS ANALYSIS

Site visits were performed on the study area to gain a better understanding of the surroundings in Fort Mill. The project team conducted site visits to key destinations (i.e., existing parks, shopping centers, essential services, and residential neighborhoods), existing open and public facilities, schools, bridges, environmentally sensitive areas, and undeveloped areas. The information gathered during site visits informed the opportunities and constraints analysis. The analysis identifies constraints to existing and potential facilities, constraints to destinations, access, connectivity, barriers to accessibility, and gaps between the project trail corridor and surrounding opportunities.

The following table provides a summary of the opportunities and constraints of existing facilities that impact the development of Fort Mill's trail network. Numbered sections in the table correspond with specific opportunities and constraints identified on the map following the table.

OPPORTUNITIES + CONSTRAINTS	DESCRIPTION
1. Neighborhood Connectivity	Fort Mill is a walkable community with many opportunities to connect neighborhoods. Fort Mill residents likely have to travel from their homes to local parks, schools, stores, and other destinations. This is an issue for many neighborhoods, but the geographic spread of the town.
2. Multimodal Connectivity	Fort Mill is a walkable community with many opportunities to connect neighborhoods. Fort Mill residents likely have to travel from their homes to local parks, schools, stores, and other destinations. This is an issue for many neighborhoods, but the geographic spread of the town.

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TOWN OF FORT MILL TRAIL MASTER PLAN | 51



# Trail Plan > Opportunities + Constraints

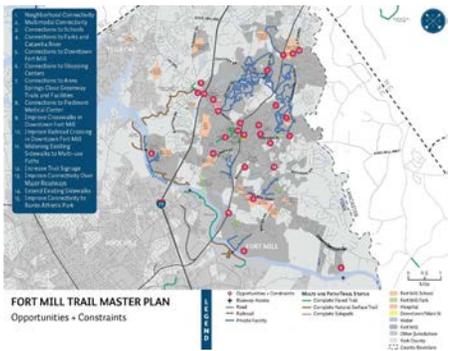


## PHOTOS ILLUSTRATING OPPORTUNITIES AND CONSTRAINTS

Photos illustrating the opportunities and constraints are annotated below. The following map shows the Opportunities and Constraints Analysis.



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FORT MILL TRAIL MASTER PLAN  
Opportunities + Constraints



# Trail Plan > Community Survey

**What destinations in Fort Mill would you like to access to travel or exercise using the Fort Mill trail network? List the top 10 destinations that you would like to reach via trail, such as where you live, work, shop, and play, and why, in detail, and explain.**

**Interactive Mapping Question #1**  
The first section of the interactive mapping question asked participants to mark their preferred destinations that they would like to access on the future trail network in Fort Mill. Participants could mark preferred destinations for their home, work location, recreational areas, schools, shopping destinations, and others. The results are outlined in the table below by category.

CATEGORY	PREFERRED DESTINATIONS
School	<ul style="list-style-type: none"> <li>Fort Mill High School</li> <li>Anderson Elementary School</li> <li>Fort Mill Elementary School</li> <li>Along Fort Mill Parkway</li> <li>Rucker Village</li> <li>Along Fort Mill Elementary</li> <li>Downtown Fort Mill</li> </ul>
Recreation	<ul style="list-style-type: none"> <li>Along Fort Mill Parkway</li> <li>Downtown Fort Mill</li> <li>Chaparral River/Bonaville</li> <li>Rucker Village</li> <li>YMCA</li> </ul>
Shopping	<ul style="list-style-type: none"> <li>Chaparral River/Bonaville</li> <li>Rucker Village</li> <li>Downtown Fort Mill</li> <li>Kingston Town Center</li> <li>Downtown Fort Mill</li> </ul>
Home	<ul style="list-style-type: none"> <li>Various areas</li> <li>Along Chaparral River/Bonaville neighborhood (3rd rd)</li> <li>Downtown Fort Mill</li> <li>Kingston Town Center</li> <li>Frederick Medical Center</li> <li>Along New Market area (200-ft)</li> <li>Downtown Fort Mill</li> </ul>
Work	<ul style="list-style-type: none"> <li>Downtown Fort Mill</li> </ul>
Other	<ul style="list-style-type: none"> <li>Downtown Fort Mill</li> </ul>

**Comments associated with the map question are summarized below:**

- A path along Chaparral River would make access to the main Anne Springs Clean Community accessible via bike.
- A path on either side of the Adventure Road entrance would be accessible via bike.
- A path along the Fort Mill Parkway/Thomas would make the Frederick Trail entrance accessible via bike.
- A path way to access the Campus YMCA would allow recreation to reach their own home park and increase the need to drive. This could be addressed with a bike lane on York Road Street.
- Increased access to Chaparral.
- Connect neighborhood to Rucker Village.
- There is concern for parking/trails that cut through neighborhoods.
- Improve access to the river.
- Improve connectivity within the Town.

**FORT MILL TRAIL MASTER PLAN**  
Preferred Destinations

# Trail Plan > Community Survey

**Which locations, schools, water crossings, neighborhoods, etc. need the most pedestrian and bicycle infrastructure improvements in the Town of Fort Mill?**

**Interactive Mapping Question #2**  
The second interactive mapping question asked participants to mark areas (e.g., streets, intersections, neighborhoods, etc.) that need the most pedestrian and bicycle infrastructure improvements in the Town of Fort Mill. The following were the top four areas that survey participants identified as needing to provide pedestrian infrastructure needs within the Town:

- Kingstony Town Center off I-85
- Downtown Fort Mill along Main Street
- St. David's Bridge Road between the Midwood and City Bridge Park
- Fort Mill Parkway area between Catawba Ridge High School and Dora's Bridge Elementary School

**Comments associated with the map question are summarized below:**

- Improve connectivity to and between schools and neighborhoods.
- Connect Rucker Village, Kingston Town Center, and the Anne Springs Clean Community.
- Connect parks and shopping centers to neighborhoods.
- Fort Mill Parkway and Chaparral Parkway need connector paths.
- Need to connect Maywood Road to the rest of the community via trails along Sublett Road.
- Need further crossing to the Millwater near Downtown Fort Mill.
- Consider connector across I-85.
- There is preference for a trail between Oakdale Foster Community, YMCA, and neighborhoods surrounding the Millwater.
- Along Dutton Ridge Road, especially as it approaches Town Hall.

**FORT MILL TRAIL MASTER PLAN**  
Areas in Need of Bicycle - Pedestrian Infrastructure Improvements

# Trail Plan > Public Workshop

**03** **WORKSHOP & PUBLIC WORKSHOP**

**18 PUBLIC WORKSHOP**  
A public workshop was held on February 8, 2023, at Unity Presbyterian Church in Downtown Fort Mill. Attendees provided information on the public survey results, draft trail network, and draft program and policy recommendations. A handbook may also be distributed by attendees that explained the purpose of the project, and attendees were asked to provide feedback on the draft trail network by marking their top 10 projects on the map.

**KEY FINDINGS**  
Three main areas were identified as high priority projects: "New Trail - Rucker Village", "New Trail - Chaparral River/Bonaville", and "New Trail - Sublett Center".

**Priority Projects**  
During an interactive mapping session, meeting attendees shared ideas on the draft trail network map to indicate areas that represented their top five priority projects. Attendees also submitted comment sheets that outlined additional details related to their top five priority projects. The top project ideas identified during the meeting are listed below. Other key comments received in the comment sheets are provided below:

- Close the sidewalk gap between Bruce Trail and Catawba Ridge High School.
- Add a trail connection between Sublett Road, the Carolina Thread Trail at Rucker Village, and Rucker Trail.
- Add trail connections between the Old Catawba neighborhood and Downtown Fort Mill.
- Repair existing sidewalks on Old Nations Road.
- Add a sidewalk along New Rock Road to link Sublett Center with the new Catawba Park.

**FORT MILL TRAIL MASTER PLAN**  
Draft Trail Network

# Trail Plan > Network Recommendations

**04** **NETWORK RECOMMENDATIONS**

**OVERVIEW**  
This chapter provides insight into design guidance identified from using the recommended trail network on the plan. Special-use sections are also identified and explained throughout this section. The end of the chapter discusses the recommended trail network for the Town and highlights the proposed existing trail network improvements that will help trail users safely and efficiently traverse the community.

**Sections:**

- Design Guidance
  - Special-Use Sections
- Comprehensive Trail Network
- Recreation
- Priority Projects
- Trail Network

**CATAWBA RIVER**  
TOWN OF FORT MILL



**Typical Cross Sections**  
The following cross sections are discussed in this section:

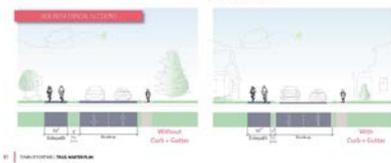
- Paved Trail (Preferred)
- Paved Trail (Optional)
- Sidewalk

A 6 to 8 foot paved trail is recommended for the preferred paved trail, to improve the level of service to pedestrians and provide a level of safety for bicyclists. Asphalt pavement is recommended for all paved trails. A 4-foot wide paved trail is recommended for the optional paved trail, to provide a level of safety for pedestrians and provide a level of safety for bicyclists. Limited sections of concrete pavement may be required for accessibility and safety. The width of a 6 to 8 foot paved trail should be at least 6 feet to ensure it is wide enough to accommodate a 6-foot wide bicycle lane.

**Paved Trail (Optional)**  
A 6 to 8 foot paved trail is recommended for the optional paved trail, to improve the level of service to pedestrians and provide a level of safety for bicyclists. Asphalt pavement is recommended for all paved trails. A 4-foot wide paved trail is recommended for the optional paved trail, to provide a level of safety for pedestrians and provide a level of safety for bicyclists. Limited sections of concrete pavement may be required for accessibility and safety. The width of a 6 to 8 foot paved trail should be at least 6 feet to ensure it is wide enough to accommodate a 6-foot wide bicycle lane.



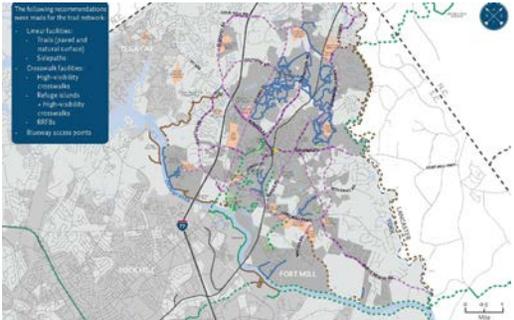
**Sidewalk**  
A 6 to 8 foot sidewalk is recommended along roadway corridors where the trail network crosses, with the transportation network, allowing pedestrians and bicyclists to safely traverse the roadway. Sidewalks are recommended for all paved trails. A 4-foot wide sidewalk is recommended for the optional paved trail, to provide a level of safety for pedestrians and provide a level of safety for bicyclists. Limited sections of concrete pavement may be required for accessibility and safety. The width of a 6 to 8 foot sidewalk should be at least 6 feet to ensure it is wide enough to accommodate a 6-foot wide bicycle lane.



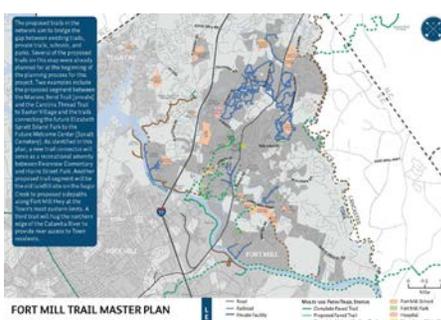
**Intersection Treatments + Crossings**  
**Crosswalk**  
A crosswalk is a designated place for trail users to cross a road or an intersection with the transportation network, allowing pedestrians and bicyclists to safely traverse the roadway. Sidewalks are recommended for all paved trails. A 4-foot wide sidewalk is recommended for the optional paved trail, to provide a level of safety for pedestrians and provide a level of safety for bicyclists. Limited sections of concrete pavement may be required for accessibility and safety. The width of a 6 to 8 foot sidewalk should be at least 6 feet to ensure it is wide enough to accommodate a 6-foot wide bicycle lane.

**Rectangular Rapid Flashing Beacon**  
A Rectangular Rapid Flashing Beacon (RRFB) is a pedestrian-activated signal used in conjunction with pedestrian or trail crossing warning signs to improve safety at uncontrolled, marked crosswalks. The device includes two rectangular-shaped amber LEDs, each with an LED array-based light source, that flash with high frequency when activated. RRFBs should be accompanied by MUTCD pedestrian warning and advance yield signs W11-1 and W11-2 at each end of the crosswalk.

**High-Intensity Activated Crosswalk (HIAC)**  
A High-Intensity Activated Crosswalk (HIAC) is a pedestrian-activated signal used in conjunction with pedestrian or trail crossing warning signs to improve safety at uncontrolled, marked crosswalks. The device includes two rectangular-shaped amber LEDs, each with an LED array-based light source, that flash with high frequency when activated. RRFBs should be accompanied by MUTCD pedestrian warning and advance yield signs W11-1 and W11-2 at each end of the crosswalk.



**FORT MILL TRAIL MASTER PLAN**  
Proposed Trail Network



**FORT MILL TRAIL MASTER PLAN**  
Existing + Proposed Trails



**FORT MILL TRAIL MASTER PLAN**  
Existing + Proposed Sidepaths

# Trail Plan > Trail Network

### 04 NETWORK RECOMMENDATIONS

#### Carolina Thread Trail

The Carolina Thread Trail is a regional network of connected greenways, trails and blueways that spans to connect and a vision and realize a vibrant people. There are over 500 miles of trails and one mile of blueway open to the public that can be linked together to create a comprehensive trail system. The Thread Trail connects natural areas and a place for recreation, transportation, and commerce as a landmark project. The Carolina Thread Trail provides public and community benefits for people of every age, every background, and every community in the region. The mission is to create a legacy of movement and connectivity through a regional network of trails.

Within the immediate region surrounding Fort Mill, the following existing trails are part of the Carolina Thread Trail:

- Baxter Village Trail**: The Baxter Village Trail is a part of a trail network of trails within the Baxter Village community. The trail is a paved multi-use trail that runs along the edge of the Baxter Village community.
- Blue Star Trail**: The Blue Star Trail is a paved multi-use trail that runs along the edge of the Blue Star community.
- Founders Trail**: The Founders Trail is a paved multi-use trail that runs along the edge of the Founders community.
- Masons Bend Trail**: The Masons Bend Trail is a paved multi-use trail that runs along the edge of the Masons Bend community.
- Riverwalk (Rock Hill)**: The Riverwalk is a paved multi-use trail that runs along the edge of the Rock Hill community.

### 04 NETWORK RECOMMENDATIONS

#### Fort Mill Trail Master Plan

Following the completion of the Fort Mill Trail Master Plan, the project team identified key trail corridors and recommended trail segments. The map shows the proposed trail network with various trail segments highlighted in different colors.

# Trail Plan > Trail Network

### 04 NETWORK RECOMMENDATIONS

#### Recommended Crossing Treatments and Blueway Access Locations

Following the completion of the trail network, the project team identified key crossing treatments for intersections, roadways, and commercial crossings. Recommended crossing treatments include:
 

- Standard Fort Mill Main Street
- Springfield Parkway at Ames Springs Close Gateway - Adventure Road
- Main Street between Calhoun Street Park and Ames Street Park
- Ames Street at Ames Springs Close Gateway (part of the existing railroad track)

 The map shows the proposed trail network with various trail segments highlighted in different colors.

### 04 NETWORK RECOMMENDATIONS

#### Fort Mill Trail Master Plan

Proposed Trail Network with Crossings + Blueways

# Trail Plan > Prioritization

### 04 NETWORK RECOMMENDATIONS

#### Prioritization

Following the network recommendation phase, the project team reviewed each proposed project and scored them according to a set of four criteria. Based on the scores provided by the project team, the project team scored proposed trail corridors that were within their limits. Prioritization criteria were established based upon both Town Staff and Planning Committee feedback and emphasized improving trail connectivity between parks, schools, and activity centers within the Town. Community needs that specifically needed more trail connectivity were also identified and scored for priority. Additional information on the criteria is included in the table below.

CRITERIA	DESCRIPTION	RANK	MEASUREMENT	WEIGHT
1. Community Needs	The corridor addresses the community needs or concerns for proposed trail features.	High	Score assigned to public comments identifying needs or concerns along the corridor.	1
		Medium	Score assigned to public comments identifying needs or concerns along the corridor.	1
		Low	Score assigned to public comments identifying needs or concerns along the corridor.	1
2. Corridor Connections/Schools	The corridor provides connections to schools.	High	Corridor is within 1/4 mile of a school.	1
		Medium	Corridor is within 1/2 mile of a school.	1
		Low	Corridor is not near a school.	1
3. Corridor Connections/Parks	The corridor provides connections to parks (e.g., Town Parks, Fort Mill Aquatic Center, WCA, Fort Mill Community Center, and Ames Springs Close Gateway).	High	Corridor is within 1/4 mile of a park or recreation area.	1
		Medium	Corridor is within 1/2 mile of a park or recreation area.	1
		Low	Corridor is not near a park or recreation area.	1
4. Corridor Connections/Activity Centers	The corridor provides connections to activity centers (e.g., Downtown, Kingsley Town Center, Springfield Park, and Kingsley Park Shopping Center).	High	Corridor is within 1/4 mile of a major activity center.	1
		Medium	Corridor is within 1/2 mile of a major activity center.	1
		Low	Corridor is not near a major activity center.	1

### 04 NETWORK RECOMMENDATIONS

#### Fort Mill Trail Master Plan

Proposed Trail Network with Highlighted Trail Corridors Scored for Prioritization

# Trail Plan > Prioritization

### 04 NETWORK RECOMMENDATIONS

The following table includes the complete list of proposed and paired trail projects recommended in the plan. The table summarizes the project description, project location, recommendation, score, and rank.

PROJECT ID#	PROJECT DESCRIPTION	RECOMMENDATION	SCORE	RANK
1	Ames Street	Subway	16	41751
2	Ames Street	Subway	16	41752
3	Ames Street	Subway	16	41753
4	Ames Street	Subway	16	41754
5	Ames Street	Subway	16	41755
6	Ames Street	Subway	16	41756
7	Ames Street	Subway	16	41757
8	Ames Street	Subway	16	41758
9	Ames Street	Subway	16	41759
10	Ames Street	Subway	16	41760
11	Ames Street	Subway	16	41761
12	Ames Street	Subway	16	41762
13	Ames Street	Subway	16	41763
14	Ames Street	Subway	16	41764
15	Ames Street	Subway	16	41765
16	Ames Street	Subway	16	41766
17	Ames Street	Subway	16	41767
18	Ames Street	Subway	16	41768
19	Ames Street	Subway	16	41769
20	Ames Street	Subway	16	41770

### 04 NETWORK RECOMMENDATIONS

The following table includes the complete list of proposed and paired trail projects recommended in the plan. The table summarizes the project description, project location, recommendation, score, and rank.

PROJECT ID#	PROJECT DESCRIPTION	RECOMMENDATION	SCORE	RANK
21	Ames Street	Subway	16	41771
22	Ames Street	Subway	16	41772
23	Ames Street	Subway	16	41773
24	Ames Street	Subway	16	41774
25	Ames Street	Subway	16	41775
26	Ames Street	Subway	16	41776
27	Ames Street	Subway	16	41777
28	Ames Street	Subway	16	41778
29	Ames Street	Subway	16	41779
30	Ames Street	Subway	16	41780
31	Ames Street	Subway	16	41781
32	Ames Street	Subway	16	41782
33	Ames Street	Subway	16	41783
34	Ames Street	Subway	16	41784
35	Ames Street	Subway	16	41785
36	Ames Street	Subway	16	41786
37	Ames Street	Subway	16	41787
38	Ames Street	Subway	16	41788
39	Ames Street	Subway	16	41789
40	Ames Street	Subway	16	41790

Fort Mill  
Trail Plan > Priority Project Cut Sheets

### Harris Road/Street Sidewalk

**PROJECT SNAPSHOT**  
 Location: Harris Road between 33rd Avenue and 35th Avenue  
 Facility Type: Sidewalk (Shoulder)  
 Total Length: 0.2 miles  
 Planning Level Cost Estimate(s):  
 Agency: \$1,842,000 | Carolina: \$1,716,000

**DESIGN CONSIDERATIONS**  
 The planning level cost to construct the facility is provided in this table and includes a site contingency. Facility cost estimates were based on the following factors: project type, project length, facility width, construction market region, and existing roadway characteristics. The estimate does not include costs of right-of-way acquisition and design.

**PRIORITY RANKING**  
 Community Needs: 1  
 Corridor Connections (Shoulder) or Corridor Connections (Park) or Corridor Connections (Active): 2  
 Total Rank: 3

**Map Legend:**  
 Road, Sidewalk, Proposed Street, Proposed Length, Proposed Depth, Proposed Right-of-Way, Proposed Right-of-Way Project, Road, Sidewalk, Proposed Street, Proposed Length, Proposed Depth, Proposed Right-of-Way, Proposed Right-of-Way Project.

Fort Mill  
Trail Plan > Priority Project Cut Sheets

### Harris Street Park Trail

**PROJECT SNAPSHOT**  
 Location: Harris St Park at Harris St between 33rd Avenue and 35th Avenue  
 Facility Type: Park Trail (Shoulder)  
 Total Length: 0.2 miles  
 Planning Level Cost Estimate(s):  
 Agency: \$1,842,000 | Carolina: \$1,716,000

**DESIGN CONSIDERATIONS**  
 The planning level cost to construct the facility is provided in this table and includes a site contingency. Facility cost estimates were based on the following factors: project type, project length, facility width, construction market region, and existing roadway characteristics. The estimate does not include costs of right-of-way acquisition and design.

**PRIORITY RANKING**  
 Community Needs: 1  
 Corridor Connections (Shoulder) or Corridor Connections (Park) or Corridor Connections (Active): 2  
 Total Rank: 3

**Map Legend:**  
 Road, Sidewalk, Proposed Street, Proposed Length, Proposed Depth, Proposed Right-of-Way, Proposed Right-of-Way Project, Road, Sidewalk, Proposed Street, Proposed Length, Proposed Depth, Proposed Right-of-Way, Proposed Right-of-Way Project.

Fort Mill  
Trail Plan > Priority Project Cut Sheets

### Old Nation Road Sidewalk

**PROJECT SNAPSHOT**  
 Location: Old Nation Rd between Spriggfield Hwy and 35th Avenue  
 Facility Type: Sidewalk (Shoulder)  
 Total Length: 0.2 miles  
 Planning Level Cost Estimate(s):  
 Agency: \$1,842,000 | Carolina: \$1,716,000

**DESIGN CONSIDERATIONS**  
 The planning level cost to construct the facility is provided in this table and includes a site contingency. Facility cost estimates were based on the following factors: project type, project length, facility width, construction market region, and existing roadway characteristics. The estimate does not include costs of right-of-way acquisition and design.

**PRIORITY RANKING**  
 Community Needs: 1  
 Corridor Connections (Shoulder) or Corridor Connections (Park) or Corridor Connections (Active): 2  
 Total Rank: 3

**Map Legend:**  
 Road, Sidewalk, Proposed Street, Proposed Length, Proposed Depth, Proposed Right-of-Way, Proposed Right-of-Way Project, Road, Sidewalk, Proposed Street, Proposed Length, Proposed Depth, Proposed Right-of-Way, Proposed Right-of-Way Project.

Fort Mill  
Trail Plan > Priority Project Cut Sheets

### Tom Hall Street Sidewalk

**PROJECT SNAPSHOT**  
 Location: Tom Hall St between 33rd Avenue and 35th Avenue  
 Facility Type: Sidewalk (Shoulder)  
 Total Length: 0.2 miles  
 Planning Level Cost Estimate(s):  
 Agency: \$1,842,000 | Carolina: \$1,716,000

**DESIGN CONSIDERATIONS**  
 The planning level cost to construct the facility is provided in this table and includes a site contingency. Facility cost estimates were based on the following factors: project type, project length, facility width, construction market region, and existing roadway characteristics. The estimate does not include costs of right-of-way acquisition and design.

**PRIORITY RANKING**  
 Community Needs: 1  
 Corridor Connections (Shoulder) or Corridor Connections (Park) or Corridor Connections (Active): 2  
 Total Rank: 3

**Map Legend:**  
 Road, Sidewalk, Proposed Street, Proposed Length, Proposed Depth, Proposed Right-of-Way, Proposed Right-of-Way Project, Road, Sidewalk, Proposed Street, Proposed Length, Proposed Depth, Proposed Right-of-Way, Proposed Right-of-Way Project.



## Trail Plan > Policies + Programs

**05**  
POLICIES + PROGRAMS

### OVERVIEW

In tandem with program and initiative recommendations, trail policy guidance improves local and pedestrian safety and enhances multi-modal transportation options and quality. The following information was reviewed to understand how it influences the planning and community. A summary of recommended policies for the Town of Fort Mill are provided in the table below.

The following sections are covered in this chapter:

- Policy Recommendations
- Program Recommendations
- Educational Programs
- Encouragement Programs
- Project Based Programs
- Evaluation Programs



## Trail Plan > Recommended Policies

**05**  
RECOMMENDED POLICIES

### RECOMMENDED POLICIES

Establish a trail classification system and accompanying trail use policies.

Establish a trail classification system will help the Town identify appropriate trail uses. The Town should set use of trails and policies for behavior and accompanying different types of trail use within the Town's network. The trail program that oversees the trail network should consider the use of the trail network, how to manage, how to maintain, and how to fund. The Town should consider the use of the trail network to be managed by the Town or a private entity.

Adopt a Complete Streets Ordinance and/or Program.

The Town of Fort Mill should consider adopting a Complete Streets Policy, similar to SCOT's Complete Streets Policy, to ensure that all streets are designed to accommodate all modes of transportation. The Town should consider the use of the trail network to be managed by the Town or a private entity.

Encourage transportation options and modes of travel.

Consideration should be given to the use of the trail network to be managed by the Town or a private entity. The Town should consider the use of the trail network to be managed by the Town or a private entity.

Manage the number of trail miles and provide trail use policies.

Consideration should be given to the use of the trail network to be managed by the Town or a private entity. The Town should consider the use of the trail network to be managed by the Town or a private entity.



## Trail Plan > Recommended Programs

**05**  
RECOMMENDED PROGRAMS

### RECOMMENDED PROGRAMS

Organize, promote, and trail-specific programs encourage and strengthen the culture for trail use within a community. Investments in these programs, when coupled with infrastructure investments, frequently lead to an increase in walking, biking, and using existing transit and other modes of travel. A variety of programs and initiatives are provided below and are organized into the following program classifications that are tailored to specific needs of the community.

- Educational Programs
- Encouragement Programs
- Project Based Programs
- Evaluation Programs

### Educational Programs

Educational programs can help promote trail use with information related to how to use trails, use facilities within the community, understand trail safety, and understand the benefits of trails. Educational programs can be developed for a variety of age groups, including children, youth, and adults. Materials may include trail maps, trail guides, brochures, and other materials. Educational programs can be developed for a variety of age groups, including children, youth, and adults. Materials may include trail maps, trail guides, brochures, and other materials.

### Encouragement Programs

Encouragement programs can be developed for a variety of age groups, including children, youth, and adults. Materials may include trail maps, trail guides, brochures, and other materials.

### Project Based Programs

Project based programs can be developed for a variety of age groups, including children, youth, and adults. Materials may include trail maps, trail guides, brochures, and other materials.

### Evaluation Programs

Evaluation programs can be developed for a variety of age groups, including children, youth, and adults. Materials may include trail maps, trail guides, brochures, and other materials.



## Trail Plan > Recommended Programs

**05**  
RECOMMENDED PROGRAMS

### RECOMMENDED PROGRAMS

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### Project Based Programs

Project based programs can be developed for a variety of age groups, including children, youth, and adults. Materials may include trail maps, trail guides, brochures, and other materials.

### Evaluation Programs

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# Trail Plan > Implementation

## Trail Plan > Implementation

# 06

## IMPLEMENTATION

### OVERVIEW

Achieving the vision and goals outlined in this plan will require a coordinated effort across all levels of government. Implementation will require both local and regional efforts for the project. Key roles in the implementation of Fort Mill's network are defined below.

The following partners are recommended to support implementation of the local trail network:

- Town of Fort Mill
- South Carolina Department of Transportation
- Rock Hill-Fort Mill Area Transportation Study (RTAS) Metropolitan Planning Organization (MPO)
- Catawba Lands Conservancy
- Regional Land Trust
- Area Springs Close Conservancy
- Rock Hill Parks - Community Development
- Private Sector Partners

**Town of Fort Mill**

Local government is often best to support the development of recreational and transportation projects within their respective jurisdiction. On projects they play a supporting role, municipal staff are the primary coordinators for community engagement and implementation. The Town of Fort Mill has established a Trail Development Program (TDP) that identifies and prioritizes projects for funding, as well as other legal and jurisdictional policies already in place. As the project leader, the Town of Fort Mill is responsible for the development of all trail segments and their respective jurisdiction. The Town will be responsible for obtaining the final design and ensuring other local plans and ordinances incorporate study recommendations. The Town will also be responsible for coordinating with the County and neighboring municipalities on sharing the same areas across jurisdiction boundaries.

**York County**

York County will develop and provide services for recreational and unincorporated areas within the county. Key departments involved in the development of Fort Mill's trail network include the Parks and Planning Department Services Department. The York County Parks Department plans and develops parks and trails and oversees various areas to provide recreational access for a residents within the county. The York County Planning Department oversees all aspects of land development in the unincorporated areas of York County, including long-range planning, zoning, servicing, and environmental compliance. The Department is responsible for identifying and coordinating state and federal land development, conservation grants, funding, and other resources. The Department also oversees the permitting process and environmental compliance for all projects. The County will be responsible for coordinating all permits on behalf of applicants and for the construction of county trail corridors through land acquisition, design, construction, and maintenance.

# Trail Plan > Partner Roles

## Trail Plan > Partner Roles

### PARTNER ROLES

Achieving the vision and goals outlined in this plan will require a coordinated effort across all levels of government. Implementation will require both local and regional efforts for the project. Key roles in the implementation of Fort Mill's network are defined below.

The following partners are recommended to support implementation of the local trail network:

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### South Carolina Department of Transportation (SCDOT)

SCDOT oversees local and state funding and regulatory related to transportation improvements in unincorporated areas of South Carolina. SCDOT oversees the State Transportation Improvement Program (STIP), which identifies projects that will receive funding during a three-year period (STIP is a four-year). The STIP is a growing initiative for unincorporated areas that are not included in a local government's transportation plan. SCDOT provides funding for planning and engineering projects. The state will be involved on a technical basis to review the final program and project information. SCDOT policies, such as Complete Streets and Central Access, provide guidance and oversight for planning and engineering projects. The Carolina State Parks program (SCSP) is a program that provides funding for the design and improvement of the state's transportation projects. The Department of Transportation provides design guidance and offers potential assistance with the design for trails, bridges, and full access points. Various services recommended include an online trail map, a trail map, and a trail map.

**Rock Hill-Fort Mill Area Transportation Study (RTAS) Metropolitan Planning Organization (MPO)**

RTAS is a local government agency that provides transportation planning for the Rock Hill-Fort Mill area. RTAS is a joint effort between the Town of Fort Mill, York County, and the Federal Transit Administration (FTA). The goal is to plan the most efficient, responsive, and cost-effective transportation system for the movement of people and goods in the unincorporated area. The RTAS Commission oversees the funding process for the federal program following each year's federal highway bill and annual appropriations act. As a leader and coordinator of the planning effort and project prioritization in the region, RTAS is responsible for coordinating with SCDOT to increase state recommendations in planning documents, assisting in facilitating project development and coordination between jurisdictions, and providing guidance along the trail network, and coordinating with SCDOT on STIP project development to incorporate viable trail segments into roadway improvements.

**Catawba Lands Conservancy**

The Catawba Lands Conservancy (CLC) is a non-profit organization that has acquired and protected several acres of trail within and around the Town of Fort Mill. CLC is managing the site as a trail. The Catawba Lands Conservancy provides a key role in advancing the project. The organization provides technical support for the project by providing funding, having awareness among the public, advocating for local officials to coordinate funding for trail development, and forming collaborations amongst jurisdictional partners. The Catawba Lands Conservancy has the experience for coordinating with regional agencies, the County, and municipalities in developing funding opportunities, project planning, and building support for the trail network. The organization should provide guidance and technical assistance to the Town on trail design and construction.

# Trail Plan > Action Plan

## Trail Plan > Action Plan

### Nation Ford Land Trust

Nation Ford Land Trust is a 501(c)(3) organization dedicated to the preservation of open lands, nature beauty, and the natural heritage of York County, South Carolina area. The conservation organization currently owns or holds conservation easements on a total of 1,500 acres in the region. Nation Ford Land Trust is responsible for the Trustee's Trail (currently closed) and proposed trail (currently closed) located in the Town of Fort Mill.

### Area Springs Close Greenway

The Area Springs Close Greenway (ASCG) offers several existing trails within the Fort Mill community. Although trail and regional and scenic, several of the existing trails will connect to new proposed trails and ultimately recommended in this plan. Coordination with ASGC will be needed to help build public support for the future trails as well as future connection points to the existing trail system.

### Local Residents - Community Groups

Residents and community groups that promote knowledge and trail system expansion have a key role in advocating for infrastructure investment. Community members and groups provide support for projects by raising awareness about the public, obtaining technical assistance to streamline funding for greenways and trail networks, and forming collaborative amongst jurisdictional partners.

### Private Sector

Private entities along the proposed trail network will play an important role in trail development. Key private sector partners include the Southern Railway, Duke Energy, York County, Catawba, and Davidson. The Southern Railway runs north to south through the Town, providing for active transportation corridors. Duke Energy and York County Cooperative manage electric utilities and other facilities throughout the study. Davidson may construct a power generation, waterplants, and a water plant as a component to development in municipal and county lines, when specified in a jurisdiction's role of jurisdiction.

### ACTION PLAN

The following Action Plan outlines strategies to implement prioritized trails and segments in Fort Mill based on a year-by-year implementation plan. The Town should apply these action plan steps to the priority projects identified in the plan. Specific identified priority projects will have a greater opportunity for implementation in the short term. Timeline flexibility for the steps are categorized into short-term, mid-term, long-term, and potential steps. These categories are defined below.

STEPS	DESCRIPTION	RESOURCES
Short-Term	The action items will be initiated or in progress within a year.	
Mid-Term	The action items will be initiated within three to seven years.	
Long-Term	The action items will be initiated within seven to ten years.	
Potential	The action items will be in progress and will continue to be implemented in the future.	

STEP #	ACTION	DESCRIPTION	LEAD	RESOURCES	STATUS
1	Adopt the Fort Mill Trail Master Plan	Plan and establish the Fort Mill Trail Master Plan as the guiding planning document for trails and public spaces.	Town Manager, Town Board	Town Board, Planning Commission, Staff	Short Term
2	Develop a work plan and establish Fort Mill Trail implementation efforts	The Town Manager and Planning Director will develop a work plan and establish Fort Mill Trail implementation efforts. Staff will be assigned to implement the plan, including public outreach, design, and construction.	Town Manager, Planning Director	Town Board, Planning Commission, Staff	Short Term
3	Provide staff resources and funding for trail development and implementation	Provide staff resources and funding for trail development and implementation. This includes staff for planning, engineering, construction, and maintenance. Funding will be provided through the Town Board, State, and Federal sources.	Town Manager, Planning Director, Finance Director	Town Board, Catawba Lands Conservancy, State Department of Transportation, York County	Mid-Term

# Trail Plan > Action Plan

## Trail Plan > Action Plan

### ACTION PLAN

The following Action Plan outlines strategies to implement prioritized trails and segments in Fort Mill based on a year-by-year implementation plan. The Town should apply these action plan steps to the priority projects identified in the plan. Specific identified priority projects will have a greater opportunity for implementation in the short term. Timeline flexibility for the steps are categorized into short-term, mid-term, long-term, and potential steps. These categories are defined below.

STEP #	ACTION	DESCRIPTION	LEAD	RESOURCES	STATUS
1	Design an Advisory Committee to guide trail development	Develop an advisory committee to guide trail development. The committee will provide guidance and oversight for the project, including funding, engineering, and construction.	Town Board	Town Board, Planning Commission, Staff	Short Term
2	Finalize the development of a Comprehensive Regional and Local Plan for the Town of Fort Mill	Finalize the development of a Comprehensive Regional and Local Plan for the Town of Fort Mill. The plan will provide a framework for trail development and implementation.	Town Board, Planning Commission, Staff	Town Board, Planning Commission, Staff	Short Term
3	Adopt the Town of Fort Mill Trail Master Plan	Adopt the Town of Fort Mill Trail Master Plan as the guiding planning document for trails and public spaces.	Town Board	Town Board, Planning Commission, Staff	Short Term
4	Develop a work plan and establish Fort Mill Trail implementation efforts	The Town Manager and Planning Director will develop a work plan and establish Fort Mill Trail implementation efforts. Staff will be assigned to implement the plan, including public outreach, design, and construction.	Town Manager, Planning Director	Town Board, Planning Commission, Staff	Short Term
5	Provide staff resources and funding for trail development and implementation	Provide staff resources and funding for trail development and implementation. This includes staff for planning, engineering, construction, and maintenance. Funding will be provided through the Town Board, State, and Federal sources.	Town Manager, Planning Director, Finance Director	Town Board, Catawba Lands Conservancy, State Department of Transportation, York County	Mid-Term

### South Carolina Department of Transportation (SCDOT)

SCDOT oversees local and state funding and regulatory related to transportation improvements in unincorporated areas of South Carolina. SCDOT oversees the State Transportation Improvement Program (STIP), which identifies projects that will receive funding during a three-year period (STIP is a four-year). The STIP is a growing initiative for unincorporated areas that are not included in a local government's transportation plan. SCDOT provides funding for planning and engineering projects. The state will be involved on a technical basis to review the final program and project information. SCDOT policies, such as Complete Streets and Central Access, provide guidance and oversight for planning and engineering projects. The Carolina State Parks program (SCSP) is a program that provides funding for the design and improvement of the state's transportation projects. The Department of Transportation provides design guidance and offers potential assistance with the design for trails, bridges, and full access points. Various services recommended include an online trail map, a trail map, and a trail map.

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06	IMPLEMENTATION	06			
TASK	ACTION	DESCRIPTION	LEAD	PROXIMATE DEADLINE	STATUS
19	Develop a recurring Bicycle and Pedestrian Count Program.	Coordinate with SCOT and FIFEI to establish a program of year-round bicycle and pedestrian counts along trail and roadway corridors. Counts will be conducted at various locations and at various times of day to capture peak and off-peak usage. Data will be used to inform trail planning and to identify areas for improvement.	Scott McNeil, FIFEI, Adams Commission, and Parks & Recreation	Next 12 Months	In Progress
20	Develop a Trail System Branding Study to establish a system brand for the network.	Develop a branding study to establish a system brand for the trail network, including signage, lighting, and other amenities to create a consistent high-quality user experience along the trail system.	Scott McNeil, Scott McNeil, Adams Commission, and Parks & Recreation	Next 12 Months	In Progress
21	Develop a Maintenance Operations Plan.	Develop a maintenance plan to establish a system of regular maintenance activities, including trail clearing, signage, lighting, and other amenities to ensure a high-quality user experience along the trail system.	Scott McNeil, Scott McNeil, Adams Commission, and Parks & Recreation	Next 12 Months	In Progress
22	Identify and implement engineering standards for the trail system.	Identify and implement engineering standards for the trail system, including signage, lighting, and other amenities to ensure a high-quality user experience along the trail system.	Scott McNeil, Scott McNeil, Adams Commission, and Parks & Recreation	Next 12 Months	In Progress



**PROJECT DEVELOPMENT PROCESS**

The Trail Plan Master Plan provides an overall framework for the development of trails and related facilities. However, it is only the first step in a larger process. As a long-term, comprehensive plan, it provides a vision and a set of goals for the trail system. The Trail Plan also provides a framework for the development of trails and related facilities, including the design and construction of trails and related facilities. The Trail Plan also provides a framework for the development of trails and related facilities, including the design and construction of trails and related facilities.

**TRAIL MAINTENANCE**

Regular maintenance is essential to the long-term viability of the network. Maintenance activities include routine tasks such as trail clearing, signage, lighting, and other amenities to ensure a high-quality user experience along the trail system. The Trail Plan also provides a framework for the development of trails and related facilities, including the design and construction of trails and related facilities.

**01 PLANNING**   **02 PRELIMINARY DESIGN**   **03 DESIGN**   **04 RIGHT OF WAY**   **05 CONSTRUCTION**   **06 OPERATIONS & EVALUATION**

**PLANNING**

- Community Engagement
- Feasibility Studies
- Conceptual Design
- Site Planning
- Environmental Assessment
- Public Input

**PRELIMINARY DESIGN**

- Conceptual Design
- Site Planning
- Environmental Assessment
- Public Input

**DESIGN**

- Conceptual Design
- Site Planning
- Environmental Assessment
- Public Input

**RIGHT OF WAY**

- Right of Way Acquisition
- Right of Way Easements
- Right of Way Agreements
- Right of Way Deeds

**CONSTRUCTION**

- Construction Management
- Construction Scheduling
- Construction Cost Estimation
- Construction Risk Management

**OPERATIONS & EVALUATION**

- Operations Management
- Evaluation and Reporting
- Performance Monitoring
- User Feedback



**APPENDICES**

**APPENDIX A: DESIGN RESOURCES**

**APPENDIX B: DETAILED COST INFORMATION**

**APPENDIX C: ADDITIONAL COMMUNITY INVOLVEMENT INFORMATION**

**APPENDIX D: FUNDING RESOURCES**

**APPENDICES**



**APPENDIX A: DESIGN RESOURCES**

**OVERVIEW**

This appendix provides a list of design resources that can be used to inform local and regional design decisions. Organizations such as the Federal Highway Administration (FHWA), American Association of State Highway and Transportation Officials (AASHTO), National Association of City Transportation Officials (NACTO), and South Carolina Department of Transportation (SCDOT) offer general guidelines and project-specific tools to help professionals make design decisions. These guidelines provide flexibility to ensure cost-effective applications.

**SCOT ROADWAY DESIGN GUIDE**

The South Carolina Department of Transportation (SCDOT) Roadway Design Manual defines standards for roadway design and is maintained by SCDOT, including technical sections for roadway, transit, and other design elements that enhance safety, mobility, complete streets, and accessibility for multiple modes of travel. Typical sections also provide guidelines for comprehensive transportation planning, project planning, and project management activities.

**SCOT COMPLETE STREETS IMPLEMENTATION GUIDANCE**

The South Carolina Department of Transportation (SCDOT) Complete Streets Implementation Guide is designed to assist SCDOT staff, project managers, and engineers in implementing the Complete Streets Policy. The document provides comprehensive guidance for incorporating a complete streets approach into SCDOT's planning, engineering, design, and maintenance processes.

**PENNSYLVANIA TRAIL DESIGN & DEVELOPMENT PRINCIPLES: GUIDELINES FOR SUSTAINABLE, MULTI-MODAL TRAILS**

This document contains the best practices and guidelines for the planning, design, and construction of trails. The document provides comprehensive guidance for incorporating a complete streets approach into SCDOT's planning, engineering, design, and maintenance processes.

**NACTO URBAN STREETS DESIGN GUIDE**

The Urban Street Design Guide shares the principles and practices of the nation's foremost engineers, planners, and designers working in urban streets. A design guide for designing and constructing urban streets, the guide provides the tools and the technical advice to make streets safer, more livable, and more economically vibrant. The Guide contains both a clear vision for complete streets and a broad set of best practices for designing them.



**APPENDIX D: FUNDING RESOURCES**

The project team identified potential funding sources, compiled criteria and requirements, and evaluated the anticipated likelihood of funding for bridge projects. Details of the practices for completed work items, as well as the funding sources for work-in-progress items, are provided in the following table. Additional funding sources for grants that would primarily fund trail projects are listed in the "Other Funding Sources" table.

**FEDERAL FUNDING**

Communities in South Carolina have partnered with federal agencies to improve regional and local multimodal paths, programs, and trails. Federal funding is primarily provided to municipalities through state agencies and Metropolitan Planning Organizations (MPO), as well as through Metropolitan grant programs.

The Frita America's Surface Transportation (FAST) Act authorizes transportation funding through three federal grant programs: surface transportation, safety programs, and infrastructure. FAST Act funding is administered by the Federal Highway Administration (FHWA). FHWA distributes funds to SCDOT and directly to MPOs through the Local Assistance Grants Program (LAGP). Surface Federal Land-Use Communities must submit their annual project requests to SCDOT for review and approval. The SCDOT's transportation investment strategies. The formula funds projects and activities that are funded in the State Transportation Investment Program (STIP). These funds require a 50% match from the municipality. Federal transportation funds for bicycle and pedestrian projects are primarily distributed through five programs: Transportation Alternatives (TA), Congestion Mitigation and Air Quality (CMAQ), the Recreational Trails Program (RTIP), the Highway Safety Improvement Program (HSIP), and the Crime Reduction Program (CRP).

Please reference the following link for information related to potential eligibility for bicycle and pedestrian activities and projects under the USDOT's surface transportation funding programs:

[https://www.fhwa.dot.gov/transportation/surface\\_transportation/transportation\\_alternatives/index.cfm](https://www.fhwa.dot.gov/transportation/surface_transportation/transportation_alternatives/index.cfm)

Additional federal funding sources for bicycle and pedestrian planning, education, and programs are administered through the Department of Housing and Urban Development (HUD) with the Community Development Block Grant (CDBG) Program. Federally discretionary grant programs are administered by the U.S. Department of Transportation, National Park Service, and the National Endowment for the Arts. Some federal programs were added under the Bipartisan Infrastructure Law (BIL) and the Inflation Reduction Act (IRA) that can be used for trail fund, urban transportation projects.

**STATE & MPO ADMINISTERED FUNDING**

**TRANSPORTATION ALTERNATIVES (TA)**

Transportation Alternatives (TA) provides federal funds for community-based projects that expand transit choices and enhance the transportation experience by integrating modes and improving the cultural, historic, and scenic aspects of our transportation infrastructure. In South Carolina, TA funds are administered by SCDOT and eligible projects include pedestrian facilities, bicycle facilities, and transitway projects.

Funds are awarded to projects in a merit-based process. Only after a project has been approved by the State Department of Transportation or Metropolitan Planning Organization (MPO) and the FHWA district office, can a city become eligible for reimbursement. Funds must be received after final design of the project is approved, or they are not eligible for reimbursement.

The formula for the standardized calculation of:

- 40,000 per mile of 2+ lane divided area of the State with an enhanced area population of over 100,000, also as seen as a Transportation Management Area;

- 40,000 per mile of 2+ lane of the State other than other areas with a population greater than 100,000;

- 40,000 per mile of 2+ lane of the State with a population less than 100,000;

The largest federal allocation for the South Carolina MPO Area Transportation Study (MATS) was for the TA program. The TA program will be used for a variety of eligible projects under the TA program. A budget is required for the jobs as well as the remaining projects. TA funds can be used for both maintenance and safety.

**State (CDBG) and MPO (CDBG) administered funding**

**CONGESTION MITIGATION & AIR QUALITY**

SCDOT created a Congestion Mitigation and Air Quality (CMAQ) program to provide funding for local governments interested in projects. The program is administered by SCDOT and eligible projects include pedestrian facilities, bicycle facilities, and transitway projects.

**CRIME REDUCTION PROGRAM (CRP)**

Funding for projects that demonstrate solutions in areas and activities that reduce crime. Funding is used for projects with the designated reimbursement or maintenance areas, as well as incident response activities. In South Carolina, the Crime Reduction Emergency Program (CREP) is the incident response services provided in Beaufort, Charleston, Columbia, Florence, Great Smoky Mountains, Greenville, and Spartanburg urban areas. Incident response services include: bicycle and pedestrian safety, and incident management, and provide for aid and emergency medical services. CMAQ funds typically require a 50% match.

Each project funded under the CMAQ program must meet three basic criteria:

- It must be a transportation project.

- It must generate an enhanced reduction.

- It must be in a (or have) a reimbursement in maintenance area.

The following pedestrian and bicycle projects are eligible to receive funding:

- Building shared-use paths, bicycle routes, and other facilities intended to reduce automobile trips, and that are not exclusively for recreational use.

- Outreach activities that promote safe bicycle use, and

- Establishing and funding state-wide pedestrian and bicycle education programs.

The FAST Act establishes a formula that is used to determine how much money each state receives through the CMAQ program. States may transfer up to 5% of these funds to other programs. Each state allocation these funds to MPOs within the jurisdiction, who are responsible for allocating these funds to different projects.

**CRIME REDUCTION PROGRAM (CRP)**

The Highway Safety Improvement Program (HSIP) is a core Federal-aid program with the purpose of achieving a significant reduction in fatalities and serious injuries on all public roads. Projects are evaluated using crash data and the cost effectiveness of proposed improvements. South Carolina's HSIP 2020 Annual Report shows that funding for non-restricted areas was \$6 million. Additional information can be viewed at <https://www.scdot.gov/transportation/safety/hsip/>

Each project funded under the CMAQ program must meet three basic criteria:



**NEXT STEPS**

- Steering Committee Members - Draft Plan Review (June 2-16th)
- Final Plan (Completed by July 7th)
- Plan Adoption (Proposed for the Aug. 14<sup>th</sup> Council Meeting)

*Thank you for serving on the Trail Plan Steering Committee! We appreciate your time and support!*



Anne Springs Close Greenway



**QUESTIONS?**

Kathryn Zeringue, Bicycle + Pedestrian Planner, McAdams  
[zeringue@mcadamsco.com](mailto:zeringue@mcadamsco.com)

Haley Nafissi, Bicycle + Pedestrian Planner, McAdams  
[nafissi@mcadamsco.com](mailto:nafissi@mcadamsco.com)



Walter Y. Elisha Park Trail Loop

# Parks + Recreation Subcommittee Meeting



## AGENDA

- Introductions
- Community Survey Review
- Opportunities + Constraints
- Draft Vision + Goals
- Facility Types Review
- Draft Network Recommendations
- Prioritization Criteria + Network Priorities
- Draft Program + Policy Recommendations
- Next Steps



Downtown Fort Mill



  
**NOV 1 – DEC 2, 2022**  
 SURVEY OPEN

  
**15 QUESTIONS +**  
 MAPPING EXERCISE

  
**1,244**  
 PARTICIPANTS

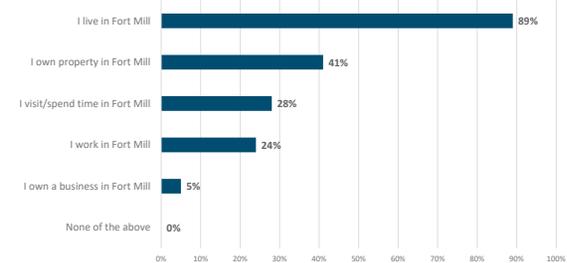
The project team distributed the public survey to help accomplish the following:

- > Introduce the project and gauge public support.
- > Solicit and compile public comment on destinations, opportunities and challenges, user preferences, and route preferences.
- > Fulfill requests for information.
- > Develop an email contact list for interested parties.

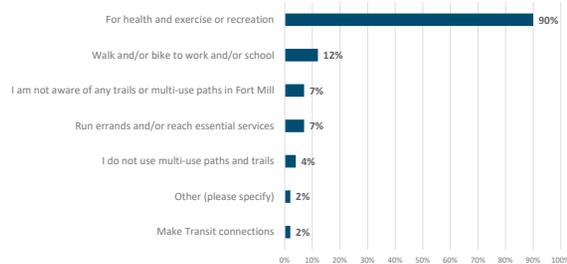
**SURVEY COMMENTS:**

- 
 "As a physician that lives and works in this community, I would love to bike to work! Additional trail network/bike path along 160 and/or 21 connecting existing trails would be incredible for those that want to commute safely on two wheels. Thank you for considering this healthy option!"
- 
 "As a runner and biker, I would love a safe route from where I live in Masons Bend off exit 83 to Downtown Fort Mill, Baxter Village, and to the Riverwalk. Being able to bike to things like Saturday morning coffee would be a game changer!"
- 
 "Fort Mill has a strong special needs community. Please consider inclusion in all aspects of this venture."
- 
 "I live on Dogwood lane and have been observing the new sewer line project progress. I sincerely hope that the 20-foot-wide path cleared through the woods is used for a trail once the pipe project is completed. I'm happy to see this trail plan is in the works. Trails are a great way to walk and explore without being near traffic, as we would be on sidewalks."

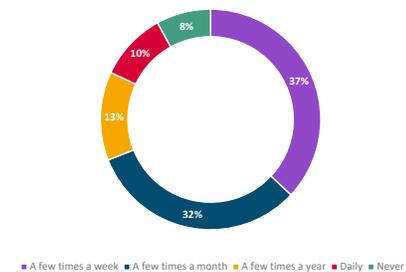
Question 1. What is your relationship to Fort Mill?  
*Select all that apply.*



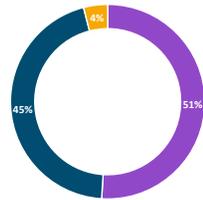
Question 2. How do you currently use trails or multi-use paths in Fort Mill?  
*Select all that apply.*



Question 3. How frequently do you use trails or multi-use paths in Fort Mill?

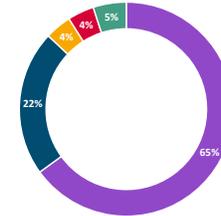


Question 4. Have your trail habits changed during the COVID-19 pandemic?



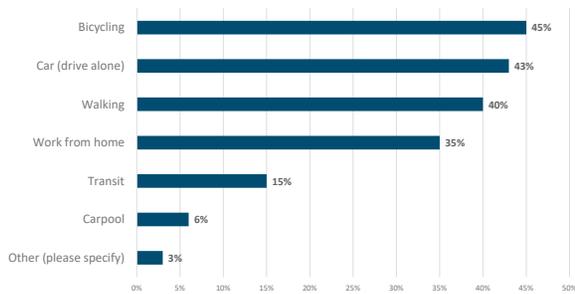
- I use trails about the same amount as I did before the pandemic.
- I use trails more than I did before the pandemic.
- I use trails less than I did before the pandemic.

Question 5. What is your primary mode of transportation for commuting to work and/or school?

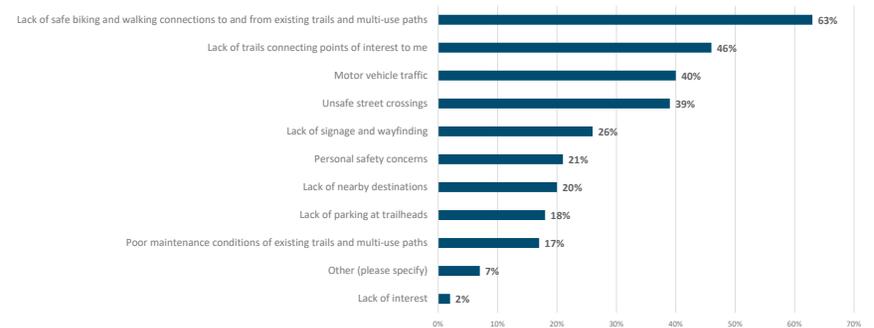


- Car (drive alone)
- Work from Home
- Bicycling
- Walking
- Other (please specify)

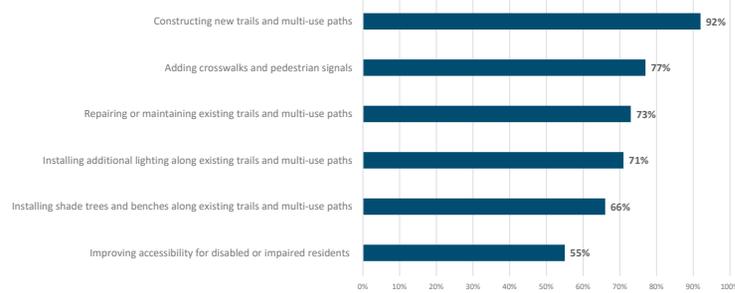
Question 6. Now consider your desired commute in the future. Which modes would you like to use? *Select all that apply.*



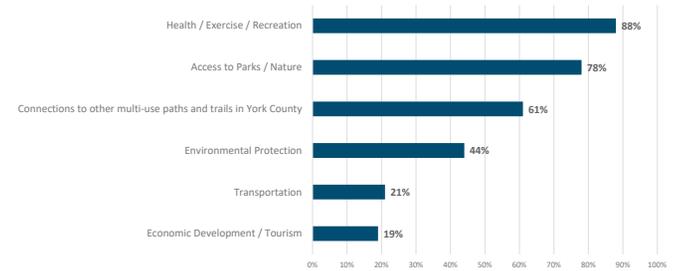
Question 7. What factors discourage you from using trails or multi-use paths in Fort Mill? *Select all that apply.*



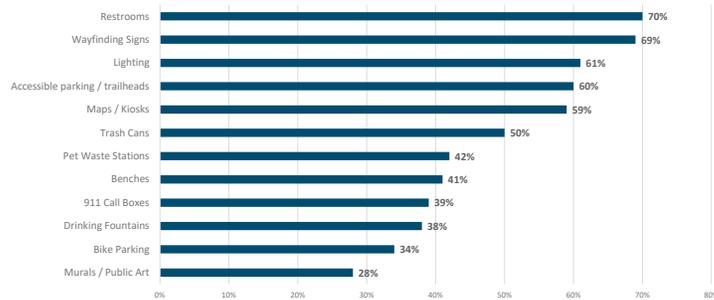
Question 8. Rank which infrastructure or program improvements would encourage you to use trails and multi-use paths more frequently in Fort Mill?  
Please rank your top 5 from 1 (most important) to 5 (least important).



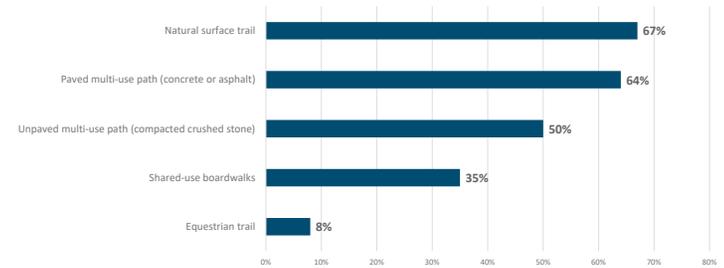
Question 9. Which factors of Fort Mill's trail network are most important to you?  
Select all that apply.



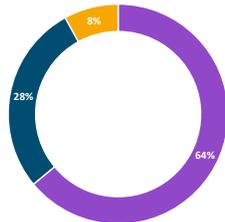
Question 10. What amenities are most important to you for trails and multi-use paths?  
Please rank your top 5 from 1 (most important) to 5 (least important).



Question 11. Multi-use paths and trails can be constructed using several different materials.  
Select all that apply.



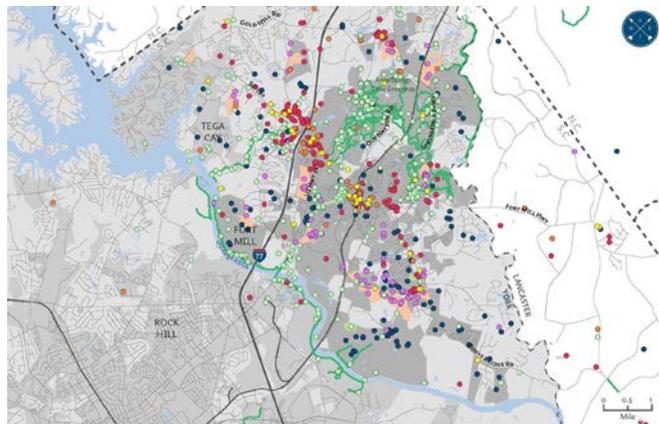
Question 12. Trail networks are often funded through bonds. Bonds are debt issued by local governments that are often used to finance large capital projects. If the opportunity presents itself, would you pay additional taxes to support a bond to expand the trail network?



■ Yes ■ Maybe / Not Sure ■ No

**INTERACTIVE MAP FINDINGS:**

- Future trails should support golf cart use and provide parking for them at trailheads.
- Respondents want crosswalks improved around the Town, especially at school entrances.
- Respondents want trails to connect to Downtown Fort Mill, Baxter Village, Kingsley Town Center, schools, parks, neighborhoods, the Catawba River, and the Anne Springs Close Greenway.
- Respondents support developer-built trails and sidewalks in neighborhoods.
- Respondents are unaware that other trails exist in the Town due to a lack of signage.
- There is support for regional connections outside of the Town (Connect to little sugar creek in Pineville).
- There are safety concerns associated with Sutton Road, Dam Road, and SC-160.
- Connections over I-77 are a priority to respondents.



**FORT MILL TRAIL MASTER PLAN**  
Preferred Destinations

**PREFERRED DESTINATIONS**

- School
- Recreation
- Shopping
- Home
- Work
- Other

**Multi-use Path/Trail**

- Road
- Railroad

**Water**

- Fort Mill School
- Fort Mill Park
- Fort Mill
- Other Jurisdiction
- York County
- County Boundary

**SCHOOL:**

- Fort Mill High
- Riverview Elm.
- Fort Mill Elm.
- Along Fort Mill Parkway

**RECREATION:**

- Baxter Village
- Anne Springs Close Greenway
- Downtown Fort Mill
- YMCA
- Catawba River/Riverwalk

**SHOPPING:**

- Baxter Village
- Kingsley Town Center
- Downtown Fort Mill

**HOME:**

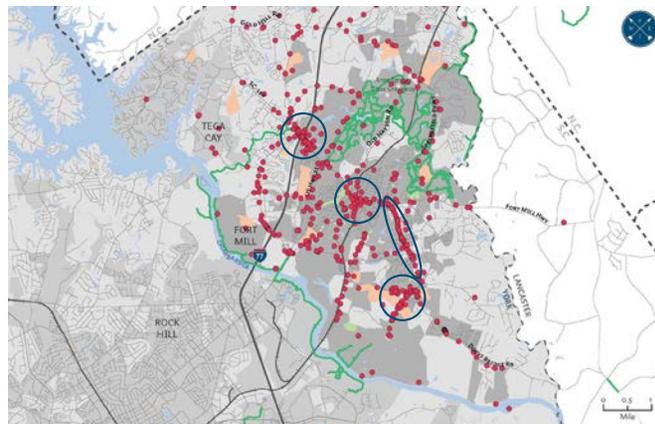
- Riverview area
- Pecan Ridge/Waterside neighborhoods (Sc-158)

**WORK:**

- Downtown Fort Mill
- Kingsley Town Center
- Piedmont Medical Center

**OTHER:**

- West Town Market area (SC-160)
- Downtown Fort Mill



**FORT MILL TRAIL MASTER PLAN**  
Areas in Need of Bicycle + Pedestrian Infrastructure Improvements

**INFRASTRUCTURE NEEDS**

- Identified Location

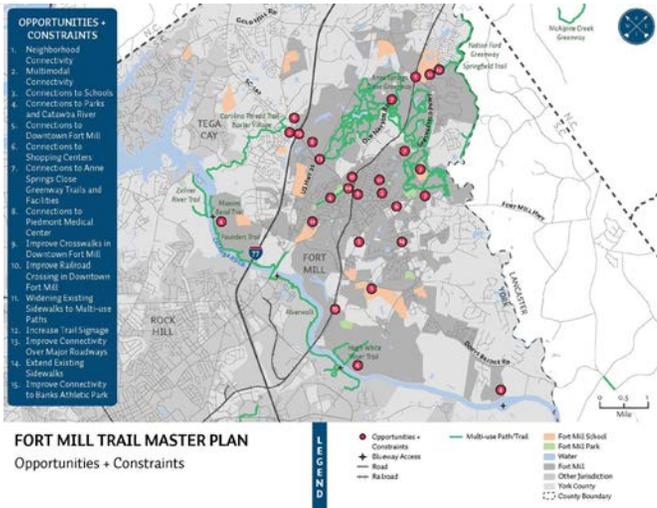
**Multi-use Path/Trail**

- Road
- Railroad

**Water**

- Fort Mill School
- Fort Mill Park
- Fort Mill
- Other Jurisdiction
- York County
- County Boundary

- Kingsley Town Center off SC-160
- Downtown Fort Mill along Main Street
- N Doby's Bridge Road between the Walmart and Doby Bridge Park
- Fort Mill Parkway area between Catawba Ridge Highschool and Doby's Bridge Elementary School



**DRAFT TRAIL MASTER PLAN VISION:**

“The Town of Fort Mill will be safe and accessible for community members of all ages and abilities to bike, walk, and hike along a comprehensive trail network that connects key destinations such as neighborhoods, parks, schools, commercial centers, and neighboring communities.”



Founders Trail, Fort Mill

**DRAFT TRAIL MASTER PLAN GOALS:**

**Accessibility + Connectivity** - Ensure that Fort Mill's trail network expands recreational access for residents and visitors and connects to key destinations within Town limits and neighboring jurisdictions.

**Safety** - Address safety needs of users of all ages and abilities in the development of Fort Mill's trail network and propose safety improvements at critical intersections and access points.

**Feasibility** - Prioritize the development of a comprehensive trail network that can be implemented and maintained with Town resources.

**Community** - Foster engagement and collaborate with community groups and residents to promote future development, use, and maintenance of Fort Mill's trail network.

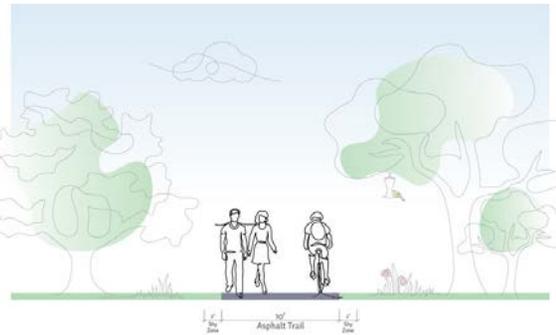
**Paved Trails**

- 10' wide paved trails (multi-use paths, shared-use paths, greenways, etc.).
- Require least amount of long-term maintenance.
- Has greater eligibility from a wide variety of funding sources.
- Asphalt pavement is recommended based on site conditions, anticipated trail use, and cost considerations. Limited sections of concrete pavement may be required to accommodate site conditions, as necessary.
- Paved trail surfaces (asphalt or concrete) offer greater accessibility to accommodate users of all ages and abilities.
- Shoulders or shy zones of 2' or greater should be kept clear of any obstacles to ensure full trail width remains usable.



Swamp Rabbit Trail, Greenville, SC

**Paved Trails**



**Unpaved Trails**

- 8-10' wide unpaved trails.
- Unpaved trails are typically constructed of compacted crushed stone.
- This trail type is a durable and affordable means of providing a natural surface for trail construction.
- Unpaved trails can accommodate most trail users (walkers, runners, bicyclists, wheelchair users, etc.) if crushed and compacted properly.



*The Palmetto Trail, SC*

**Unpaved Trails**



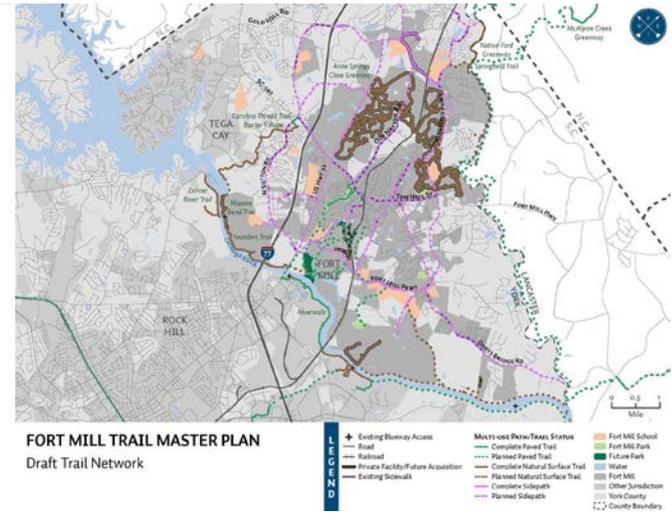
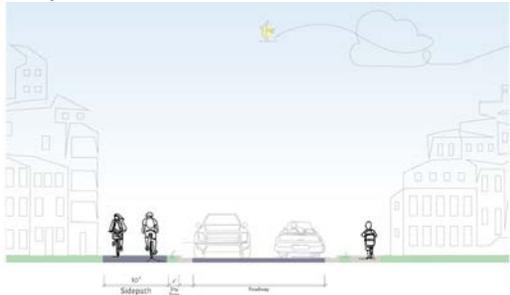
**Sidepaths**

- 10' paved sidepaths
- Asphalt pavement is recommended based on site conditions, anticipated trail use, and cost considerations. Limited sections of concrete pavement may be required to accommodate site conditions, as necessary.
- A five-foot planting strip is provided to separate users from motorized traffic on the adjacent roadway. Planting strip width may be reduced in highly constrained areas where curb and gutter are present.



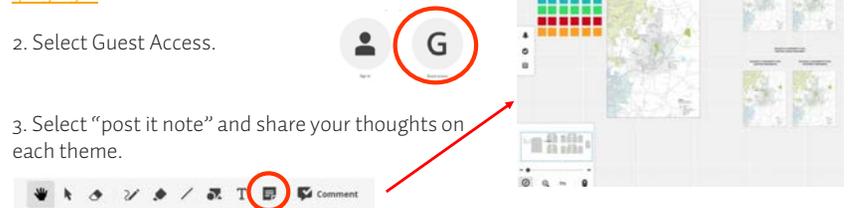
*Sidepath, Chapel Hill, NC*

Sidepaths



DRAFT NETWORK MAPPING EXERCISE – CONCEPTBOARD

- Click on the following link.  
<https://app.conceptboard.com/board/ug3r-cefx-392y-4ck3-s9r6>
- Select Guest Access.
- Select “post it note” and share your thoughts on each theme.



**Trail Network Prioritization**

- Discuss evaluation criteria for ranking and scoring priority corridors
- Evaluation criteria guided by the plan goals include the following:
  - **Accessibility + Connectivity**
    - Within ¼ or ½ mile of existing trails and bicycle & pedestrian facilities
    - Within ¼ or ½ mile of parks and recreation centers
    - Within ¼ or ½ mile of schools, libraries, civic centers
    - Within ¼ or ½ mile of commercial areas, Downtown, etc.
    - Within ¼ or ½ mile of residential neighborhoods
  - **Safety**
    - Improves areas with high rates of bicycle/pedestrian crashes
  - **Feasibility**
    - Available land/easements, etc.
  - **Community**
    - Community input, project requests
- Other evaluation criteria to consider?



Trail Connector to Walter Y. Elisha Park

**DRAFT POLCIY RECOMMENDATIONS**

- Establish dedicated funding in the Capital Improvements Plan (CIP) for trails and multi-use paths
- Establish a trail facility maintenance policy and operations plan
- Reduce Town-wide speed limit to 25 mph
- Create standard guidelines for trail design within the Town's policies and guidance documents
- Developers should dedicate right-of-way and construction for trails within new developments and redevelopment projects
- Use native plants in landscaping along trails or multi-use paths
- Adopt outdoor lighting ordinances or codes
- Adopt a Complete Streets Ordinance and/or Program
- Encourage conservation / open space subdivision design within the Town
- Minimize the number of cul-de-sacs constructed and promote trail connections at existing cul-de-sacs
- Encourage land and easement acquisition along trail corridors to ensure permanent use as trails
- Support trail development in tandem with the implementation of riparian buffer restoration and mitigation projects
- Establish a trail classification system and accompanying trail use policies
- Establish trail wayfinding standards

**PROGRAM RECOMMENDATIONS**

- **Educational Programs**
  - Safe Routes to School
  - SCDOT Safety Tips for Biking and Walking
  - Environmental Education
- **Encouragement Programs**
  - Trail Network Map and/or Mobile App
  - Wayfinding Signage
  - Bicycle Parking
  - Bicycle Repair Stations
  - Bike/Walk to School Day Events
  - Bike/Walk to Work Day Events
  - Theme Biking/Walking Events
  - Prescribe-a-Trail/TrailRX/Walk with a Doc
  - Trail Scavenger Hunts/Geocaching
  - Greenway/Trail Ambassadors
  - Bicycle Friendly Community Designation
  - Walk Friendly Community Designation
  - Trail Town Program



**PROGRAM RECOMMENDATIONS**

- **Enforcement Programs**
  - Trail Watch Program
  - South Carolina Code of Laws Concerning Bicycles
  - South Carolina Code of Laws Concerning Pedestrians
  - South Carolina's Target Zero Project
- **Project Based Programs**
  - Pop-up Demonstrations/Pilot Programs
  - Paint the Pavement Programs
  - Trail Mural Programs
  - Adopt-a-Trail Segment
- **Evaluation Programs**
  - Trail Count Program
  - Trail User Surveys
  - Bike and Walk Audits
  - 311 Service/ See-Click-Fix



Painted sidewalk and crosswalk at 9th & Brevard  
Paint the Pavement Program, Charlotte, NC

**NEXT STEPS**

- Public Workshop (Feb. 8)
- Trail Network Prioritization
- Draft Recommendations Refinement
- Steering Committee Meeting #3 – Draft Plan Review (Spring 2023)
- Final Plan + Plan Adoption (April-May 2023)



Anne Springs Close Greenway

**QUESTIONS?**

Kathryn Zeringue, Bicycle + Pedestrian Planner, McAdams  
[zeringue@mcadamsco.com](mailto:zeringue@mcadamsco.com)

Haley Nafissi, Bicycle + Pedestrian Planner, McAdams  
[nafissi@mcadamsco.com](mailto:nafissi@mcadamsco.com)



Walter Y. Elisha Park Trail Loop

# Community Survey

12/2/22, 9:33 AM

Town of Fort Mill, SC - Report Creation

## Town of Fort Mill Trail Master Plan - Public Survey

Project Engagement

VIEWS	PARTICIPANTS	RESPONSES	COMMENTS
2,603	1,244	33,194	2,641

MOVE + TOGGLE COMMENTS FILTER BY SEGMENT OPTIONS

1. What is your relationship to Fort Mill? *Select all that apply.*

89%	I live in Fort Mill	1092 ✓
41%	I own property in Fort Mill	506 ✓
28%	I visit/spend time in Fort Mill	338 ✓
24%	I work in Fort Mill	289 ✓
5%	I own a business in Fort Mill	66 ✓
0%	None of the above	2 ✓

1,224 Respondents

16. Use the map to mark your home and/or work locations. *Mark the map with your home and/or work location using the icons below.* Note: Fort Mill's boundary is shown in red.

No data to display...

2. How do you currently use trails or multi-use paths in Fort Mill? *Select all that apply.*

90%	For health and exercise or recreation	1058 ✓
12%	Walk and/or bike to work and/or school	141 ✓
7%	I am not aware of any trails or multi-use paths in Fort Mill	86 ✓
7%	Run errands and/or reach essential services	85 ✓
4%	I do not use multi-use paths and trails.	48 ✓
2%	Other (please specify)	24 ✓
2%	Make transit connections	22 ✓

1,182 Respondents

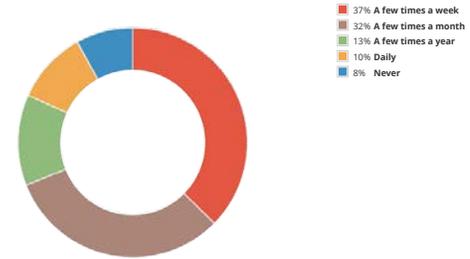
<https://publicinput.com/Reporting/ReportPreview/17425>

1/100

12/2/22, 9:33 AM

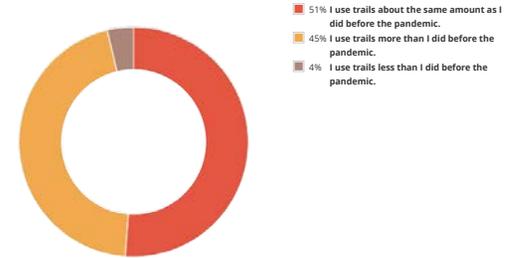
Town of Fort Mill, SC - Report Creation

3. How frequently do you use trails or multi-use paths in Fort Mill?



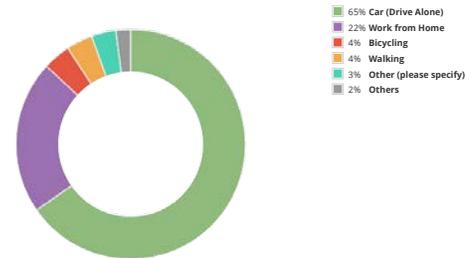
1,195 respondents

4. Have your trail habits changed during the COVID-19 pandemic?



1,163 respondents

5. What is your primary mode of transportation for commuting to work and/or school?



1,159 respondents

<https://publicinput.com/Reporting/ReportPreview/17425>

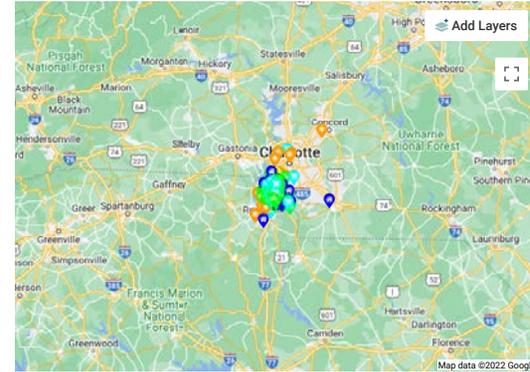
2/100

6. Now consider your desired commute in the future. Which modes would you like to use? *Select all that apply.*

45%	Bicycling	504 ✓
43%	Car (Drive Alone)	491 ✓
40%	Walking	451 ✓
35%	Work from Home	391 ✓
15%	Transit	166 ✓
6%	Carpool	64 ✓
3%	Other (please specify)	29 ✓

1,131 Respondents

16. What destinations in Fort Mill would you like to access by trails or multi-use paths? *Use the pins below to mark locations that you would like to reach via trail, such as where you live, work, shop, work out/play, go to school, and other.* Note: Fort Mill's boundary is shown in red and existing trails/multi-use paths are in green.



Build a connection walking bridge between Waterside trail and the River Walk in Rock Hill.

yesterday

Home

yesterday

Connect different neighborhoods near the Catawba Ridge High School

yesterday

x

2 days ago

Ball parks

2 days ago

I would very much appreciate this current trail system linking up with other neighborhoods along the river

2 days ago

Would like safe paths/sidewalks to connect my neighborhood (Catawba) to the larger town area

2 days ago

Would like to be able to do a destination walk/hike and safely get to downtown destinations

2 days ago

Access to Forest Creek middle school

2 days ago

Waterside

2 days ago

12/2/22, 9:33 AM

Town of Fort Mill, SC - Report Creation

Home

2 days ago

Pins aren't working on the phone. I would love a connection between Regent Park and Main Street. Also a safe path on Highway 21 and Gold Hill.

2 days ago

We need a sidewalk that goes to the schools from Waterside

2 days ago

Trails at home

2 days ago

Trail use in this neighborhood.

2 days ago

Trail use

2 days ago

Every Friday spring , summer , fall

2 days ago

4 graduates

2 days ago

Bebr

2 days ago

Home

3 days ago

School

3 days ago

Shopping and dining area

3 days ago

Local hospital

3 days ago

Local shopping and dining area we use

3 days ago

Local park

3 days ago

Farmers Market & Downtown Events

3 days ago

My kids school

3 days ago

Local Park

3 days ago

X

3 days ago

<https://publicinput.com/Reporting/ReportPreview/17425>

5/100

12/2/22, 9:33 AM

Town of Fort Mill, SC - Report Creation

we need some sort of trail to easily get here without having to drive

4 days ago

home

8 days ago

45 mile commute

8 days ago

X

10 days ago

I have some concerns about publicizing trails that cut through neighborhoods, opening them up to higher traffic in areas that are so close to existing homes. This isn't what the space was advertised to be used for when people bought homes that abut wooded areas. These wooded areas have little visibility and oversight and raises the chances of safety issues. There is no buffer area around parts of proposed public trails and existing private property. Seems a little unfair. I'd like to see open, public sidewalks and bike lanes improved and expanded around parks and schools or other public areas that were originally intended to be high traffic public access.

10 days ago

Greenway. Visit 3+ times per week in the summer

11 days ago

Founders Trail

11 days ago

X

11 days ago

Map was too clunky to use on my phone

11 days ago

Walk to work

12 days ago

Massey to Catawba River

13 days ago

It is also dangerous to walk from the back entrance of the FMHS down to Sutton Rd. Trails/sidewalks connecting the school to a safe way home would be great.

13 days ago

I would love a clear trail to connect my home to the Gold Hill schools. Right now there is no way to walk safely to the school.

13 days ago

Connect Catawba Park with existing trails.

13 days ago

Home

13 days ago

Lots of opportunities!

14 days ago

<https://publicinput.com/Reporting/ReportPreview/17425>

6/100

12/2/22, 9:33 AM

Town of Fort Mill, SC - Report Creation

Kingsley via bike  
14 days ago

Baxter village via bike  
14 days ago

Lite grocery via bike  
14 days ago

Brewery via bike  
14 days ago

Concerts in the park  
14 days ago

Pick up dinner via bike  
14 days ago

Grocery via bike  
14 days ago

Lite grocery shopping via bike  
14 days ago

Dam access  
14 days ago

Walking riding  
14 days ago

Work  
14 days ago

Mountain biking dog hiking  
14 days ago

Mountain biking  
14 days ago

Love the walking trails here!  
14 days ago

Mostly shop in Indian Land at the TJ Mxxx/Burlington/Home Goods outdoor shopping center. It's nice that there are multiple places to eat nearby as well. I don't do much shopping in Fort Mill as there are not good places nearby.  
14 days ago

Access along the Catawba River would give nearby trail access to a good portion of Fort Mill residents  
15 days ago

Anne Springs is a critical connection  
15 days ago

Catawba Park is a good connection  
15 days ago

<https://publicinput.com/Reporting/ReportPreview/17425>

7/100

12/2/22, 9:33 AM

Town of Fort Mill, SC - Report Creation

We are ft mill too  
15 days ago

There is currently no sidewalk between waterside at the Catawba and forest creek middle school and Catawba ridge HS even though the distance is small and many students walk back home from school  
15 days ago

X  
15 days ago

fun  
16 days ago

shop  
16 days ago

shop here, library  
16 days ago

kids play here  
16 days ago

We do Boy Scouts at St. Johns uMC  
16 days ago

We do a homeschool co-op at the Fort Mill Church of the Nazarene  
16 days ago

Home  
16 days ago

I live off of Doby's and it would be really nice to have a trail that went the whole distance and tied into the sidewalks  
16 days ago

Dinner  
16 days ago

Home  
Recreation  
16 days ago

Connecting thread trail at Nation Ford HS and Springfield Community to Little Sugar Creek greenway is of utmost excitement  
16 days ago

Home  
16 days ago

Walking  
16 days ago

X  
16 days ago

ride/walk along this path  
16 days ago

<https://publicinput.com/Reporting/ReportPreview/17425>

8/100

12/2/22, 9:33 AM

Town of Fort Mill, SC - Report Creation

ride and walk here.

16 days ago

hike here

16 days ago

hike here.

16 days ago

mountain bike and hike here.

16 days ago

Being able to access the ASC Greenway by foot or bike would be wonderful.

16 days ago

approximate home

16 days ago

grocery shop here.

16 days ago

Access to the Riverwalk trail system would be wonderful.

16 days ago

Fort Mill High School -- for the track and cross country teams, as well as kids going to and from school.

16 days ago

Main Street has become a very attractive and child-friendly destination for shopping and dining, but there is no safe way to access from Banks Street/Fairway Drive

17 days ago

The new Banks Athletic Park is an incredible addition to our community. While there are ample sidewalks and trails within the Waterside at the Catawba community, there are no safe trails or walkways to access the park from Banks Street/Banks Road or Fairway Drive. Our son has regularly scheduled baseball games at the park and it would be nice to be able to ride bikes or walk to games/practice and it would help with the limited parking situation when there are games on all fields.

17 days ago

Doby Bridge Park is a very short distance from our house but is unsafe to walk or bike to baseball practice and games due to lack of safe trail infrastructure on Fairway Drive and Doby's Bridge. There is a sidewalk on Doby's Bridge that comes to an abrupt end that could be extended.

17 days ago

Would like to be able to walk from Jones Branch RD to Kingsley for dining and shopping

17 days ago

Goddard School is a very short distance from our house but is unsafe to walk our daughter or ride bikes to school due to lack of safe trail infrastructure on Fairway and Doby's Bridge. There is a sidewalk on Doby's Bridge that comes to an abrupt end. Our family picks up trash regularly in the area where a sidewalk is absent.

17 days ago

There are walkways here that outside residents cannot access. I'd like to be able to use these walk ways.

17 days ago

<https://publicinput.com/Reporting/ReportPreview/17425>

9/100

12/2/22, 9:33 AM

Town of Fort Mill, SC - Report Creation

The complex is a very short distance from our house but is unsafe to walk our son or ride bikes to access for baseball practice due to lack of trail infrastructure.

17 days ago

There are walkways here that outside residents cannot access. I'd like to be able to use these walk ways.

17 days ago

We own Arrow & Bow Wealth Advisors on Market Street in Baxter. We also run a sanctioned GORUCK Ruck Club. When we are mapping out rucks (walks with rucksacks) for members, our options are divided by I-77. We either have to start on the Baxter side and stay on the Tega Cay side or start on the Kingsley side and stay on the Fort Mill side.

17 days ago

I'd like to be able to walk to this park from Jones BRanch Rd. There is currently no safe walk way or trail.

17 days ago

I'd like to be able to walk to this park from home.

17 days ago

Daughter will be walking to school but there is no safe walk way to get to Fort Mill High school from Jones Branch Rd

17 days ago

Kingsley has nice infrastructure for walking and biking, but is inaccessible by foot or bike from Doby's Bridge Road, Fairway Drive, Banks Street AND from the other side of 77. Our office is in Baxter on Market Street, but crossing the interstate to get to Kingsley for lunch is impossible because there is no safe way for pedestrians to get across.

17 days ago

We don't have any trails accessible to this neighborhood. Traffic on Harris RD makes it dangerous to walk on the side of the road as there is no shoulder.

17 days ago

We can walk to the Peach Stand from Walter Elisha Park or Main Street, but can't get from our house to Main Street safely because of the lack of trail/path infrastructure on Fairway Drive and Banks Street. The new 55+ community added sidewalks that extended down Banks a little bit but then it stops. Fairway Drive and Banks Street are both double yellow line roads with vehicles constantly driving over the speed limit. It is not safe for children to walk or ride their bikes or for families to stroll or push a stroller.

17 days ago

There is no possible way I would ever consider my child riding his bike to school with the lack of accessible and safe walking/biking/multiuse paths.

17 days ago

We live in Springfield and ride our bikes from here down to the Riverwalk - would like to be able to ride all over Fort Mill!

17 days ago

All over.

17 days ago

Lo

17 days ago

Not user friendly on mobile device

17 days ago

<https://publicinput.com/Reporting/ReportPreview/17425>

10/100

12/2/22, 9:33 AM

Town of Fort Mill, SC - Report Creation

No safe crossing from AO Jones to greenway  
17 days ago

This trail is never open  
17 days ago

Na  
17 days ago

Elementary and middle school  
17 days ago

Lots of kids here  
17 days ago

Trail from Waterside to forest creek  
17 days ago

Trail from waterside to nearby schools (River trail, forest creek, catawba)  
17 days ago

X  
17 days ago

I hike from here regularly  
18 days ago

I work here.  
19 days ago

X  
19 days ago

X  
19 days ago

X  
19 days ago

Would love to be able to ride bikes from Springfield community to Kingsley  
19 days ago

Home is in springfield  
19 days ago

walk trails at various site in Fort Mill including Anne Springs  
Greenway  
20 days ago

Gold Hill Middle School  
20 days ago

X  
20 days ago

X  
20 days ago

<https://publicinput.com/Reporting/ReportPreview/17425>

11/100

12/2/22, 9:33 AM

Town of Fort Mill, SC - Report Creation

I don't really have enough knowledge of what is able to be connected to come up with specific ideas. I would generally think where there could be trail connections from mega housing developments to their schools would be a good option. Also, large housing developments to nearby shopping centers and the greenway where possible. I feel like the major roads in town are too hazardous to really allow for safe pedestrian/bike crossings. If they are added, traffic lights are desperately needed to increase safety. I often see cyclists on Coltharp Road (which I consider exceptionally dangerous). I wonder if they are cycling to the greenway and if a trail could be added so cyclists no longer ride on the street there. A light and pedestrian crossing at Coltharp and 21 (at the primary entrance to the greenway) would be useful for increased safety.

20 days ago

doesn't work  
20 days ago

Walking to downtown fort mill needs better crosswalk options  
20 days ago

Can we make the greenway accessible by foot or bike?? Sidewalks and stop lights are needed at entrances  
21 days ago

Would love to access the greenway without having to drive. It's currently too u safe to cross over spring field parkway on foot or bike when cars are going 50mph  
21 days ago

I also wish I could bike to various local shops  
21 days ago

My student wishes she could safely bike to high school  
21 days ago

I love cycling all around town.  
21 days ago

Church  
21 days ago

Bike trails  
21 days ago

Shopping Baxter  
21 days ago

Shopping downtown  
21 days ago

Accessing Greenway to get to CLT / Pineville  
21 days ago

Bike riding fun  
21 days ago

Bike riding fun  
21 days ago

Home off dobys bridge road (N)  
21 days ago

<https://publicinput.com/Reporting/ReportPreview/17425>

12/100

12/2/22, 9:33 AM

Town of Fort Mill, SC - Report Creation

Pineville is work for me. I commute from Fort Mill pretty regularly.

21 days ago

access to Kingsley

21 days ago

Connecting existing trails is critical

21 days ago

Trails connecting schools city and other local areas of interest.

21 days ago

Pinning did not work properly for me from my phone.

21 days ago

I live in this neighborhood within walking distance to three schools, but the sidewalks on FortMill Parkway don't cross ditches/ravines/creeks and there aren't safe crosswalks. You can't walk very far before getting to a dead end

21 days ago

Unless maps change a lot between now and then, my son will go to Fort Mill Elementary and Middle school, which should be within walking distance but doesn't seem safe with the traffic on Springfield Parkway

22 days ago

My son's preschool is 1.1 miles from our front door to the school, but there is no way I would feel safe biking him to school along Springfield Parkway.

22 days ago

Would love to be able to ride bikes to downtown safely

22 days ago

Work!

22 days ago

Home, while technically in the county, is surrounded by Town

22 days ago

Bike to reiverwalk for restaurants and recreation

22 days ago

Bike to Trader Joe's.

22 days ago

Bike and/or hike to Anne Springs for recreation.

22 days ago

Reside in Carolina Orchards. Bike from here to downtown Fort Mill via Old Nation as well as to Freedom Park in CLT, Noda in CLT, Southend in CLT

22 days ago

Connection to little sugar creek trail headed into Charlotte or McAlpine Creek Trail to Trader Joes

22 days ago

Anne Springs should be one of the stops people go to without a doubt

22 days ago

<https://publicinput.com/Reporting/ReportPreview/17425>

13/100

12/2/22, 9:33 AM

Town of Fort Mill, SC - Report Creation

Connect other trails to parks

22 days ago

Main Street- dining and shopping

22 days ago

School

22 days ago

Connect Masons Bend to shopping access of Baxter Village via path or sidewalk on sutton

22 days ago

Sidewalk along fort mill bypass

22 days ago

Sidewalk leading to school

22 days ago

School sidewalk

22 days ago

X

22 days ago

X

22 days ago

Get to Anne Springs by bike from North of I77 carowinds boulevard

22 days ago

Home

22 days ago

Access schaeffler from carowinds boulevard North of I77

22 days ago

connect from downtown ft mill to riverwalk trail here?

23 days ago

connect these two paths/trails here?

23 days ago

restaurants shopping and downtown events

23 days ago

Harris Street park

23 days ago

restaurants shopping and events downtown

23 days ago

Home

23 days ago

School

23 days ago

<https://publicinput.com/Reporting/ReportPreview/17425>

14/100

12/2/22, 9:33 AM

Town of Fort Mill, SC - Report Creation

Dobys Bridge to Main Street and Dobys to Elizabeth and Elizabeth to Main Street  
23 days ago

Work uptown Charlotte  
23 days ago

Live near Walter Elijah  
23 days ago

hike at the greenway  
23 days ago

Shopping and entertainment at Kingley. Walk through often from my neighborhood through Walter Elijah  
23 days ago

Walk almost daily at this park  
23 days ago

Not able to get this to work well  
23 days ago

School events  
23 days ago

School events  
23 days ago

Shopping  
23 days ago

School  
23 days ago

School, need walking trails around these 4 schools in area.  
23 days ago

The sidewalks between the schools stop. It looks like pedestrian bridges need to be added on both side of Fort Mill pkwy.  
24 days ago

This trail has been abandoned, why? It would serve as fantastic connector to the new greenway that connects up into Pineville!  
24 days ago

PLEASE ask Springs to open this side to bikes. It would be great to have access to the Cantina.  
24 days ago

If this trail is still there, it needs revitalizing.  
24 days ago

Need sidewalk fully added to Doby's Bridge road. There are 2 daycares on this road and it's too dangerous for strollers/walkers right now.  
24 days ago

Home  
24 days ago

<https://publicinput.com/Reporting/ReportPreview/17425>

15/100

12/2/22, 9:33 AM

Town of Fort Mill, SC - Report Creation

Rumor is new food and shopping is going in here. Would be great if residents of Massey and Waterside could connect to it without getting on Fort Mill pkwy  
24 days ago

Na  
24 days ago

This is hard to do on a phone.  
24 days ago

Home - would love a connecting path to 160  
24 days ago

Downtown  
24 days ago

Lowes  
24 days ago

Walter Elisha  
24 days ago

Carolina Thread Trail  
24 days ago

Baxter Trails  
24 days ago

HT  
24 days ago

PF  
24 days ago

HT  
24 days ago

ASCG  
24 days ago

YMCA and ASCG  
24 days ago

ASCG!  
24 days ago

NAFO  
24 days ago

SES  
24 days ago

SMS  
24 days ago

work  
24 days ago

<https://publicinput.com/Reporting/ReportPreview/17425>

16/100

12/2/22, 9:33 AM

Town of Fort Mill, SC - Report Creation

home

24 days ago

Carowinds is not easily accessible unless you drive.

24 days ago

would love to see a trail that would connect to Riverwalk from the Lake Wyllie Dam

24 days ago

great trail, would be awesome if it would connect to Baxter and Lake Ridge trails

24 days ago

Path connecting Whitegrove to the sidewalk on 160

24 days ago

shop

24 days ago

lots of traffic would cross over by 160 from baxter to kingsley if they didn't have to go across half a dozen traffic intersections and had a dedicated foot bridge over the freeway

24 days ago

nine intersections in this mile, but foot traffic from baxter to kingsley would be awesome without using the streets

24 days ago

workout groups start here and go find trails

24 days ago

we use DT fort mill to do walks and such

24 days ago

Greenway

24 days ago

Integrated master plan with surrounding communities, such as Rock Hill and the Riverwalk

24 days ago

Integrated master plan with surrounding communities, such as Tega Cay

24 days ago

Several connections to Catawba river, and along the river itself

24 days ago

Pedestrian connectivity to high schools and surrounding open space and communities, such as between Ann Springs, nation Ford HS and Springfield

24 days ago

Better pedestrian accessibility between existing and future neighborhoods through transportation corridors. With some separation from actual roadway for safety, and to make it more pleasant for pedestrians.

24 days ago

More pedestrian accessibility to and throughout downtown

24 days ago

<https://publicinput.com/Reporting/ReportPreview/17425>

17/100

12/2/22, 9:33 AM

Town of Fort Mill, SC - Report Creation

X

24 days ago

Ann Springs

24 days ago

Connect River Trail Elementary, Catawba Ridge High School and Forest Creek elementary with sidewalks/trails. Could maybe even connect Banks Trail Middle School as well.

24 days ago

Can we get some trail running through this power line easement down the river?

24 days ago

Would like to be able to bike to here for shopping and dining.

24 days ago

There is some land for sale around here in large tracts. Please preserve this with new trail systems!

24 days ago

Connect this to the James K Polk Greenway along Little Sugar.

24 days ago

A trail following Sugar Creek all the way from Pineville to the Catawba is needed.

24 days ago

This trail needs to connect to the Baxter Trail system to allow travel from the dam to River Park.

24 days ago

Need a way to get from here to Anne Springs/Downtown on a trail.

24 days ago

Mecklenburg County

24 days ago

Ride 160 from 21 all the way to Tega Cay then 460 crossing over 21 at the light to continue back to 21 at QT (corner of 21 n 460) and then to FM park way to back home off 21. It's a 16 mile route that isn't always bike friendly. No shoulders, no bike lane n al though the state law allows cyclists to take the car lane to be safe, not all motorists are aware of the law n those that aren't, can get quite irate with you, causing unsafe conditions for both the cyclist and the motorists. Another area that needs improvement is the hill on 21 just before the Fort Mill DMV is located. That section of road really needs to be repaired (bottom of the hill to the dmV) is really bad for to cars and cyclists in the way the road has risen up at the light.

24 days ago

River walk, Baxter

24 days ago

Commercial Area

24 days ago

Peach Stand

24 days ago

The neighborhoods along North Dobys Bridge can not get anywhere in Fort Mill without using that busy road. Many spots have no sidewalks or shoulders making it very unsafe for walkers and bikers. Maybe a trail between the neighborhoods or through the gold course would help.

24 days ago

<https://publicinput.com/Reporting/ReportPreview/17425>

18/100

12/2/22, 9:33 AM

Town of Fort Mill, SC - Report Creation

There are no trails in or out of our neighborhood. Maybe something could be added to the FM golf course so we can get to downtown FM.

24 days ago

X

25 days ago

existing parking, limited sidewalk access to other locations.

25 days ago

CRHS

25 days ago

miles of existing sidewalk and cart paths to run/bike

25 days ago

FCMS

25 days ago

DBES

25 days ago

We currently have to drive to everything but the elementary school

25 days ago

Shopping/dining

25 days ago

Primary park

25 days ago

Masons Bend

25 days ago

Connect Baxter to Masons Bend

25 days ago

Sutton has no sidewalks in the area between Dam Road and 77. The Masons Bend community has been looking for safe ways to connect to Baxter and their businesses but we cannot get there with children without a vehicle.

25 days ago

Sutton has NO sidewalks in this area and is so dangerous! Trails would allow these neighborhoods to visit baxter and other areas of interest safely.

25 days ago

Masons Bend community needs to be connected to other areas via trails. Sutton is way to dangerous and has NO sidewalks so we are cut off from the rest of the community.

25 days ago

A walking path to Steel Street Park would make accessing downtown from the Greenway easier.

25 days ago

Connecting the two Greenways makes sense since they are in such close proximity.

25 days ago

<https://publicinput.com/Reporting/ReportPreview/17425>

19/100

12/2/22, 9:33 AM

Town of Fort Mill, SC - Report Creation

Linking the Anne Springs Close Greenway to Nation Ford High School with a raised walking path would help pedestrians on the side of the highway connect to the Greenway without having to drive. Sidewalks exist in this area already and would link Anne Springs Close Greenway with Nation Ford Greenway which is underused and underdeveloped.

25 days ago

Work

25 days ago

X

25 days ago

X

25 days ago

Connect Greenway with Kingsley in a way that is safe for pedestrians and bikers across 21

25 days ago

Connect the Baxter trails with Riverwalk. Requires easement along Sutton Road.

25 days ago

Springfield Parkway sidewalk/bike lane.

21, though people drive too fast, bike lane Old 21, repaved with bike space

25 days ago

x

25 days ago

Downtown Fort Mill for Recreation / Dining

26 days ago

Catawba Park

26 days ago

Various Baxter Trails

26 days ago

Riverwalk Trail

26 days ago

Catawba River Access

26 days ago

Pathway along 160 by Peach Trees

26 days ago

Market St. Food...

26 days ago

Trails by YMCA

26 days ago

Bike parking and designated bike trails would be helpful

26 days ago

<https://publicinput.com/Reporting/ReportPreview/17425>

20/100

12/2/22, 9:33 AM

Town of Fort Mill, SC - Report Creation

lunch

26 days ago

River access - Waterside at the Catawba area

26 days ago

Forest Creek & Catawba Ridge sidewalks from Dominion Bridge

26 days ago

Dominion Bridge to Forest Creek & Catawba Ridge

26 days ago

Would like trails or paths we could use to walk to school from the back of the neighborhood

26 days ago

This is about as far as I have or would walk or bicycle. Of course today it is not safe to walk across I77 on 160

26 days ago

X

26 days ago

Connections between neighborhoods and safe way to cross streets

26 days ago

Add trail system along Fort Mill parkway to safely access schools, shopping and for recreation

26 days ago

walking trail to shopping

26 days ago

Trail along Fort Mill Parkway and safe crossing under or over Fort Mill parkway

26 days ago

Trail from Nim's Village to River Trail Elementary

26 days ago

Work commute would be improved if I could access downtown from a trail network or sidewalk from east part of Fort Mill. Currently I am 2.5 miles from downtown but have to drive and take up parking spot for even quick tops. This often prevents me from stopping at Olives for a coffee whereas I would bike almost daily.

26 days ago

Very difficult to get here unless you hop into car for a 2 minute drive. No crosswalk signage, no sidewalks, no lights.

26 days ago

Public access to the Catawba River from Fort Mill! Who wouldn't want this? Run and bike for days!

26 days ago

Currently no easy way to access Forest Creek and Catawba Ridge HS aside from very short shoulder on busy road.

26 days ago

I would love to ride my bike or run downtown from my home alone Dobys Bridge but there are no sidewalks. I would use this nearly everyday to go to Hobos and Olives.

26 days ago

<https://publicinput.com/Reporting/ReportPreview/17425>

21/100

12/2/22, 9:33 AM

Town of Fort Mill, SC - Report Creation

Congestion makes it difficult to turn left in these areas. Ability to walk to restaurants, grocery, and shops works be great!

26 days ago

Would be great is our kids could walk to school at Forest Creek and Catawba Ridge. Currently the road is too busy and has very narrow shoulders that prevent kids from walking to school and events. Ability to walk world cut down on morning and afternoon congestion considerably!

26 days ago

Would be wonderful for my children and those in my community to be able to walk to schools that are across the street, attend school events without driving 2 minutes and creating traffic on an ashtray busy road.

26 days ago

N/-

26 days ago

X

26 days ago

Entertainment/arts

26 days ago

Recreation

26 days ago

Shopping

26 days ago

Shopping

26 days ago

This trail has not been available for the past 5 years. I miss it.

26 days ago

Home

26 days ago

Connect Fort Mill to Baxter on 160 via sidewalk or bike/walk lane.

Bike lane on Fort Mill parkway(460)

26 days ago

This would be great to connect a riverfront path to Mason's Bend and ultimately Riverwalk

27 days ago

Trail system built by neighbors in neighborhood

27 days ago

Clicking on pins on an android tablet does not work

27 days ago

A safe way to access the Complex YMCA would allow recreation to start from your front door and eliminate the need to drive. This could be achieved with a bike lane on Tom Hall Street.

27 days ago

<https://publicinput.com/Reporting/ReportPreview/17425>

22/100

12/2/22, 9:33 AM

Town of Fort Mill, SC - Report Creation

A safe path along the Fort Mill Parkway/bypass would make the Founders Trail entrance accessible via bike. Right now it is extremely dangerous to ride on the Parkway/bypass. This is an area that could be greatly improved.

27 days ago

A path on 460 would make the Adventure Road entrance safely accessible via bike.

27 days ago

A path along bypass 21 would make access to the main ASC Greenway accessible via bike.

27 days ago

Trails/bike paths/sidewalks or bike lanes would be helpful along all major roadways in fort mill, 21 and business 21 for getting around town safely

27 days ago

Expansion of trails in this area linking with Walter Elisha Park.

27 days ago

Regent park connection/bike lanes would be great to connect to greenway

27 days ago

On Regent Park, by Saxon Place

27 days ago

ability to bike to friend's house in next door communities without taking car

27 days ago

ability to access greenway from bike

27 days ago

Ability to access shopping from house

27 days ago

Ability to access this areas from bike

27 days ago

Ability to get to this area from my house on bike. Either pedestrian bridge over Catawba, or trail system

27 days ago

Ability to leave community on a bike to access shopping or other residences/communities

27 days ago

X

27 days ago

Develop the area along the Catawba River with public trails and dining/shopping like Rock Hill's Riverwalk or Greenville's River District.

27 days ago

X

27 days ago

Map is difficult to "Pin" other

27 days ago

X

27 days ago

<https://publicinput.com/Reporting/ReportPreview/17425>

23/100

12/2/22, 9:33 AM

Town of Fort Mill, SC - Report Creation

X

27 days ago

X

27 days ago

X

27 days ago

I live near Regent Park, grocery shop at Food Lion Regent Park and Harris Teeter Springfield, go to Home Depot and Lowes Rock Hill, Fort Mill, and Pineville, SC

27 days ago

Home

27 days ago

X

27 days ago

X

27 days ago

X

27 days ago

X (too small of a map to zoom in enough)

27 days ago

A bike path along the bypass would greatly enhance our quality of life and allow us to bike to the Anne Springs Close greenway instead of driving.

27 days ago

Bike trail use ~2 times per week

27 days ago

X

27 days ago

x

27 days ago

Greenway Adventure

27 days ago

Need sidewalk on Colthrop to shopping and Anne Springs Greenway, please!

27 days ago

X

27 days ago

School

27 days ago

Home

27 days ago

<https://publicinput.com/Reporting/ReportPreview/17425>

24/100

12/2/22, 9:33 AM

Town of Fort Mill, SC - Report Creation

Pleasant Knoll schools. Cant get to it by sidewalks or trails from Gold Hill. Child has died on this road near the schools because of how unsafe it is.

27 days ago

connect bypass neighborhoods to greenway

28 days ago

Would love connecting trails to Waterside

28 days ago

Home

28 days ago

We would love to bike somewhere from near the Catawba river! Please give us sidewalks and trail access! We can see river walk trail but can't get there or anywhere on foot right now. Banks and Whites and Dobys need pedestrian access

28 days ago

Access to the founders trail and river walk thru the woods between banks and 21

28 days ago

Downtown fort mill via Banks road or Dobys Bridge

28 days ago

Catawba ridge and forest creek

28 days ago

Home

28 days ago

School

28 days ago

Would love a trail that connects in with my neighborhood so that I don't have to drive to the trailhead

28 days ago

x

28 days ago

Dining

28 days ago

And a way to get to our downtown - you can't get from teh greenway to downtown to Kingsley/Baxter safely.

28 days ago

Same here - has to be a way to get here with sidewalks or trails.

28 days ago

Has to be a way to get to this school using safe sidewalks or trails!

28 days ago

Extend sidewalks/trail system from Springfield neighborhood to Springfield Town Center

28 days ago

Connect trail from shopping center to Springfield neighborhood

28 days ago

<https://publicinput.com/Reporting/ReportPreview/17425>

25/100

12/2/22, 9:33 AM

Town of Fort Mill, SC - Report Creation

Access from Springfield neighborhood

28 days ago

Wouldn't allow me to drop a pin. 160 to Springfield Pwky along Old Nation Rd.

29 days ago

X

29 days ago

Trails leading to areas of recreation

one month ago

I live at Waterside at the Catawba and currently have to drive everywhere. As new retail development goes into Elizabeth and other places nearby it would be nice to be able to bike, run or walk to them.

one month ago

ASC Greenway - be able to access this in ways other than having to drive there

one month ago

Kingsley

one month ago

Shopping, grocery stores, and the green way

one month ago

Here

one month ago

X

one month ago

exercise

one month ago

home

one month ago

exercise

one month ago

School

one month ago

X

one month ago

Downtown and Kingsley

one month ago

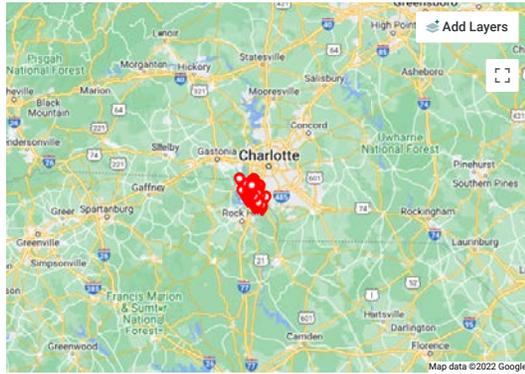
<https://publicinput.com/Reporting/ReportPreview/17425>

26/100

12/2/22, 9:33 AM

Town of Fort Mill, SC - Report Creation

17. Which locations (streets, intersections, neighborhoods, etc.) need the most bicycle and pedestrian infrastructure improvements in Fort Mill? Please use the pin option on the map to mark your locations. Note: Fort Mill's boundary is shown in red.



X

yesterday

Connect Waterside trails

yesterday

Meant to select 160 @ Peach Stand and near Walter Elisha Park. Plus 21 Bypass near the Peach Stand. Could not delete what was pinned.

yesterday

Connect neighborhood trails around or near Catawba Ridge High School.

yesterday

Pedestrian sidewalks needed on both sides of Whites Road to at least Fort Mill Parkway

2 days ago

Pedestrian sidewalk needed on both sides of the road. All of Whites Road

2 days ago

Pedestrian sidewalk needed on both sides of road

2 days ago

People cross to shop at Walmart and there is no safe pedestrian crossings.

2 days ago

Pedestrian/bike crossing from Kingsley across I77 overpass to sidewalk at Baxter needed.

2 days ago

Pedestrian crossing needed at Bozeman.

2 days ago

<https://publicinput.com/Reporting/ReportPreview/17425>

27/100

12/2/22, 9:33 AM

Town of Fort Mill, SC - Report Creation

I have seen cars bump pedestrians and cyclists at this intersection. Lots of walkers from Paradise/Whiteville Park using sidewalks to downtown and to Walmart need more protection at Steele St and Doby's Bridge intersections.

2 days ago

Access to forest creek middle school

2 days ago

Highway 21 and 160 crossing the highway.

2 days ago

Need a sidewalk and/or bike path

2 days ago

With the shopping center (7-11 and Harris Teeter coming), it would be great to have a sidewalk from White's Road to those areas on Fort Mill Parkway. That would make it safer for students and families that choose (or have) to walk to those locations

2 days ago

White's Road needs sidewalks. With all of the kids that attend Catawba Ridge & Forest Creek, it is necessary. The high schoolers run this road and I'm so afraid that someone is going to get hurt one day. White's Road was not adjusted to handle 1000 homes and two schools.

2 days ago

Sidewalks needed on Banks Trail.

2 days ago

This trail needs to connect to Mason's Bend and Waterside at Catawba neighborhoods.

2 days ago

No sidewalks

2 days ago

Need sidewalks to get to schools

2 days ago

springfield parkway needs bike lanes

2 days ago

springfield parkway needs a bike lane

2 days ago

No sidewalk along Harris or Tom Hall

3 days ago

No sidewalks along 21 allowing walking between high schools/elementary schools and residential developments

3 days ago

No sidewalks to link Masons Bend to Baxter Village/Catawba Park

3 days ago

No bike/pedestrian access to Catawba Park

3 days ago

No pedestrian/bike crossing or access down New Gray Rock Road to access new Catawba park

3 days ago

<https://publicinput.com/Reporting/ReportPreview/17425>

28/100

12/2/22, 9:33 AM

Town of Fort Mill, SC - Report Creation

Dangerous intersection where cross time is not long enough if not running.  
3 days ago

This intersection has NO pedestrian crossing at all and is extremely dangerous  
3 days ago

X  
3 days ago

X  
10 days ago

bike lanes and side, tree shaded sidewalks or paved paths.  
10 days ago

bike lanes and safe sidewalks  
10 days ago

We desperately need sidewalks on Doby's Bridge from the bypass to Tom Hall Street.  
11 days ago

Difficult street on bike (Clebourne)  
12 days ago

Cross to get to work  
12 days ago

There needs to be an improved crosswalk here, where cars know to stop for pedestrians.  
13 days ago

There needs to be an improved crosswalk here.  
13 days ago

There is a lot of good shopping here, but no nearby pedestrian crosswalk, and the cars do not pay any attention to pedestrians.  
13 days ago

Crossing the 77 near Baxter is very dangerous. There are no sidewalks.  
13 days ago

Entire 21 route needs off street bike path  
14 days ago

Entire 460 route needs off street bike path  
14 days ago

Bike path!  
14 days ago

Entire bypass needs a safe, off street bike path.  
14 days ago

Challenging to cross I-77 on/off ramps  
14 days ago

Major greenways are the way to start this program. Build connectors based on demand later.  
15 days ago

<https://publicinput.com/Reporting/ReportPreview/17425>

29/100

12/2/22, 9:33 AM

Town of Fort Mill, SC - Report Creation

X  
15 days ago

Along Dobys Bridge. Esp as it gets closer to Tom hall. We would ultimately like to be able to get to catawba ridge hs and all the neighborhoods i. Between safely from Oakland point.  
15 days ago

Need path for k&ds to walk to school  
16 days ago

Hard time selecting a pin. Northern part of fort mill on Regent parkway needs better infrastructure and better ways to connect to town and southern parts of fort mill  
16 days ago

Riding on Regent Pkwy is hairy  
16 days ago

Baxter village to Kingsley  
16 days ago

From Baxter village to main street  
16 days ago

Pedestrian crosswalk needed on overpass over I-77 (along 160)  
16 days ago

Needs sidewalks  
16 days ago

Needs bike and side walks  
16 days ago

Needs bike lanes  
16 days ago

Down banks street to down town  
16 days ago

Sidewalks would be nice on Fairway Dr  
16 days ago

nice short path provided by neighborhood but doesn't go anywhere. Lots of growth around the bypass. Imagine if there was a path nearby that went for longer distances. It might connect to Harris Teeter area  
16 days ago

Nice path out here, but doesn't go far and does not connect. Would be great to have a path that connect neighborhoods and even provides ways to commute from neighborhood to schools. Rides along the bypass with families would be nice.  
16 days ago

the sidewalk here is in shambles. Would be great if it went all the way up to bypass and was rideable.  
16 days ago

no side walk here and no established pedestrian lane across bridge.  
16 days ago

<https://publicinput.com/Reporting/ReportPreview/17425>

30/100

12/2/22, 9:33 AM

Town of Fort Mill, SC - Report Creation

FM High School

16 days ago

Bike parking needed near Main street. I asked a policeman once where I could find bike parking during a main street event and he had no idea - he just had me park my bike on a tree in front of the police station.

16 days ago

I would like to be able to ride my bike this way towards Indian Land, but it seems like I'd be risking my life if I tried.

16 days ago

Sidewalk down Business 21 needs better ramps at the turnoff spots, and lots of pavement fixes.

16 days ago

This town boundary line is wild. Is it so that the work done in this plan won't include anything outside of these boundaries? Including the random circles that are completely enclosed by FM?

16 days ago

The entire I-77 overpass.

16 days ago

it appears none of the areas near me are even an option.

17 days ago

walking and biking access needed.

17 days ago

Sidewalk and bike lanes needed from waterside at the Catawba to Forrest Creek and Catawba High School

17 days ago

Doby's Bridge road needs sidewalks

17 days ago

21 needs bike/ped infrastructure. Especially between anne close and the river.

17 days ago

I think the bike routes need their own bike lane next to the road instead of using the road.

17 days ago

NA

17 days ago

Bike lane at white road from waterside to nearby schools

17 days ago

From Waterside at the Catawba to forest creek middle and Catawba ridge high and river trail elementary

17 days ago

There are no sidewalks on Park Drive.

17 days ago

Increased usage of Calhoun Street & Walter Elijah Park is causing traffic conjection on Sidney Johnson as homeowners have to park on the street and traffic can only pass in 1 direction at a time.

17 days ago

<https://publicinput.com/Reporting/ReportPreview/17425>

31/100

12/2/22, 9:33 AM

Town of Fort Mill, SC - Report Creation

Lighted crosswalks needed from Steele Circle to Steele St Park and additional crosswalks/sidewalk improvement throughout this neighborhood. Heavy foot traffic by kids in this area.

17 days ago

X

17 days ago

X

19 days ago

X

19 days ago

Connecting parks and shopping plazas to neighborhoods is what I'd like to see. Springfield community to Downtown Fort Mill or to Kingsley/Hospital area would be a very nice bike ride but traveling on Springfield Parkway and on 21 would be risky in certain places but a delineated bike path would be wonderful.

19 days ago

x

20 days ago

Mr. Puttys Fun Park

20 days ago

need improved biking access on raods

20 days ago

for bike access

20 days ago

Sidewalks and light needed

21 days ago

It's impossible to access this entrance without a car. Sidewalks and street lights are needed

21 days ago

Let's connect all sides of town, and surrounding towns as well

21 days ago

This side of town needs some bike paths

21 days ago

Need to be able to ride along new gray rock

21 days ago

access from existing trails north to points of shopping and parks

21 days ago

mup along dobys. Already a lot of travel on natural surface paths b/w neighborhoods

21 days ago

Need a bridge over little sugar creek/sugar creek

22 days ago

<https://publicinput.com/Reporting/ReportPreview/17425>

32/100

12/2/22, 9:33 AM

Town of Fort Mill, SC - Report Creation

Need a trailhead in this location currently occupied with small York County shed. Existing trail goes inside the rail road in this area to connect to little sugar creek.

22 days ago

Need biking, pedestrian infrastructure on old nation. Can be a trailhead as well as trail to connect along old nation to downtown fort mill. If old nation is connected through Carolina orchards and regent park to little sugar creek we can have a fort mill destination

22 days ago

So many bikers in fort mill= more bike friendly paths!

22 days ago

So many bikers in fort mill= more bike friendly paths!

22 days ago

There is an old trail that is falling apart and with recent sewer line construction it has broken the path even more that it was prior.

22 days ago

X

22 days ago

X

22 days ago

Bike lanes would be a great addition to key roads and streets.

23 days ago

Bike Lane on 460

23 days ago

Bike Lane on 160

23 days ago

Needs sidewalk and safe crossing to ASC Greenway entrance.

23 days ago

Needs sidewalks/trail so people can bike from Springfield to Springfield Shopping Center.

23 days ago

160 should have a proper bike path all through fort mill as well as hwy byp 21 and business 21 and 460

23 days ago

Harris street park needs signal / access across harris street to path to walter elisha

23 days ago

need to connect across 160 to walter elisha park

23 days ago

X

23 days ago

Need better school transport

23 days ago

<https://publicinput.com/Reporting/ReportPreview/17425>

33/100

12/2/22, 9:33 AM

Town of Fort Mill, SC - Report Creation

Would love to have connector to Baxter/ Tega Cay area and a walking trail/ sidewalk to Rock Hill River Walk

23 days ago

No safe way to cross at I 77 from town of FM toward Baxter/ Tega Cay

23 days ago

Need to connect Spratt St.to the paths along the Catawba

23 days ago

They ate needed all over the area!

23 days ago

All streets need sidewalks or breakdown lanes

23 days ago

Getting across 77

23 days ago

Currently a dirt trail with poor drainage, needs a short sidewalk as all of Massey uses this to safely get to 7Eleven. This will be a key connector once the Harris Teeter shopping center is built across the street.

24 days ago

add sidewalks to all of Doby's Bridge rd

24 days ago

Trail needs to be revitalized

24 days ago

Would be nice to add a walkway or trail near between Oakland Pointe Community, YMCA and Walmart Neighborhood

24 days ago

Na

24 days ago

York County has a Bike Route through Fort Mill, Route 4. It seems logical to direct improvements along this route that supports Biking and pedestrian activity.

24 days ago

The entire north and south parkway area.

24 days ago

bike lane on 160

24 days ago

Put a bike lane on Hwy 460

24 days ago

Put a bike lane on Fort Mill Pkwy

24 days ago

Spratt St needs a bike lane from Hwy 21 all the way to downtown.

24 days ago

Sutton Road to Hwy 21 needs a bike lane.

24 days ago

<https://publicinput.com/Reporting/ReportPreview/17425>

34/100

12/2/22, 9:33 AM

Town of Fort Mill, SC - Report Creation

Pleasant Road from Carowinds to Hwy 160  
24 days ago

Pleasant Road from Carowinds all the way to 160, then down Sutton Road to Hwy 21.  
24 days ago

This bridge over I77 is so dangerous for a bike rider, would love to see some consideration for a small bike lane when the new bridge arrives  
24 days ago

Downtown areas need bike lanes wherever possible or bike sharing signage on roadsides.  
24 days ago

X  
24 days ago

X  
24 days ago

Pleasant Road from 160 to Carowinds  
24 days ago

pleasant road from 160 to Carowinds  
24 days ago

While picking up my son from Nation's Ford HS, I have seen kids walking along side Springfield Parkway between the high school at AO Jones Blvd and Hwy21. Sidewalks would help them stay safe.  
24 days ago

With the active community at Carolina Orchards, a sidewalk connecting the existing one at their location to the shops nearby would likely help everyone involved.  
24 days ago

I see people walking alongside Carowinds Blvd between the state line and I77 but there are no sidewalks. They walk in the grass. With all the traffic that's a little dangerous.  
24 days ago

Better connectivity and improved pedestrian environments along 160 from Baxter all the way to downtown and throughout downtown itself  
24 days ago

Pedestrian connectivity to high schools and surrounding open space and communities, such as between Ann springs, nation Ford HS, Springfield and Carolina Orchards  
24 days ago

There is a small section along Hwy 160 just east of the Hwy77 overpass where there is no sidewalk at all.  
24 days ago

The sidewalks along Hwy 160, both sides, between Gold Hill Rd all the way to Walter Elisha Park are VERY heavily used. They could stand to be widened to get users a safer distance away from the traffic, as well as repaired in some sections. Plus, vegetation growth in some areas needs to be controlled.  
24 days ago

The sidewalk along Gold Hill Rd, both sides, are in dire need of repair.  
24 days ago

<https://publicinput.com/Reporting/ReportPreview/17425>

35/100

12/2/22, 9:33 AM

Town of Fort Mill, SC - Report Creation

A sidewalk on Pleasant Rd between Altura Rd all the way to Hwy 160 would be great.  
24 days ago

Need way to get to Haigler entrance of ASCG on bike safer.  
24 days ago

This road is a death trap for bikes. It needs a paved trail next to the road for bike traffic.  
24 days ago

Sidewalks and crosswalks desperately needed up Whites Rd.  
24 days ago

This stretch of 21 has shoulder but no sidewalk or trail to get from Rock Hill to Fort Mill. Very dangerous.  
24 days ago

Connections needed between Springs trail system and Riverwalk trail system to connect York County through Fort Mill. Bypass 21 & new 460 alignment to connect to Founders Trail System & Riverwalk Trail System to connect N/S Thread Trail Connections.  
24 days ago

Connection To Little Sugar Creek Greenway at Polk & State Line.  
24 days ago

See above comment  
24 days ago

n/a  
24 days ago

We need more sidewalks connecting our neighborhoods.  
24 days ago

It would be great if the middle & high schoolers could bike to school. We need shoulders and/or sidewalks  
24 days ago

We need sidewalks or shoulders all the way down N. Doby's Bridge  
24 days ago

X  
25 days ago

<https://publicinput.com/Reporting/ReportPreview/17425>

36/100

12/2/22, 9:33 AM

Town of Fort Mill, SC - Report Creation

Safe passage over 77 and 160 to Kingsley and ASG

25 days ago

N/a

25 days ago

This intersection frequently holds up traffic and does not have any shoulders to connect Sutton road to any of the Baxter walking/bike paths.

25 days ago

This area is not bike, pedestrian or accessible to an elderly man who rides his wheelchair against traffic daily to access the convenience store. With the addition of Bojangles, having access to these businesses without a car would be a big benefit

25 days ago

None.

25 days ago

Safe pedestrian walkways are needed along Sutton to connect neighborhoods.

25 days ago

No sidewalks here

25 days ago

No sidewalks in this area. Very dangerous roads for anyone on a bike or trying to walk.

25 days ago

Needs to connect Masons Bend to the rest of the community via trails along sutton.

25 days ago

X

25 days ago

Bridge at I-77 and 160

25 days ago

X

25 days ago

Baxter to Kingsley to Greenway

25 days ago

Needs a connection to Baxter Trails and to the new Tega Cay park that is very close by.

25 days ago

Tried to mark the different routes I would use to get from my neighborhood to the grocery store, Downtown, Baxter, etc. It's not safe between small roads without a shoulder and big, highway like corridors like 160.

25 days ago

160 & I-77 is priority #1

Harris St Park crossing is so unsafe for children.

Elisha Park crossing over 160 where the new Bossy Chicken Place is going.

21 in front of forest ridge please put a sidewalk those kids have been walking 21 to school or Harris park for years with cars doing 55-60mph past them.

25 days ago

<https://publicinput.com/Reporting/ReportPreview/17425>

37/100

12/2/22, 9:33 AM

Town of Fort Mill, SC - Report Creation

Getting across 77

25 days ago

Make it easy for the TC peninsula to get to Catawba Park, and for the communities to get to GHMS and GHES.

25 days ago

Connect Baxter to Catawba Park

25 days ago

all areas of fort mill need better access

25 days ago

Bike path needed desperately on 160 from end to end. Completely inadequate for 2022 going into 2023. Stop spending on donut eating empty brains sitting in police cars.

25 days ago

Bike path needed desperately. Completely inadequate for 2022 going into 2023.

25 days ago

Bike path needed desperately. Completely inadequate for 2022 going into 2023.

25 days ago

Bike path needed desperately. Completely inadequate for 2022 going into 2023.

25 days ago

Bike path needed desperately. Completely inadequate for 2022 going into 2023.

25 days ago

Please focus on sidewalks and crosswalks with lights and markings

25 days ago

Bike path needed desperately. Completely inadequate for 2022 going into 2023.

25 days ago

Sidewalks need to connect to access parks

25 days ago

Connect the sidewalks please down Dobys

25 days ago

Sidewalks on Dobys please

25 days ago

Crosswalk, and sidewalk needed

25 days ago

Sidewalks are needed the entire length of Whites Rd to provide access to schools. Would make going to school and events much easier.

25 days ago

Need sidewalks desperately!

25 days ago

No markings for crosswalk, very dangerous intersection for pedestrians.

25 days ago

<https://publicinput.com/Reporting/ReportPreview/17425>

38/100

12/2/22, 9:33 AM

Town of Fort Mill, SC - Report Creation

x

25 days ago

I-77 crossing

26 days ago

21 bypass needs sidewalks, at least from Fort Mill High School to the neighborhoods south. There is no safe way to walk to the school, to Harris St park, or downtown Fort Mill.

26 days ago

Sidewalks

26 days ago

To Access Doby's Bridge Elem

26 days ago

From Dominion Bridge to Catawba Ridge & Forest Creek

26 days ago

X

26 days ago

Need Path from Banks Rd and Fort Mill parkway to 160 and Fort mill park way and safe ways to cross fort mill parkway and paths into downtown fort mill and to Doby's Bridge baseball park

26 days ago

Sidewalks to connect schools and neighborhoods. Currently these all dead end. Extend up the parkway in areas with neighborhoods.

26 days ago

Sidewalks to access playgrounds and fields.

26 days ago

Please connect neighborhood sidewalks so that we have a single path along Doby's Bridge Rd. This would provide easy access to downtown for residents on east side of town.

26 days ago

Need sidewalks for school and recreation

26 days ago

Cross walk here but very intimidating to try and cross. There are no sidewalks or signage on the road to inform drivers of the crosswalk. Sidewalks here would allow those on Doby's and Whites Rd to access schools, shops, and school events without having to drive. Kids and teens would be able to cross for ice cream and 7-11 more safely. Congestion will increase when Harris Teeter is built, having an easy way to get to and from will be paramount.

26 days ago

We have 2 schools nearby but it is very difficult to walk or bike. Sidewalks would encourage healthy lifestyles by allowing kids to walk or ride bikes to school, would also be used for recreation with a large population up and down Whites Rd. This would also reduce congestion during morning and afternoons when school traffic is highest and make it easy to attend events at the High School.

26 days ago

There are 4 schools within a mile or less around this area and it is difficult to walk or bike safely to any of them. This increases congestion and requires cars for everything.

26 days ago

<https://publicinput.com/Reporting/ReportPreview/17425>

39/100

12/2/22, 9:33 AM

Town of Fort Mill, SC - Report Creation

Springfield Parkway: needs separated path on entire length for pedestrian/bicycle traffic

26 days ago

Fort Mill Parkway: needs separated path on entire length for pedestrian/bicycle traffic.

26 days ago

There needs to be connecting sidewalks to Dominion Bridegroom to all the schools surrounding it and to the new Harris Teeter that is coming. There should also be sidewalks along Doby's bridge connecting the neighborhoods

26 days ago

Bike path is too narrow on Springfield parkway. I see kids biking to Nation fort hs - it's not safe. And there are people who bike from regent parkway, Springfield & Carolina orchards to the Anne Springs close greenway. Bike path needs major improvement

26 days ago

Pleasant and 460, colthrap and pleasant, 460 and 21, colthrap and 21

26 days ago

X

27 days ago

A trail to make it safe to cross 77 on 160 would be tremendous. I currently see pedestrians and cyclists attempting to cross this area and it is always so unsafe.

27 days ago

Having a safe path all along 460/Fort Mill Parkway will open access to so many existing trails via bicycle or walking/running. People would be able to leave their homes without a car and recreate. Not to mention connecting neighborhoods and many Fort Mill Schools, allowing students/parents to commute via trail as well for pick up and drop off.

27 days ago

Adding a path along 160 would increase access to many downtown businesses via bike for both shopping and work.

27 days ago

Banks/Steele Street is a good connection between the Parkway and downtown Fort Mill and 460. It also connects the primarily African American neighborhood to the trail system.

27 days ago

Fort Mill Parkway/Bypass is dangerous currently but with a bike lane could open up tremendous access to the Founders Trail and downtown Fort Mill, as well as the schools and new developments being built along the Parkway.

27 days ago

Fort mill parkway needs sidewalks and bike lanes

27 days ago

Banks street could be a great connector from the parkway to town, or old brickyard road!

27 days ago

Regent Park area

27 days ago

Fort mill parkway needs bike lanes and side walks! This would connect folks to riverwalk from many neighborhoods.

27 days ago

<https://publicinput.com/Reporting/ReportPreview/17425>

40/100

12/2/22, 9:33 AM

Town of Fort Mill, SC - Report Creation

Need a trail to get over the bridge. Very dangerous intersection that no one would take a bike on, however, it the only access point to the bridge.

27 days ago

need trail access along n. doby's bridge

27 days ago

Need to be able to get up here on the parkway on a bike to access the greenway/YMCA complex.

27 days ago

Big construction project for this intersection, but need to make sure the new intersection includes trail system so bikes could go from parkway down or up 21.

27 days ago

Need trail to connect all these schools to residences.

27 days ago

There's no trail to take you into downtown on N. Doby's Bridge. This road is main corridor, but no contiguous trail.

27 days ago

Needs path to connect to school

27 days ago

needs path

27 days ago

Possible pedestrian bridge opportunity over the river to connect to the future Riverbend Park being built by York county on the other side of the river. Can leverage the town communities as access point to cross. All the communities on north side of river can't access that despite being on the other side of river.

27 days ago

path needed along river to connect communities.

27 days ago

Catawaba River path needed to connect all the communities on the North side of Catawba river to 21/bridge.

27 days ago

Need path on Doby's Bridge Rd.

27 days ago

Need path on doby's bridge rd

27 days ago

need path on Doby's bridge rd

27 days ago

X

27 days ago

X

27 days ago

No bikes on equestrian trails.

27 days ago

<https://publicinput.com/Reporting/ReportPreview/17425>

41/100

12/2/22, 9:33 AM

Town of Fort Mill, SC - Report Creation

Pleasant road needs bike lane or path

27 days ago

X

27 days ago

X

27 days ago

Not sure

27 days ago

Gold Hill Road

27 days ago

X

27 days ago

Wilshire @ Pleasant and Gold HILL are death traps to cross the street

27 days ago

Fort Mill bypass needs a bike path along it like many other communities around the area.

27 days ago

X

27 days ago

Old Nation sidewalks

27 days ago

It would be safer to have some kind of sidewalk or bike lane to connect 460 to 160 on Old Nation Rd

27 days ago

A sidewalk would make it safer to get to the Greenway from Carolina Orchards

27 days ago

None needed

27 days ago

x

27 days ago

X

27 days ago

Sam Smith Road needs a sidewalk

27 days ago

Lets be honest.... All of Fort Mill town AND unincorporated Fort Mill. There are no pins that can determine where it should be.

27 days ago

x

27 days ago

greenway to downtown to riverwalk

28 days ago

<https://publicinput.com/Reporting/ReportPreview/17425>

42/100

12/2/22, 9:33 AM

Town of Fort Mill, SC - Report Creation

Dobys Bridge Rd  
28 days ago

Banks Rd  
28 days ago

X  
28 days ago

Same as above  
28 days ago

Add bicycle lanes the entire length of 460 to 160 intersection, and on 160 from 460 to Baxter Village, also, on Hwy 21 from 460 to 160  
28 days ago

Won't let me pin  
29 days ago

A safe bike path from neighborhoods and schools.  
29 days ago

Pedestrian/bike path from waterside/pecan ridge to the high school (crhs)/ middle school(fcms)  
29 days ago

X  
29 days ago

sidewalks along Banks St  
one month ago

sidewalks along N. Doby's Bridge  
one month ago

Safe Bike/pedestrian crossing of I77 exit 85 from Kingsley area to baxter village which would ultimately connect a path from Baxter village all the way to downtown fort mill.  
one month ago

Pleasant Road into nc  
one month ago

All of them.  
one month ago

X  
one month ago

Fort mill bypass  
one month ago

X  
one month ago

Sidewalk or path from pecan ridge along whites road to new high school and middle school so students can walk or ride their bikes.  
one month ago

<https://publicinput.com/Reporting/ReportPreview/17425>

43/100

12/2/22, 9:33 AM

Town of Fort Mill, SC - Report Creation

Sidewalk/path from pecan ridge to forest creek middle and Catawba ridge high school to allow students to walk and ride bikes.  
one month ago

Connection of greenway to Walter Elisha Park and then Downtown area of Fort Mill  
one month ago

Crosswalk improvements  
one month ago

Main Street connection from nearby neighborhoods.  
one month ago

7. What factors discourage you from using trails or multi-use paths in Fort Mill? *Select all that apply.*

63%	Lack of safe biking and walking connections to and from existing trails and multi-use paths	680 ✓
46%	Lack of trails connecting points of interest to me	504 ✓
40%	Motor vehicle traffic	429 ✓
39%	Unsafe street crossings	427 ✓
26%	Lack of signage and wayfinding	282 ✓
21%	Personal safety concerns	232 ✓
20%	Lack of nearby destinations	212 ✓
18%	Lack of parking at trailheads	192 ✓
17%	Poor maintenance conditions of existing trails and multi-use paths	188 ✓
7%	Other (please specify)	74 ✓
2%	Lack of interest	27 ✓

1,086 Respondents

<https://publicinput.com/Reporting/ReportPreview/17425>

44/100

12/2/22, 9:33 AM

Town of Fort Mill, SC - Report Creation

8. Rank which infrastructure or program improvements would encourage you to use trails and multi-use paths more frequently in Fort Mill? Please rank your top 5 from 1 (most important) to 5 (least important).

92%	Constructing new trails and multi-use paths	Rank: 1.59	799 ✓
77%	Adding crosswalks and pedestrian signals	Rank: 2.85	665 ✓
73%	Repairing or maintaining existing trails and multi-use paths	Rank: 2.94	636 ✓
71%	Installing additional lighting along existing trails and multi-use paths	Rank: 3.46	614 ✓
66%	Installing shade trees and benches along existing trails and multi-use paths	Rank: 3.99	577 ✓
55%	Improving accessibility for disabled or impaired residents (curb ramps, audible pedestrian signals, etc.)	Rank: 4.83	482 ✓

869 Respondents

9. Which factors of Fort Mill's trail network are most important to you? Select all that apply.

88%	Health / Exercise / Recreation	938 ✓
78%	Access to Parks / Nature	830 ✓
61%	Connections to other multi-use paths and trails in York County	648 ✓
44%	Environmental Protection	473 ✓
21%	Transportation	220 ✓
19%	Economic Development / Tourism	198 ✓

1,067 Respondents

<https://publicinput.com/Reporting/ReportPreview/17425>

45/100

12/2/22, 9:33 AM

Town of Fort Mill, SC - Report Creation

10. What amenities are most important to you for trails and multi-use paths? Please rank your top 5 from 1 (most important) to 5 (least important).

69%	Wayfinding Signs	Rank: 2.77	601 ✓
70%	Restrooms	Rank: 3.41	613 ✓
60%	Accessible parking / trailheads	Rank: 3.49	523 ✓
59%	Maps / Kiosks	Rank: 3.53	513 ✓
61%	Lighting	Rank: 3.57	529 ✓
50%	Trash Cans	Rank: 4.79	439 ✓
41%	Benches	Rank: 5.00	361 ✓
38%	Drinking Fountains	Rank: 5.39	335 ✓
39%	911 Call Boxes	Rank: 5.44	342 ✓
42%	Pet Waste Stations	Rank: 5.55	369 ✓
34%	Bike Parking	Rank: 6.71	293 ✓
28%	Murals / Public Art	Rank: 7.80	247 ✓

874 Respondents

11. Multi-use paths and trails can be constructed using several different materials. Please select your preferred facilities below. Photos with brief descriptions are provided to the right. Select all that apply.

67%	Natural surface trail	692 ✓
64%	Paved multi-use path (concrete or asphalt)	654 ✓
50%	Unpaved multi-use path (compacted crushed stone)	518 ✓
35%	Shared-use boardwalks	359 ✓
8%	Equestrian trail	83 ✓

1,026 Respondents

<https://publicinput.com/Reporting/ReportPreview/17425>

46/100

12/2/22, 9:33 AM

Town of Fort Mill, SC - Report Creation

13. Please provide any additional comments/feedback on Fort Mill's trail network.

Would love to see more trails made available and/or maintained. Would like you to consider making a trail through the area of trees that were all destroyed to make way for the new sewage lines (along 21, through Walden Park, etc.). Would like to see the loss of all those trees turned into something useful. Would also like to restrict bike usage on narrow paths and/or widen paths to have a bike lane. Perhaps have some designated walker only paths? I regularly walk on the trail from Harris St. to Walter Elisha and have almost been run over by bikes several times because visibility around corners is low and the path is narrow. I like that it's narrow due to the shade the trees provide, but bikers fly down that path. It's hard to keep small children and pets out of the way of bikers and the riders typically act as if they have right of way and don't want to slow down.

20 hours ago

Connect to Waterside

yesterday

We need sidewalks (concrete) installed from Catawba Ridge High School to the main entrance of Waterside at Catawba. High school students run along this road, they need a sidewalk to exercise safely, away from traffic. We need the sidewalk to continue to the 711 gas station. As the area continues to grow, we need safe paths for people, mostly kids, to walk to these places.

yesterday

We need sidewalks from waterside at Catawba to surrounding schools and sidewalks to nearby 711/shopping center. Kids walk to the gas station, they need a safe place to do so. And Catawba Ridge high school students run on the road by the Pecan Ridge/Waterside, they need a sidewalk to exercise safely.

yesterday

Would like to see the trails in the Waterside community connected to other neighborhood trails such as the one at Mason's Bend.

yesterday

Keep nature as natural as possible. Pavement isn't a solution. Not having facilities for waste disposal (pet and trash) is harmful. We need to keep any trails as low impact as is possible, otherwise, what's the point?

2 days ago

-Access to trails from Waterside At The Catawba as well as clear access to Catawba River  
-Town Funded Kayak with public access on Fort Mill side  
-Sidewalks on Whites Rd from Waterside At The Catawba extending down to Fort Mill Parkway to each new school constructed in the area to allow for kids to potentially ride bikes to/from school if they choose

2 days ago

There aren't any trails or sharing the road from the new communities from waterside or etc I encourage the city to build that since a lot of people riding there bikes on the street

2 days ago

https://publicinput.com/Reporting/ReportPreview/17425

47/100

12/2/22, 9:33 AM

Town of Fort Mill, SC - Report Creation

Thank you for doing this survey! I could not figure out a way to see plans for the trail system - is this information available? I hope that the trail plan includes plans for the "Zack Spratt Trail," the trail required by the 2016 Kanawha Land Development Agreement on the land formerly owned by the Spratts/Kanawha Farms. The DA requires a trail to be built throughout the property, a minimum of ten feet wide, from the top of the property near the town and the Spratt cemetery all the way down to the Catawba River. The park donation deeds accepted by the town and the Kanawha Farms contract with the buyer also mandate this trail/require it be called the Zack Spratt Trail. Zack was a cousin born in the nineteenth century who was an avid hiker. When he sold his portion of the Fort Mill land to my father in the 1970s, Zack knew it was likely that my father would one day sell it for housing/commercial development. Zack expressed his wish that when my father sold the property, my father ensure a trail be constructed for the public to enjoy. Thus, my father insisted on this trail being in the 2016 DA. The trail is very important to my father as he agrees with Zack that a trail will be a nice benefit for the town and he wants to honor Zack's memory and wishes. I hope the town is following through with the town's and developer's legal obligations to construct and maintain the Zack Spratt trail. Thank you!

2 days ago

I think the trail network should connect all of Town, especially underserved communities, neighborhoods with elderly populations, and households without vehicles. All schools in Fort Mill should have connections to the surrounding neighborhoods with multi-use trails.

2 days ago

I think the trail network should connect all of the community, especially those in underserved areas, the elderly, and households without vehicles. All schools in Fort Mill should be connected to the surrounding neighborhoods with multi-use trails.

2 days ago

I think it is very important for the trail network to connect all communities, especially in underserved areas, households without vehicle access, and seniors. All schools should be connected to the surrounding neighborhoods with multi-use paths.

2 days ago

I think it is very important for the trail network to connect all communities, especially those in underserved areas and with households without vehicles.

2 days ago

There should be trails that connect neighborhoods to schools. Right now it's not realistic for kids who live near schools to walk to school.

There should be a bike and running lane as well.

2 days ago

Sidewalks on Whites Road and Dobys Bridge. Connections to Riverwalk from Waterside at the Catawba.

2 days ago

Connect waterside at Catawba trail to new Lennar neighborhood and to Catawba river in a safer more even way

2 days ago

Recycling stations

2 days ago

- Trail access to the Carolina thread trail in Elizabeth and Riverwalk from Waterside  
- sidewalks on Banks and Dobys Bridge rd to downtown

2 days ago

https://publicinput.com/Reporting/ReportPreview/17425

48/100

12/2/22, 9:33 AM

Town of Fort Mill, SC - Report Creation

I would like to see trails connecting the major neighborhoods in a given area. For example, a trail along the Catawba River should have connectors to each of the relevant neighborhoods (e.g. Mason's Bend, Elizabeth, Waterside, etc.). This would make a wonderful way of having extended recreation and exercise.

2 days ago

Please work to link the Waterside at the Catawba neighborhood to the larger area network(s) of trails.

2 days ago

It would be good to connect existing trails and add more river access. I would like to see sidewalks down whites road to add access to pedestrians and bikes. I would like to see pedestrian trails restricted from motor vehicle use and barricades yo prevent golf carts from traveling down narrow pathways.

2 days ago

Waterside at the Catawba has some nice rails, but connection to masons bend and the soon to come Elizabeth would be really great.

2 days ago

I love this initiative!!

2 days ago

Love this!

2 days ago

It would be wonderful if the trails connected to Charlotte's trail system. Also a trail that allows for a longer distance for exercising. Right now I go to Charlotte for most of my trails- especially Little Sugar Creek Greenway.

2 days ago

It would be nice to have more tails in Fort Mill. Charlotte has many "greenway trails". Having some here would be a good addition.

2 days ago

Connect the trails systems within fort mill, sidewalks down whites rd - crosswalks on fort mill parkway. Make fort mill more Accessible to walkers, bikers and hikers.

2 days ago

Connection of the trail systems in fort mill and sidewalks on whites road/ crosswalks T fort mill parkway. Make the town more accessible to hikers, bikers and walkers

2 days ago

Need trail access to Carolina thread trail in Elizabeth and Riverwalk from Waterside at the Catawba and sidewalks along Bsnks and Dobys Bridge Rd to downtown Fort Mill

2 days ago

Please consider the following:

- Trail access to the Carolina thread trail in Elizabeth and Riverwalk from Waterside at the Catawba
- sidewalks on Banks and Dobys Bridge rd to downtown

2 days ago

It would be helpful and safer to provide a sidewalk on Whites Road for students to walk/bike to school (FCMS & CRHS).

2 days ago

We need a connector from Waterside community to Carolina thread trail and sidewalks to Sony's bridge and downtown.

2 days ago

<https://publicinput.com/Reporting/ReportPreview/17425>

49/100

12/2/22, 9:33 AM

Town of Fort Mill, SC - Report Creation

Connect Elizabeth to waterside

2 days ago

I will be honest that I'm more concerned about the road conditions throughout fort Mill. They are horrible. Especially in unpaved new neighborhoods and down Banks Road.

2 days ago

Connect to Waterside at the Catawba and Catawba Ridge high school and new surrounding schools

2 days ago

We need more sidewalks on the parkway. We have 3 schools in a walkable distance from waterside but we are unable to walk any where bc the roads don't have shoulders or sidewalks.

2 days ago

A trail from Sparkling Brooke Pkwy on Banks Rd all the way to down town Fort Mill would get tons of use. Also a trail along Fort Mill Pkwy that connects Dobys Bridge Rd to 21, which would also create access to Riverwalk across the river.

2 days ago

- Trail access to the Carolina thread trail in Elizabeth and Riverwalk from Waterside
- sidewalks on Banks and Dobys Bridge rd to downtown

2 days ago

- Trail access to the Carolina thread trail in Elizabeth and Riverwalk from Waterside
- sidewalks on Banks and Dobys Bridge rd to downtown

2 days ago

Would love to have trails on the Doby's Bridge/Catawba Ridge side of town. Sidewalks would be a great addition to White's road, or a paved path even.

2 days ago

Trail access to the Carolina Thread Trail in Elizabeth and Riverwalk from Waterside.

Sidewalks on Banks Trail and White's Road.

2 days ago

Please connect trailhead to neighborhood Waterside at Catawba.

2 days ago

Would n Ed to know the amount on taxes.

2 days ago

Sidewalks down whites rd to schools

2 days ago

Would like to see mup /sidewalks from Waterside at Catawba neighborhood to downtown. Whitez road and connection to carolina tread trails

2 days ago

- Trail access to the Carolina thread trail in Elizabeth and Riverwalk from Waterside at the Catawba
- sidewalks on Banks and Dobys Bridge rd to downtown

2 days ago

<https://publicinput.com/Reporting/ReportPreview/17425>

50/100

12/2/22, 9:33 AM

Town of Fort Mill, SC - Report Creation

Need more trails on the south east side of town around waterside and CHRS  
2 days ago

The only trails I currently use are in the greenway. Unfortunately, the trail along springfield parkway doesn't reach my neighborhood. I would use connectivity within the town to bike or walk for local errands or travel.  
2 days ago

We need sidewalks installed to help people get to trails. Many areas are inaccessible for people due to lack of transportation combined with unsafe walking areas from lack of sidewalks. I say this whole picturing the parent I see pushing a stroller dangerously down Dobys Bridge Rd during rush hour.  
3 days ago

There are some obvious easy wins that could be undertaken regarding existing sidewalks which should be included in this trail study. It should be fairly easily to create safe links for pedestrians and cyclists of the current 'urban islands' of Tega Cay/Baxter/Kingsley & Fort Mill.

I-77 and the lack of any kind of safe crossing of it on 160 is something that immediately needs looking at and not just waiting for the intersection work to begin.

I unfortunately don't live within the Town Of Fort Mill (Sutton Place) and I hope this study includes the residents of surrounding areas. I would love to take part in any discussions and put forward ideas.  
3 days ago

More paved biking trails would be amazing  
3 days ago

The town really needs to invest in a third fire station located near the masons bend neighborhood.  
4 days ago

Can we please make a sidewalk system from baillwyck (neighborhood) to Baxter  
4 days ago

lake ridge needs trails to places such as baxter or tega cay walmart.  
4 days ago

Would love more connectivity with paved paths to access businesses, nature. Fort Mill has beautiful forests and trees. Would like planners to try to preserve trees and green spaces. Try to find green developers that preserve trees and green spaces in neighborhoods that would complement our wonderful greenway and not clear cut. There are cities doing more sustainable neighborhoods like Greenville and Asheville. Let's keep Fort Mill special and unique instead of selling out land to Lennar homes and Meritage homes with every forest destroyed and look like common suburbia. Neighborhoods close to greenway should be more well thought out, natural, unique, and perhaps sustainable architecture. And more trail systems would be fantastic.  
6 days ago

Make existing trail way locations more Publix I'm not even sure where they are or how to find them  
7 days ago

More trails less development of new homes safe our environment  
7 days ago

Glad to see this initiative and thanks for allowing input.  
8 days ago

Very excited to see developments on this. So happy the Town of Fort Mill is getting behind this concept.  
8 days ago

<https://publicinput.com/Reporting/ReportPreview/17425>

51/100

12/2/22, 9:33 AM

Town of Fort Mill, SC - Report Creation

Would love to see our beautiful town on a walk or bike  
9 days ago

I love the trail system expansion and management idea--but unless there were biker rules, it is unsafe for elderly person to compete with them and so probably wont use trails as much  
10 days ago

Please connect the sidewalks between Catawba Ridge High and Trail Ridge elementary.  
10 days ago

I support trail use and maintenance for access to nature and recreational exercise. I have huge concerns about interconnecting trails ending up being used by people for illegal activities like drug sales, hiding stolen property (which has happened on my neighborhood trails) or even just unintended purposes like access for homeless encampments or illegal / unsavory transaction meetups. I question the ability to properly supervise or oversee traffic and activity. Who would be responsible for regular oversight?  
10 days ago

I live on Dogwood lane and have been observing the new sewer line project progress. I sincerely hope that the 20 foot wide path cleared through the woods is used for a trail once the pipe project is completed. I'm happy to see this trail plan is in the works. Trails are a great way to walk and explore without being near traffic, as we would be on sidewalks.  
11 days ago

Trail safety is a major concern for women. I would not want to see any community trails connected to the private Anne springs greenway trails as membership to the greenway creates a community that increases safety  
11 days ago

Need to connect trails in a way that avoids the horrible fort mill traffic and dangerous drivers  
11 days ago

NA  
11 days ago

Desperately need bike lanes and/or dedicated paved bike/running paths.  
12 days ago

Trail network for both walking and or biking with connections into other communities  
12 days ago

I live off South Doby's Bridge Rd. It would be nice to have things like the greenway closer to me.  
13 days ago

Need sidewalks to walk safely to trails.  
13 days ago

Can we use hospitality tax as a source for funding? Rails to Trails Conservancy, Safe Streets and Roads for All (SS4A) Grant Program, and PeopleForBikes offer grants.  
13 days ago

More bike paths along streets, that are safe and off the street.  
13 days ago

<https://publicinput.com/Reporting/ReportPreview/17425>

52/100

12/2/22, 9:33 AM

Town of Fort Mill, SC - Report Creation

I think it would be great to connect all of the trails and make it clear when you are on a public trail vs. risk trespassing. I'm concerned about some of the gaps in trails that make it dangerous, especially near heavy traffic (e.g. crossing over the 77 from Baxter until a trail picks up near Kingsley Park.). I would love Fort Mill trails to also connect to the Tega Cay trails.

13 days ago

Connect the back entrance Masons Bend to Baxter Village by extending the sidewalk

14 days ago

Also encourage new neighborhoods and industrial parks to always include safe and accessible sidewalks in their construction plans that can be eventually linked to trails. Benchmark five rivers metro parks trail system in Dayton, Ohio. I used to be able to ride my bike 20 miles from my house in the burbs to Dayton city center safely.

14 days ago

I would love to see Fort Mill expand its trail network and connect to other networks. I've greatly enjoyed trails like the GAP, C&O, and New River Trail and believe these could greatly enhance living in, and visiting Fort Mill.

14 days ago

Would be great if there was a path to safely get to carowinds from downtown fort mill. Currently you must ride up pleasant rd which is very narrow and has lots of blind spots or straight up us21 which will get you killed trying to ride under i77

14 days ago

Getting over 77 safely on foot or bike is a huge safety problem

14 days ago

Please connect founders trail by river to Walter Elisha to and around bypass to Catawba neighborhood and more

14 days ago

I'm from Minneapolis. Minneapolis has a great trail system connecting all of the city and surrounding suburbs. Something to reference while you make your decisions.

15 days ago

We need to connect to the city of Charlotte's cross Charlotte trail, Anne Springs, and downtown Fort Mill to gain the economic development potential of other communities that have developed successful trails.

15 days ago

Sidewalks to school and safe street crossings near schools would be great

15 days ago

We live in such a beautiful area, if we don't intentionally add these trail networks, we will lose our primary means of connecting to nature. Thank you for your efforts!

15 days ago

The city govt has for years greatly reduced the funds that can be legally collected from new home sales and could be allocated for sidewalks. Eliminate the discount and collect 100% before pushing a bond.

15 days ago

This is important to thousands of people in Fort Mill. Please see this as an opportunity to help alleviate some of your traffic problems as well as make the town more pedestrian friendly.

15 days ago

<https://publicinput.com/Reporting/ReportPreview/17425>

53/100

12/2/22, 9:33 AM

Town of Fort Mill, SC - Report Creation

With the increase of E-Bike use in today's culture, please make trails E-bike friendly. I'm a senior and can only use an E-Bike for exercising. So please help the older generation still get out and enjoy our beautiful city and county.

16 days ago

This is a great survey and we really do need bike paths! This will alleviate traffic congestion. Getting from Baxter to Kingsley is unsafe at this time. Need dedicated bike lanes.

16 days ago

These trails are vital to our community. It gives a growing concrete jungle that is Fort Mill a stable foundation in nature that we need.

16 days ago

Need to increase trails in northern part of Fort Mill. Would be nice to have a path along the railroad from the north part of fort mill down into town.

16 days ago

There needs to be more bike lanes, sidewalk, and crossing signals everywhere

16 days ago

Connect to little sugar creek in Pineville

16 days ago

Fort Mill should use the 21 sewer connection to create a pathway after the construction. Also Sugar Creek greenway to Fort Mill so we can access Mecklenburg County trails

16 days ago

Sidewalks and bike paths could be used better to connect trails. I see very few of either in the Fort Mill area.

16 days ago

There are very few trails that I know of for public use in the area of fort mill that I live in - around nation ford HS

16 days ago

I use the trail system in Pineville frequently. It would be nice if it was joined in our area or we had similar trails.

16 days ago

Playground near trail network would be a huge win for families and use of trails! Voted for paved trails for children to ride bikes easily

16 days ago

Would love to see trails connect, additional side walks, and bike lanes throughout fort mill

16 days ago

Trails are needed but sidewalks in Whiteville Park and Fairway Drive should be the top priority!

16 days ago

Let's do this! If Charlotte can provide a network of trails, we can do it. :)

16 days ago

I help coach cross country at Fort Mill High School, and in that area, it's difficult for the 100+ runners on the team to enjoy a variety of routes, because there aren't a ton of sidewalks around. A path along hwy 21 in front of the school would be wonderful. Thank you!

16 days ago

<https://publicinput.com/Reporting/ReportPreview/17425>

54/100

12/2/22, 9:33 AM

Town of Fort Mill, SC - Report Creation

As you know, the demand for trails and natural areas is growing in our country. It is good for the local economy. It brings families, attracts visitor and builds the town as a great place to live and play.

16 days ago

Idk if this is part of the conversation, but a pedestrian-friendly way to get across the i-77 bridge on 160 is critical!! Parts of Fort Mill are great for walking and biking, but the town is just sliced in half by I-77 and crossing the intersections and bridge there feels really dangerous as a pedestrian or cyclist.

16 days ago

I love the current trail system and I fully support expanding and building upon it. This is a great idea.

17 days ago

We live on river wood drive / merryweather farms community - we have a trail that goes to Harris street park, but it would be wonderful if a trail could be added to connect to Riverview elementary. We and several other families have children that attend school there and it is currently unsafe to walk.

17 days ago

We need to improve our streets to reduce the traffic issues first.

17 days ago

Pedestrian safety is a major concern on the downtown side of Fort Mill. There are sidewalks from downtown on Academy and Confederate but they stop on Banks and do not extend down Fairway Drive. I see young children walking on a double yellow line road each morning and afternoon to get to the bus stop. Before adding additional recreational trails, the safety of our children and neighbors should be prioritized with the addition of safe walkways.

17 days ago

I selected asphalt, I really appreciate paved trails as it ensures I wont get bitten by fleas, etc on walk paths. Thank you for asking for public opinion.

17 days ago

Pennies and DOT do a terrible job with sidewalks, bike lanes and cross walks.

17 days ago

Bike safety is nonexistent on Sutton, dam road and 160. Also golf cart transit needs to be developed and I don't own a golf cart. They can coexist with bikes ext and are quiet and family oriented. Road crossings on Sutton and gray rock are dangerous. Speed breakers like in from of schools would help more traffic lights are needed. It would be wonderful to have a bike golf cart path from dam road to schools and Baxter. Also a pedestrian, bike, walking golf cart trail across i77 and across the Catawba river would help with a feeling of community and safety

17 days ago

Stop developing in Fort Mill. Would love for the trails to still be beautiful and functional but that won't be the case if there is traffic and subdivision after subdivision along the trails.

17 days ago

Fort Mill has a strong special needs community. Please consider inclusion in all aspects of this venture.

17 days ago

My kids and I mountain bike a lot. Getting from point to point with nice trails and road ways would be great. Want to be careful with bathrooms, because there's already a growing homeless problem that no one is doing anything about. Safety is number one concern.

17 days ago

Fort Mill needs more sidewalks on heavily used roads. Example, Doby's Bridge, Banks Street, finish sidewalks from new hospital in both directions (to Kingsley on 21 & 160) etc...

17 days ago

<https://publicinput.com/Reporting/ReportPreview/17425>

55/100

12/2/22, 9:33 AM

Town of Fort Mill, SC - Report Creation

Personal safety is very important

17 days ago

Would love for the Nation Ford Trailhead to be reopened and expanded.

17 days ago

I'm an avid mountain biker at Anne Spring Greenway. There is a tremendous opportunity to build A paved bike path along the bypass as there is no safe way to bike to the greenway. Connecting the various subdivisions along the Catawba river also parents a major opportunity to connect people with mixed biking and hiking paths.

Connecting neighborhoods and parks would elevate the experience of living here on fort mill. It's really the one area we come up short when comparing out infrastructure to surrounding towns like rock hill and ballyntyne

17 days ago

Connect the existing trails first

17 days ago

One of the proposed trails is right behind my property. I worry that it will no longer be safe for my kids to play in my backyard without constant supervision. How are you going to ensure that it's safe? How are you going to ensure that no one enters my property? How can you ensure that no crime will happen along the trail?

17 days ago

Not trail related but desperately need shoulders, bike lanes and sidewalks along many main roads in Fort Mill; not currently a bike friendly or walk friendly area.

17 days ago

Can we please get some consideration for a sidewalk on doby's bridge rode north. Being able to walk/bike/run into town and to restaurants and bars would be a great addition to the community.

17 days ago

Would love to see the trail connect into the trails in Pineville which would connect to the Cross Charlotte Trail. Would love to be able to ride safely from Fort Mill to downtown Charlotte

17 days ago

I wish that new neighborhoods were required to add sidewalks to join up to existing sidewalks to allow people to walk from one place to another

17 days ago

We need trails that lead to schools. It's better for kids' health and eases the burden on busses and his drivers.

17 days ago

There is a need for dog parks.

17 days ago

Please consider incorporating unused golf carts paths. Especially when they are established in new development projects (EX: the old Regent Park golf course)

17 days ago

No additional taxes!!!

17 days ago

I'd like access to major areas of fort mill, downtown, Kingsley, Baxter, etc to be able to connect without worry of traffic. Like what was done in Baxter trails to go UNDER Sutton Rd.

17 days ago

<https://publicinput.com/Reporting/ReportPreview/17425>

56/100

12/2/22, 9:33 AM

Town of Fort Mill, SC - Report Creation

Connect Walter Elisha Park to Rock Hill Riverwalk

17 days ago

Ensure lawful carry with a CWP, as preempted by state law, is clarified and allowed. Police cannot be on every trail and 911 boxes still take time you won't have.

17 days ago

Love having swings instead of only benches!

18 days ago

Being a nature lover, I would love to see more opportunities to connect to nature. What fun it would be to walk from greenway to town to River walk as well!

19 days ago

Would love to have more trails

19 days ago

I will walk the Baxter trails with My kids and they enjoy it. It is nice to have these options in our town

19 days ago

Having easily accessible, well-maintained trails would provide SO much to the Fort Mill community and its residents. These public trails connect Fort Mill residents of all ages, capabilities, and backgrounds-which is something beautiful. Expanding this (and maintaining what we currently have) is sure to make a positive impact in this town. We need to steer away from paved asphalt trails (adding more paved surfaces is not an environmentally friendly option) and boardwalk trails (need constant repairs, uneven boards are a risk to older walkers)- natural trails are the way to go. Accompanied by unpaved, open lots- this is an environmentally friendly choice that accommodates this growing community's needs :)

19 days ago

I personally do not use trails and do not see using them in the future

19 days ago

It would be great to connect the Baxter trails to the dam, down to mason's bend, Catawba ridge and up to Anne springs

20 days ago

Improving Fort Mill's Trail network will inevitably improve the health of the environment and our society by improving on and working together towards a movement that builds a sense of community, sharing, health, and overall wellbeing..

20 days ago

Would like to see trails connect from the Riverwalk to Elisha Park to ASC Greenway

20 days ago

There are many young families here so trails that support strollers would be nice.

Safety is also key for a lit of women. Making sure there's a way to get help is ideal

20 days ago

Sidewalks should be available on all major streets and thoroughfares. It is a shame that many people can't walk from their neighborhood to downtown fort mill. (Doby Bridge for example)

20 days ago

Please add a small dog park to Walter Elisha

20 days ago

<https://publicinput.com/Reporting/ReportPreview/17425>

57/100

12/2/22, 9:33 AM

Town of Fort Mill, SC - Report Creation

Anything we can do to make Fort Mill more walkable and accessible is a great idea. So glad to live in a town that is asking the public their opinions via surveys!

20 days ago

I wish there were more sidewalks to connect students to schools.

21 days ago

We have a good start of a network. I don't mind paying a small fee to be able to access private trails, so I am able to commute from my house near downtown to Baxter, out to Tega Cay, and using Anne Springs, getting to Pineville is now mostly safe. I would love to see a connector trail from the XCLT greenway to Anne Springs, as well as something from down town to the bridge to get to riverwalk.

I am always available to help ad would be interested in being on the board or whatever if that's an option.

21 days ago

Would love to see an official trail connection between Baxter and Masons Bend trail systems. Also, a pedestrian/cycling bridge connecting Masons Bend to Riverwalk, avoiding current crossing on Highway 21 which has considerable vehicular traffic.

21 days ago

Would love for masons bend trails To connect to Baxter

21 days ago

I do not want restrooms on the trails because all sorts of gross bad things happen in restrooms in the woods.

21 days ago

All for the initiative- need more and better trails

21 days ago

I use trails and parks in Fort Mill multiple times a week and would love for expansion and new connections. This will promote a healthier community and be much more appealing to young families.

21 days ago

I think there is more there than what people know

21 days ago

The network is pretty great it could just use some connections between masons bend and baxter village headed towards anne springs and maybe a safer extended crossing across the catawba river towards riverwalk and extending into rock hill

21 days ago

I'm greatly looking forward to being able to bike from the new Tega Cay recreational fields, along the river, through down town and into the ASCG without riding on roads or sidewalks!!

21 days ago

Would love to see Masons Bend trails connect to Baxter and also to downtown fort mill!

21 days ago

Connect Masons Bend to Baxter Bike trails

21 days ago

Trails that lead to restaurants or bars

21 days ago

<https://publicinput.com/Reporting/ReportPreview/17425>

58/100

12/2/22, 9:33 AM

Town of Fort Mill, SC - Report Creation

Connect Masons Bend trail with Baxter's and perhaps a pedestrian lane along the route 21 bridge to connect to Riverwalk more safely.

21 days ago

Great opportunity to be a leader in designing a trail system to connect the greenway and the city

21 days ago

We absolutely need more trails connecting around Fort Mill , Road cycling has become extremely dangerous with the amount of Traffic this area has grown

21 days ago

The trail I most use, to Harris St Park from Creekside Crossing, always has SO much poison ivy on each side of it. Need to spray.

21 days ago

Please add sidewalks on Doby's Bridge Rd. It's very dangerous to walk, particularly with small children.

21 days ago

Would love to see the Founders Trail in Masons Bend connect to the trails in Baxter as well as the other direction towards Waterside at Catawba

21 days ago

Fort mill has a very large mountain bike community. To be able to safely pedal from river walk to Ann springs to Baxter to access the trail systems at each area just simply doesn't exist with the volume of traffic that the area has.

21 days ago

n/a

21 days ago

There is so much opportunity to connect trails to schools. The kids in our neighborhood should be able to walk to schools, but there are no connecting paths or safe crosswalks

21 days ago

The only trail network I am aware of is the Greenway trails. You could consider some of the sidewalks in the downtown area a trail.

21 days ago

The single most important factor for Fort Mill is to connect to the Little Sugar Creek Greenway at the SC line so that residents can use the entire CLT/Mek county system. It would be quite easy to route through Regent Park using existing but abandoned golf cart paths, connect to the Carolina Orchards trail and then connect to downtown Fort Mill via Old Nation Hwy using existing sidewalks and adding sidewalks from the current end point to connect to Carolina Orchards trail. The trail could then reach from CLT to downtown fort mill. To continue south we could use existing sidewalks out of fort mill and add sidewalks/trail extending to US 21 and to the existing bike lane across the catawba to connect to the Riverwalk trail. We need a trailhead on old nation at the dairy barn entrance to Anne springs. We also need a trailhead near the railroad crossing in regent park where the York County shed is located. A trailhead south of fort mill before US21 would also be useful.

22 days ago

Interconnecting trailways are a fantastic way for people to explore/experience their immediate and surrounding community. I live in Raleigh currently but am moving to Fort Mill at the end of the year and the biggest thing I love about Raleigh is that their trail system connects to so many places throughout the capital. I can ride my bike to the art museum, then to Umstead State Park, and finally into the heart of downtown all without having to bike on main roads. The natural aspect is also pleasing. A well paved trail that's quiet and goes through the woods or along the river gives one a sense of calm and enjoyment to have such a luxury. A trail system absolutely needs to be expanded to connect all parts of york county together!!

22 days ago

<https://publicinput.com/Reporting/ReportPreview/17425>

59/100

12/2/22, 9:33 AM

Town of Fort Mill, SC - Report Creation

Something needs to be done to make it safer to get to the trails. Although there are bike route signs, Fort Mill is NOT bike friendly.

22 days ago

I would love to be able to walk/bike with my kids to school instead of relying on bus/car transportation.

22 days ago

Golf carts have become much more common in downtown fort mill. It would be wonderful to have golf cart parking available as well at trailheads.

22 days ago

We need some kind of safe way to walk/ride bike along Sutton rd to reach Baxter Village from Masons Bend

22 days ago

Our children need to be able to safely travel to and from school if they choose to walk or bicycle to school.

22 days ago

It would be great if there was a sidewalk along the fort mill bypass

22 days ago

Safer crossings are critical. I enjoy walking from the Baxter side of town, to main street, but crossing the 77 bridge is very dodgy. Same for the intersection of Munn Road and 160, I work on Munn road and like to walk to kingsley, but its like playing frogger. Key areas of down, Baxter, main street, kingsley, and nearby neighborhoods should all be easily walkable.

22 days ago

Would love to see bike lanes designated on the roads for safety.

22 days ago

I also am very eager to see some walkable green space that is truly public and not reliant on a private entity. The possible connectivity with the Carolina Thread Trail and Riverwalk is outstanding and I would love to see the small walking trail between Walter Elisha and Harris Street Park continued and expanded.

Fort Mill has historically not capitalized on the access to the river and as a Fort Mill native, I think this is incredibly short-sighted and we are rapidly losing any chance at preserving land publicly near the Catawba. Working with developers to incorporate PUBLIC hiking trails (nothing like taking a hike along part of the Carolina Thread Trail in Masons Bend and finding the other trail access is "private" and only for Masons Bend residents, not to mention the Greenway now charging \$12 per person for entry) along the river and in greenspace would help, but the Town needs to work to preserve land itself.

22 days ago

I would like to see pervious surface vs traditional concrete or asphalt used for paved trails. I also am a big believer in "complete streets" and would love to see Fort Mill become more walkable with sidewalks that don't disappear half-way down a road. Dedicated bike lanes and sidewalks are a must to make our community safer and make it so folks can actually walk from point A to point B vs having to drive less than 1/2 a mile to the next destination for lack of safe walkable access. Springfield Parkway is a prime example of a road that not only is already in need of widening (lord help us) but also needs to have pedestrian and cyclist infrastructure in place--there are hundreds of kids & school staff who could be walking to school or biking if there was a safe way for them to do so. My commute is less than 3 miles (15 minutes by bike) but I would be terrified to even try that since Springfield Parkway seems like a death trap.

22 days ago

<https://publicinput.com/Reporting/ReportPreview/17425>

60/100

12/2/22, 9:33 AM

Town of Fort Mill, SC - Report Creation

I live in Creekside (Merryweather Farms) off of 21 and our neighborhood uses the trail/path that connects our neighbor all the way to Walter Elisha Park. The trail is made up of walking path, gravel access road, parking lot through Harris street park, street crossing and finally connects to a walking path to Walter Elisha park. So many people use this pathway that it would be awesome to see it become a real trail. Lots of children use it as well.

22 days ago

I'd love a trail that connects creekside crossing to Riverview elementary

22 days ago

trail yield signs. More signage is needed.

22 days ago

Would love to see more connections for neighborhood to have safe passage to schools and area events.

22 days ago

I find it embarrassing that we spend time and money increasing the "car rider/pickup" lanes at ALL of our schools rather than fund paths and bike racks for our children to walk and ride bikes to school. The benefits of paths in Fort Mill are astonishing, from health to environmental, social to general well-being. If those reasons aren't for you, the return on investment by increasing property values and reduction of traffic in town should be enough to convince everyone.

23 days ago

I was not aware or sure of what trails are available other than within the Anne Close Springs greenway.

23 days ago

I'm very excited by the proposition of adding multi-use trails. I bike a lot and the roads are generally unsafe for bikes around here, given how busy they are and the narrow shoulders.

23 days ago

Would like to see a walkable nature trail network connecting town.

23 days ago

It would be great to get additional

Safe trails in the area. Kids could be riding their bikes to middle school if there were paths. Plus kids could be riding around instead of being on their phones etc. Being outside in fresh air is so important to their well being

23 days ago

I would love to be able to go from Fort Mill to Tega Cay or Fort Mill to Rock Hill without

23 days ago

Desperately need to invest in trail system to improve safety, and contribute to healthier lifestyles. I love to bike but it us hazardous to do so here. Very disappointing when living in an area with the perfect climate for biking and hiking.!

23 days ago

Connect trails i.e. Elisha Park to Riverwalk

23 days ago

Could install sidewalks or breakdown lanes

23 days ago

Paths, crosswalks and sidewalks so our children could ride their bikes or walk to schools along with lighting at all cross walks to protect the students and crossing guards especially on dark mornings would be GREAT!!!

23 days ago

<https://publicinput.com/Reporting/ReportPreview/17425>

61/100

12/2/22, 9:33 AM

Town of Fort Mill, SC - Report Creation

N/A

23 days ago

More bike and walk paths, please. Focus on traffic enforcement in and around the downtown neighborhoods. Make the east and west sides of I77 connected through safe walk / bike paths.

23 days ago

Currently the town of fort mill has Baxter trails, mason's bend/founders trail, river walk and anne springs as its core large trails. For someone to connect these it is difficult, for instance if you are at baxter and want to get to mason's bend, the straight shot is to go across the powerlines to get there without having to get on the road but it is labeled 'private property' so as a biker you need to go back out of the woods and bike down new greystone and Sutton road (both of which are generally busy) to connect. In addition, when going from riverwalk to fort mill there is a lot of opportunity to increase riverwalk greenway to fort mill so people would get on at fort mill and connect without having to get on business 21 and fight traffic the whole way there.

This leads me to the final point and that is fort mill and anne springs are not far away from each other, having a designated path to/from anne springs and maybe a program with anne springs to purchase a day pass from a local establishment could potentially be something useful.

23 days ago

Need connectivity!!!

23 days ago

Fort Mill is in a prime position to offer residents the opportunity to volunteer to help shape an amazing trail network. A trail that would connect Founder's trail down the river towards Massey, and then up to Anne Springs, would yield an amazing loop option. It would also create safe access to trails for many residents. There are already trails behind Waterside. Additionally, if Anne Spring's opened or built a greenway connecting adventure road to the Cantina, the east side of Fort Mill (think Springfield) would have walking/biking access to the Cantina. Meaning less car traffic. From there, open a trail to connect the cantina to the Peach Stand. It's then easy to connect over to Baxter - if a sidewalk is added to the 160 bridge over I-77.

24 days ago

Na

24 days ago

I would love to have an accessible trail or multi-use path from Masons Bend to Baxter Village, either a sidewalk along Sutton or a nature trail that cuts through the woods safely. Currently there is no safe way to walk to bike there.

24 days ago

Would love to have the founders trail/Carolina thread trail connect to Baxter village.

Also, would like to have the homeless encampment that has popped up under the highway bridge addressed.

24 days ago

A connecting pathway from white grove neighborhood to 160 would provide many houses and people access to downtown and other amenities

24 days ago

24 days ago

Connection of the trails into one large system that is safe to navigate is most important.

24 days ago

Fort Mill needs designated bike lanes on roads! Other cities have these and they add to the quality of life and overall safety!!!

24 days ago

<https://publicinput.com/Reporting/ReportPreview/17425>

62/100

12/2/22, 9:33 AM

Town of Fort Mill, SC - Report Creation

I would like to see the trails connect to places outside of Fort Mill (Kingsley, Baxter Village, etc.)

24 days ago

-

24 days ago

Property taxes are too high.

24 days ago

I would prefer to Town of Fort Mill apply for grants for funding or use hospitality dollars. Both would be better than issuing a bond.

24 days ago

MOAR TRAILZ.

REALLY need a safe way to cross 77 at or near 160. Traffic here is INSANE and there are too many traffic control systems to cross here even if there were better lanes on the bridge.

24 days ago

There isn't a lot of promotion of the trails that are available, where people can park to access them, etc. A lot of people probably aren't aware of what we do have.

24 days ago

ty so much love it!!

24 days ago

Thank you for doing this! I am outdoors in the Fort Mill area a lot and I am looking forward to what you all have planned.

24 days ago

Fort Mill has developed several walkable communities/areas such as Baxter, Kingsley, Orchards, Ann Springs but have minimal pedestrian and bike accessibility between these areas (along with the downtown area). Transportation corridors are heavily focused on vehicles with minimal thought on pedestrian and bike transportation. This needs to be part of the plan as well.

24 days ago

Trails and/or sidewalks to connect neighborhoods to school and to other neighborhoods is absolutely needed to help relieve traffic issues. There is no reason for us to drive a 1/4 or 1/2 mile to school if we had a way to send our kids other than driving. I would love to be able to mtn bike all the way down the Catawba river in FM. Or connect downtown to other points in FM. Most trails or sidewalks just stopped when it gets complicated, like over an interstate, etc.

24 days ago

Connecting trails with schools would be amazing! The high school cross country teams only have the Greenway to use and that's become very limited lately. If they run around their campuses it's extremely dangerous because they are all near very busy roads. It seems all of the schools could benefit from being connected to paths/trails.

24 days ago

I support adding as much single-track trail as possible that is accessible to mountain biking, hiking, and trail running. Connecting these trails together to facilitate travel around the region would be priceless.

24 days ago

More than anything there needs to be a safer way to get to these trails via non-vehicle transportation. I would never walk out of my neighborhood to anywhere in Fort Mill, I live in Waterside at the Catawba and there is no sidewalk on either White Rd. or Band Road to get to the trails or even downtown Fort Mill.

24 days ago

<https://publicinput.com/Reporting/ReportPreview/17425>

63/100

12/2/22, 9:33 AM

Town of Fort Mill, SC - Report Creation

Carolina thread trail has attempted to connect the Baxter and Masons Bend trail system and has gotten pushback from land owners along Sutton road. An effort should be made to use the existing Duke Energy distribution line right of way to connect these two trails. This would allow trail access connecting Tega Cay/Lake Ridge/Baxter/Masons Bend/Riverwalk. In addition to this, the group should leverage the work done by Winthrop in 2021 to connect Rock Hill and Fort Mill with trails via ASCG. This would allow almost all of fort mill west of 177 and Tega Cay to be connected to downtown Fort Mill and Rock Hill via trail systems with these two enhancements.

24 days ago

Keep fighting the good fight to keep our area beautiful with as much green space as possible!!!

24 days ago

While improving trails for bikes, some consideration should be provided to those who ride on the road around Fort Mill. Many of the roads do not have a shoulder which forces one to be in the lane of traffic, which allowed by law, but if there were a bike lane, it would be so much better. Especially on certain areas of the Fort Mill Parkway or Business Route 21 and sections of 460, as well as 160. There are many groups that ride these routes. There is data by the state and local that indicates by accidents that have occurred, that a bike lane would help.

24 days ago

Need side walks on Sutton btwn Loves and Baxter. No safe way to walk on this road.

24 days ago

Destination / Overlook Areas

24 days ago

There is a new construction project being conducted along business 21. That includes a pipe of some sort being layer. From what I can tell. That pipe being installed runs from Anne Springs to the new road section be built by riverview elementary school. It would be amazing if the area where the pipe is being laid turns into a trail. As it would almost connect with the Founders trail system that ends near the corner of Sutton road and 21. Right near the fort mill waste treatment plant. That founders trail connects the masons bend neighborhood to river walk.

24 days ago

There are so many separated neighborhoods in Fort Mill. I would love to see them connected by trails so kids don't have to bike/walk along busy roads to see their friends.

25 days ago

It all comes down to traffic. I feel like my safety us at risk anytime I ride in Fort Mill.

25 days ago

Sutton Road is a nightmare but people are often biking on it. There needs to be a safer lane for bicyclists and walkers, set off from the driving lanes separated from them by grass and/or hedging to make the lanes' purposes clear; well-lit; with 911 call boxes available. The intersection of Sutton and New Gray Rock needs a traffic light. Sutton Road needs to be widened to accommodate the increased traffic due to developments and population growth south of Baxter. Especially once Elizabeth is completed, because that traffic through to Baxter for dining, etc is going to be a big problem. The Bojangles going in at the roundabout near Loves will also increase traffic and necessitate widening of Sutton Rd.

25 days ago

N/A

25 days ago

Regularly use the networks in York and Lancaster. There are little "nodes" of trails. The Greenway, Riverwalk, Winthrop Lake, Sun City in Lancaster with cart/bike paths. The more we can connect these and double up on access points with existing parking i.e. Parking at Kingsley to hop on the sidewalk to run/walk/bike into fort mill or onto the Greenway or at the FMAC or a grocery store, etc. Provide the network with proximity to existing areas of public accessibility.

25 days ago

<https://publicinput.com/Reporting/ReportPreview/17425>

64/100

12/2/22, 9:33 AM

Town of Fort Mill, SC - Report Creation

Access needed for student safety walking from Dominion Bridge & surrounding neighborhoods to Catawba Ridge, Forest Creek & Doby's Bridge Elementary

25 days ago

I love the idea of connecting Anne Springs to the river. Also recommend connecting Baxter trails to Masons Bend. One last idea: putting in trails that border existing neighborhood boundaries, usually where creeks / sewer paths are located. We really need more connectivity to the town and sidewalks can be expensive. Walking paths between neighborhoods is two birds with one stone.

25 days ago

Sugar creek trail is a great example. Paved, good signage, wide open and safe for women to run or bike. Trail system should be wide enough for bikers, walkers and walkers with dogs to pass safely

25 days ago

Also would LOVE sidewalks from masons bend to Baxter so that we can walk/ride to Baxter safely from MB. Thank you!

25 days ago

Connecting with current trails (asg, Ctt) to key town land marks (old town, Kingsley) would be a game changer

25 days ago

The largest challenge is our neighborhood is cut off from existing pedestrian walkways through Baxter village. It's unsafe to walk or bike along Sutton road.

25 days ago

I'm very grateful for the generosity of the Close family and their legacy gift to our community.

25 days ago

Would love to see Founders Train in Mason's Bend connect to the damn. Would also love walking path from Mason's Bend to Baxter.

25 days ago

I am a trail builder who has developed trails like Ballantyne District Park and Purser Hulse Park. I live in a Fort Mill and would be very happy to help develop and maintain local trails.

25 days ago

Fort Mill has Ann Springs. If residents want to use it, they pay for it. If not, they don't. Don't spend tax payer money on this.

25 days ago

We need a way to safely connect Masons Bend to Baxter Village. There are no sidewalks and Sutton is a dangerous road. Allowing those 2 neighborhoods to be connected in a safe way would make a huge difference in the quality of life, not to mention make it easier for the Masons Bend community to frequent Baxter businesses.

25 days ago

It is a great start, excited to see how it turns out.

25 days ago

As a physician that lives and works in this community, I would love to bike to work! Additional trail network/bike path along 160 and/or 21 connecting existing trails would be incredible for those that want to commute safely on two wheels. Thank you for considering this healthy option!!

25 days ago

I personally will go out for a nature walk/bike ride more often if there is an interconnected trails system.

25 days ago

<https://publicinput.com/Reporting/ReportPreview/17425>

65/100

12/2/22, 9:33 AM

Town of Fort Mill, SC - Report Creation

Need more/ safer access to founders trail/ Catawba river. Hopefully find a way to bike across the river with out it being so intimidating for kids! Thanks

25 days ago

Need pedestrian/bike safe connection across I-77 on 160. Also would like to connect Catawba river trail to Baxter village trails.

25 days ago

This entire town, incorporated and unincorporated, lacks sidewalks, thus discouraging walking.

25 days ago

Would love trails to connect to mixed used zoning areas. Retail, homes and restaurants/bars. Similar to the rail trail, belt line, high line. Get people outdoors and allow for new business opportunities to drive recreation and pedestrian traffic.

25 days ago

Would like to see more UNPAVED MULTI-USE PATH at Anne Spring Close.

25 days ago

Mountain biking infrastructure and purpose built trails has been proven to bring a lot of revenue to towns. There is a lot of public land that could be used for this (and potentially tied into the trail systems at Anne springs).

25 days ago

Please consider the benefit of connecting Baxter Village trails to Carolina Thread Trail.

25 days ago

Would be amazing to see Fort Mill connect major trail systems like the Greenway, Baxter, and Riverwalk. Loudon County Virginia has a great setup with their WO&D Trail that could provide inspiration for how we connect residential and commercial development in Fort Mill.

25 days ago

We love them for walking, biking and enjoying nature!

25 days ago

It would be really amazing to have an official connection between the Mason's Bend and Baxter Trails. The unofficial link goes across private property.

25 days ago

As a runner and biker, I would love a safe routes from where I live in Masons Bend off exit 83 to Downtown Fort Mill, Baxter Village, and to the Riverwalk. Being able to bike to things like Saturday morning coffee would be a game changer!

25 days ago

When the new I-77 massive signs went up a year or so ago on 160 I genuinely thought we were getting side walk crossing signs and was so disappointed when they were just poles. Whatever we do with 160&i-77 needs to be 1000 times more pedestrian friendly than what was done at the gold hill intersection.

25 days ago

<https://publicinput.com/Reporting/ReportPreview/17425>

66/100

12/2/22, 9:33 AM

Town of Fort Mill, SC - Report Creation

Harris St Park needs lights and major signals for crossing over Harris Street, 21 from Peach Stand to Riverwalk needs a safe bike lane to connect Anne Springs to Founders Trail. Old 21 needs repaved for cyclists connecting to Pineville and eventually Nation Ford Rd/Old Pineville, Crosswalk Sign at North/South White St from Downtown doesn't change to Pedestrian Crossing, Clebourne St train bridge needs sidewalk leveling-smaller kids can't go over large bump connecting bridge...major safety concern for them falling into road, make the back of Whiteville Park to Anne Springs a commuter connection already, buy back the land Sugar Loop was on behind Fort Mill Middle (so much volunteer work on those trails shameful or closed), buy back land North Tunnel trail was on in front of FMMS. I've emailed the town for water fountains being turned on and it expanded in the past and the parks manager told me to bring a water bottle on my runs...embarrassing add more. Fountain in front of Horse Stable entrance hasn't worked in years. rock hill is the capital of disc golf and fort mill has 1/2 a course at Springfield middle....cmon y'all seriously? Adventure Road Greenway disc golf don't allow alcohol. Call me with questions. 803-517-6466

25 days ago

Need a better option to connect Riverwalk, Baxter Trails, and Founders trail (Masons Bend) than the roads.

25 days ago

Would love to see a way for residents on west side of 77 to get to east side of 77 safely. For example Baxter/Braydon residents being able to walk to Kingsley or downtown SAFELY

25 days ago

River floaters are extremely dirty and throw trash everywhere. It needs to stop.

25 days ago

Would love to connect to Tega Cay, especially Catawba Park and Dam Road.

25 days ago

N/A

25 days ago

Biking is impossible on 160. It is really pathetic in 2022 going to 2023 that there are no bike paths on 160. The stretch from the Food Lion and all the way to Baxter or even Harris Teeter should be a top priority to make bicycle safe. Instead, Fort Mill uses funds on silly festivals that minimum residents attend, closing off main street, and police sitting in hiding to ticket people trying to turn right onto 160 at main street. Defund the police and improve bicycle paths is simple, easy, and benefits more people than holding festivals and having those donut eating empty brains getting paid for sitting.

25 days ago

This is a great idea. Something that will help differentiate us from every other suburb in the region if done correctly. This is an investment in our future.

25 days ago

Please focus on sidewalks near schools. Our children currently have a difficult time walking on shoulder of the road.

25 days ago

First Thank you for your focus on this topic. I encourage Fort Mill to set a path that other communities can model such as Indian Land. I live in between the 2 cities and appreciate the preserved trees, paths of many types. Thank you

25 days ago

Events and food resources nearby will keep me out all day. No pavement. I walk there to get away from the pavement. Dog friendly.

25 days ago

https://publicinput.com/Reporting/ReportPreview/17425

67/100

12/2/22, 9:33 AM

Town of Fort Mill, SC - Report Creation

I would like to see better interconnectivity between Fort Mill / Baxter and Riverwalk, a safe crossing of I-77, safe connectivity between Baxter and downtown Fort Mill, and beyond to Indian Land.

26 days ago

Fort Mill already has an amazing network of trails. As an equestrian, cyclist, and hiker, I am excited to see the possibility of the trails continuing to be maintained and expanded. Please do not forget the equestrian community. Equestrians spend more money in the community than hikers and cyclists. There is already a large contingent and Fort Mill is unique to have the horse trails it already does.

26 days ago

Multi-use trails for pedestrians and their pets and children and also equestrians.

26 days ago

Keep up the good work of the Close family! This is what makes fort Mill such a popular place to live work and enjoy recreation!

26 days ago

I did not know there were trails. But I am big fan of them and use one near my house frequently. Trails are a nice way to get a break from work during the day.

26 days ago

I love Fort Mill's existing trail network. It is very good, but maintaining and upgrading is always welcome.

26 days ago

Have it start/end somewhere with healthy attractions

26 days ago

More biking trails. Look at what Bentonville Arkansas has done for the city. Amazing

26 days ago

Sidewalks and crosswalks are urgently needed for walking and biking pedestrians and students along Whites Road, connecting Dominion Bridge and surrounding communities to neighboring schools, Forest Creek, Catawba Ridge and Doby's Bridge Elementary. Safety should be of the utmost concern and giving proximity of all schools, increased commuter traffic, and lack of bus drivers, there should be a safe way for children to walk or bike to school.

26 days ago

We need continuous safe sidewalks along all roads including over interstates. There is currently no safe way to walk along 160 between Baxter and Fort Mill. We need more crosswalks on Pleasant/Sutton

26 days ago

I would also like to see programs in place to help keep these trails clean and green. Littler clean ups on a regular basis

26 days ago

Please first focus on improvements to the sidewalk and crosswalk infrastructure around schools and public areas.

26 days ago

Please focus first on connecting neighborhood sidewalks around schools and public sites. Crosswalks at intersections with painted lines on ground.

26 days ago

N/a

26 days ago

Would love sidewalks along Fort Mill bypass to walk to stores.

26 days ago

https://publicinput.com/Reporting/ReportPreview/17425

68/100

12/2/22, 9:33 AM

Town of Fort Mill, SC - Report Creation

Would like more bike lanes, repaired sidewalks and connecting trails.

26 days ago

Though it may seem extensive, a separated, paved path (such as Tega Cay Drive) along Springfield and Fort Mill Parkways (with pedestrian crossing lights where needed) would be an immense benefit to the community. Given the number of schools and parks along this route, it would also be a wonderful way to increase exercise opportunities for Fort Mill's younger citizens.

26 days ago

There are no sidewalk connections. The sidewalks around all the schools on fort mill parkway do not connect and they all go nowhere. They need to connect to each other and to surrounding neighborhoods so kids can walk and ride to school.

26 days ago

This is a much needed asset to fort mill. Particularly if you find ways to connect the Anne Springs Greenway to downtown fort mill, Kingsley, Baxter, etc.

26 days ago

Trails/greenways need to be more accessible for neighborhoods

26 days ago

I support all efforts to improve Fort Mill's trail network!

26 days ago

There are plenty of taxes we pay that could be reallocated. Rather than tax us more for such a lifestyle benefit, let's reallocate some of the school taxes. It would be great if the trails came along the river and there was a footbridge across to Riverwalk or near the railroad bridge. That would be something.

27 days ago

It would be really cool to have some sort of scavenger hunt for kids to look for on these trails... During these trails there could be things put along the way for kids to spot, when you were mentioning artwork maybe there could be a snake hidden somewhere on these trails and a raccoon, or a possum made out of artwork and kids would have to try to spot these things along the way.

27 days ago

There are some great trail systems already present in the area, but what is lacking are safe routes to access them. Ideally someone would be able to hop on their bike or walk from their home with a safe route (bike/walk lane or trail) to the Greenway, downtown Fort Mill, Founders Trail and Riverwalk. Those safe routes don't yet exist. By focusing on creating those, it would create SO much more access. Having to drive to a trailhead is a barrier (an extra step), but if people could hop on their bike or walk safely directly from their home, I believe usage of existing trails and alternative transport (bikes) to reach primary destinations (downtown, greenway, crossing 77 to Baxter etc.) would rise tremendously.

27 days ago

Excited that the town is finally working towards a trail master plan.

27 days ago

Would love more bike paths to make cycling on the road safer.

27 days ago

Trails for transportation have to be long, wide and straight with great sight lines and usage signs. Ok to be on roads but a part of the road infrastructure (bike lanes, not merely a trash-filled shoulder).

Pedestrians from all over Fort Mill (town and outlying areas) should be able to get to places of interest without use of a car. Good pedestrian walk/bike access is a revenue generator for communities and help connect communities across all income levels. Lastly, the plan must tap into greenway/trails in Charlotte, Rock Hill, Indian Land, etc.

27 days ago

<https://publicinput.com/Reporting/ReportPreview/17425>

69/100

12/2/22, 9:33 AM

Town of Fort Mill, SC - Report Creation

Fort Mill and the Greenway provide fantastic trails but nothing is connected to neighborhoods. I would like to see trails provide a more walkable community and decrease traffic. As an example, I live in Carolina Orchards and the Greenway Dairy Barn entrance is walkable but not safe. Carolina Orchards has a sidewalk and there is a crosswalk across 160 but then I have to walk in the road to the Greenway entrance. I end up driving the half mile between my house and the Greenway. Thanks!

27 days ago

This program would certainly add a much needed quality of life. The hustle/bustle of an enormous population would benefit from time spent outdoors, disconnecting from the craziness.

27 days ago

Crime has become a concern in the past year +; everywhere. How would you address this? Crime is through the roof out here!

27 days ago

Please consider a new trail between Still Ave and Old Nation Rd. There are some historic features in there like old road signs and railroad tracks. In addition, there's a creek that runs the length and opportunities to connect to Walter Elisha and the back entrance to Lake Haigler

27 days ago

Maintain and improve current trails, and then talk about grandiose future plans. I don't want to pay higher taxes that will lower impacted property owner's home values and increased security risks.

27 days ago

The town does not maintain the current trails within the city limits with current tax money. I don't want to pay more taxes for future trails that will lower property values for residents impacted by them. These are grand plans, but based on past history, this makes no sense. Take care, maintain and improve the trails we do have, and then let's talk about the future..

27 days ago

A wonder idea to connect the community together. Ensure trails are well lighted. Safety a concern for early morning joggers/children and women.

27 days ago

I grew up here my entire life and know the benefit of connecting all of the existing trails. If we could connect the greenway to downtown and downtown to riverwalk, that would be great! We also need more safe bike lanes for cyclists to connect trails and ride on the road. Every new road should have a large shoulder, sidewalk, and bike lane!

27 days ago

It would be nice if there was a trail system connecting Tega Cay and Fort Mill better. Hopefully when they construct the new I77/I160 interchange this will happen.

27 days ago

I love this initiative and am passionate about the need. Suggestions: 1) Somehow we need to be able to connect to the Riverwalk trail from the Fort Mill parkway. The parkway is a significant corridor into residential areas. Currently, the riverwalk is only accessible by car, unrealistic to bike down the parkway to the riverwalk. 2) "bike lanes" is unreasonable to consider it a trail or access, don't rely on that in your plan. Using Fort Mill parkway, no one in their right mind is riding a bike on the side of that road. 3) Create a trail along the north shore of the Catawba that leads to the 21 bridge to connect to the Riverwalk. These communities need to be connected anyways, and there's currently no trails/side walk - just isolated communities. 4) Trail on S. Doby's Bridge. Lots of town tax dollars comes from this area, but there are no connection points between communities. All these communities are a short distance from the schools, but you can't bike there, since there's no trails. 5) I would love to be able to take a trail to go Anne Spring Greenway, ideally, a trail on the side of the parkway, to be able to access it at the Rec center entrance. Again, another awesome trail system, but you have to drive to it, since you can't bike/no trail system on parkway to get there.

27 days ago

<https://publicinput.com/Reporting/ReportPreview/17425>

70/100

12/2/22, 9:33 AM

Town of Fort Mill, SC - Report Creation

I'd love to see Fort Mill develop the areas along the Catawba River with trails and restaurants like Rock Hill's Riverwalk and Greenville's River District.

27 days ago

Thank you for doing the survey. I hope more trails will be built and connected along the Catawba river and also trails into the town of Fort Mill. I also hope that busy crosswalks can be improved with flashing lights or something so that drivers understand that they're supposed to yield to the walkers/bikers.

27 days ago

Glad to see asking for community feedback..

27 days ago

How about taxing out of state residents that are coming to FM, they have to pay their fair share one time fee, what happened with the building fee, can that be use towards this expense, how about before starting new projects. Why we don't fixed current road conditions, make downtown more attractive, better sidewalks, get away from power lines hanging around houses and make them underground. Also, maybe large businesses can pay some towards this. It will make their company look better.

27 days ago

Thank you for taking this initiative, and I look forward to a more pedestrian and cycling friendly Fort Mill in the near future!

27 days ago

Equestrian trails should be separate from biking trails and dogs must be leashed at all times.

27 days ago

It would be great if the trail network could double as a wildlife corridor

27 days ago

It would be great if trail connectors could also serve as wildlife corridors.

27 days ago

I would like to see more trail accessibility for bikes.

27 days ago

I would like to see more trails that are accessible by bicycle.

27 days ago

I cant walk a long way but my kids and grandkids enjoy the trails.

27 days ago

Please keep the trails natural. Walking through the woods and preserving nature is of upmost importance. No room here for tourist. If you do this do it for the community. Traffic is horrible as it is without drawing more in.

27 days ago

Think of the future... This is what people are increasingly demanding in communities

27 days ago

The Fort Mill bypass needs a bike path along it for sure.

27 days ago

Build or get access for more mountain bike trails

27 days ago

Expand the trails to all parts of the Fort Mill area such as the Gold Hill Rd / Tega Cay corridor not just historical downtown Fort Mill.

27 days ago

<https://publicinput.com/Reporting/ReportPreview/17425>

71/100

12/2/22, 9:33 AM

Town of Fort Mill, SC - Report Creation

The sidewalks along Old Nations Rd are atrocious and dangerous. These pathways were used for years for people walking to work and now for walking to Elisha Park and downtown. It is my understanding the City has paid damages to people who have been inured due to injuries caused by disrepair. I believe your #1 priority should be the repair of these walkways.

27 days ago

Fort Mill Trails outside of Anne Springs Greenway are not very well communicated/broadcasted/easy to learn about and find.

27 days ago

I love living in this community and enjoying everything it has to offer.

27 days ago

Just stop developing every last piece. Land with no control and no infrastructure. We have the same single lane tiny roads with huge developments being added. It already takes 40 mins to get through the small town of Fort Mill due to traffic. Building Moratorium!!!

27 days ago

What I would love to see is a 20 mile long bike trail paved or with crushed gravel. We have some good walking trails, but as far as I know, nothing that provides for a good bicycle ride unless mountain biking.

27 days ago

I think a great thing for the fort mill trail network would be to connect the Nations ford greenway to the upcoming cross charlotte trail when that is completed. I would also like a sidewalk/trail connector from the Anne springs greenway near YMCA to the sidewalk near Empire Pizza in Indian land. That would greatly increase the ease of coming from one area to the other.

27 days ago

Please do not ruin ASC Greenway by linking to it with outside trails or trying to connect the property to the rest of Ft Mill. The ASC Greenway should be left alone! It is beautiful and a jewel.

27 days ago

Trail network is a great idea!

27 days ago

Need to install/connect sidewalks/ multi use paths on major arteries, I.e., Dobys Bridge, Banks St, Fairway Dr, etc.

27 days ago

N/A

27 days ago

Linking/construction of sidewalks & crosswalks within Ft Mill boundaries to intersect with ASC Greenway trailheads would be an improvement. Publishing & distribution of an inventory of existing walkable trails/sidewalks/pathways would help promote usage. Especially routes not currently considered by walkers/hikers.

27 days ago

I would like more information on existing network

27 days ago

I'd like to see a trail accessible on the west side of I-77 off Pleasant Road that would take me to downtown FtM or ASCG.

27 days ago

It would be ideal for the town to have easements along existing utility pathways such as the sewer easement currently being constructed along HWY-21. These potential trail pathways are already cleared and buffered from vehicular traffic.

27 days ago

<https://publicinput.com/Reporting/ReportPreview/17425>

72/100

12/2/22, 9:33 AM

Town of Fort Mill, SC - Report Creation

The natural areas and trail network I believe set our community apart for the other surrounding towns. I would love to see it preserved and expanded upon.

27 days ago

Every, Single, Fort Mill Public School should have a way to get to/from with sidewalks and or trails. there is absolutely no reason for not connecting ALL schools first with trails and sidewalks and the rest can follow. It will cut down on the amount of traffic immensely by connecting schools and neighborhoods with trails. I see kids walking on the grass ditches to get to school, yet we dont have enough funds to connect neighborhoods to the schools. Very dangerous. I lived in a community that prioritized connecting the area neighborhoods and schools and it was AMAZING! Bike rides, walking, running... the weekends were fun on the trails. Here, you have to drive everywhere.

27 days ago

would love to see a recycled path for running that is bouncy

27 days ago

N/a

27 days ago

I'm very pleased to see an investment being made in this regard.

27 days ago

I look forward to our trail network being expanded and improved!

27 days ago

Would love wide sidewalks to connect neighborhoods especially along busy Dobys Bridge Rd including those in unincorporated York County. For example, Riverchase is within town limits but the sidewalk does not connect to other neighborhoods/ Massey etc because Beckenham (right next door) does not have a sidewalk.

27 days ago

Integrate with Greenway and Carolina Thread trail

28 days ago

Trails near I-77 in Baxter are very loud due to the traffic. Expanding into quieter areas would be nice.

28 days ago

I would like to see trails for everyone to use, make sure they are accessible for everyone.

28 days ago

The trails for the most part have 0 lighting and this is needed especially during the fall/winter months. A lot of people like to walk/run after work and having no light makes it impossible.

28 days ago

Police need to enforce speed limits and stop signs in and around downtown. 30 mph on Banks and cars are driving 50, 20 mph in most neighborhoods and cars exceeding 35 mph. No one, including police and town vehicles, stop at four way stops on academy and confederate and Monroe White.. Police dont have to ticket, but should stop blatant speeders and people that blow through stop signs and warn them. Stopping them will slow them down enough without having to ticket them. Have a police presence on Tom Hall so people will slow down. I live less than a mile from my office and can't get across Main Street or Tom hall without worrying about getting hit by speeding and inattentive drivers. We already have a mechanism to make town more walkable...law enforcement enforcing traffic laws.

28 days ago

Would be great to connect to trails at river walk, sidewalk over interstate from downtown fort mill to Baxter area/tega cay

28 days ago

<https://publicinput.com/Reporting/ReportPreview/17425>

73/100

12/2/22, 9:33 AM

Town of Fort Mill, SC - Report Creation

Whiteville Park has a ravine that runs through most of the neighborhood. It is totally unkept and looks awful in some parts. There could be a trail that runs through it if it was maintained and beautified.

28 days ago

We love the trails but we cannot access them from our neighborhood. Would love to see sidewalks on the roads leading to the trails so we can bike or walk there. Specifically on the east side of 77 off the Ft mill bypass

28 days ago

We would greatly benefit from a trail along the fort mill bypass.

28 days ago

Please add more natural trails, the cost is considerably lower than paved trails and provides a connection to nature.

28 days ago

The lack of sidewalks and connections between the existing ones in communities is something I consider should be addressed as well.

28 days ago

We need sidewalks!!! The fact that I live 1 traffic light from Springfield Elementary but can't walk my kids to school is crazy. 2 traffic lights to the Greenway (Adventure Road) and no sidewalks the entire way!

28 days ago

Safe biking on busy roads ! Bike paths to shopping restaurants

28 days ago

Please consider linking to other existing trail systems adjacent to Fort Mill in your planning and recommendations.

28 days ago

Fort mill has a very very important history. So much of the history has been abandoned for the building of new infrastructure instead of acknowledging the history. The land we walk on was walked on by many former presidents and wars. That history needs to be remembered.

29 days ago

I've mentioned this before but was told it's in the county... Old Nation would be a perfect path,connector. The sidewalk is in terrible shape. Just imagine a beautiful trail/sidewalk that would connect W.E.Park to the Dairy Barn entrance. A free path for all to use.

29 days ago

Safer road biking, Bike lanes in all major roads

29 days ago

Need more bike lanes to get to trails/parks/etc

29 days ago

A well maintained trail network that accesses our communities and can be used by people that walk, ride bikes and use gold carts - simultaneously - could be very beneficial, especially if they provide access to schools and businesses without causing kids to cross traffic or walk on unsafe streets.

29 days ago

77 running through the middle makes things difficult, but it would be cool to see more places connected. Anne Springs with a trail down to the riverwalk, or dobys bridge out to indian land, etc

29 days ago

<https://publicinput.com/Reporting/ReportPreview/17425>

74/100

12/2/22, 9:33 AM

Town of Fort Mill, SC - Report Creation

Would love to see the natural side of this town flourish as it did in the 90s.

one month ago

There is not a trail system that connects to our neighborhood, if there was I could run/bike to work, walk my kids to school, and run errands.

one month ago

I am a proponent for complete streets—in an ideal world, we would have sidewalks and bike lanes on every road, which would help with connectivity for commuters and kids getting to school. I would love to see the defunct nation ford greenway revitalized or repurposed, as well as land the Town owns near Sugar Creek, to include blue ways in the plan, as well as greenways. Connecting people to nature is so important for mental health as well as physical health and it can not be left to private entities alone. Nature is for everyone in our community and as a Fort Mill native, I want to see our Town preserve more green space and expand our recreation opportunities beyond youth sports. I can not wait to see the cross Town trail system become a reality!

one month ago

Fort Mill should not tax its residents for this amenity given the size of Fort Mill and the current tax revenue. This is an after thought (albeit appreciated to be in the works) and FM is well behind other thriving communities of comparable size and tax base. A master sidewalk plan should really be a priority over a trail plan due the lack of sidewalks throughout Fort Mill that would provide safe connectivity to the trails. Or at least if a "Trail" bond will be proposed then incorporate both sidewalks and trails.

one month ago

Anne springs greenway is already such a great treasure!

one month ago

The more hiking trails the better

one month ago

Communicate more about them.

one month ago

.

one month ago

In addition to new trails, better publicize the existing trails. Also coordinate with York County and Rock Hill.

one month ago

Fort Mill could potentially be a perfect town for biking, walking, and a generally healthy lifestyle, etc. if it was set up properly. Would be great to safely and easily have a way to access various locations throughout town if the infrastructure was there. Id love to be able to walk or bike into town, a brewery, or to a farmer's market (we really need to establish a good one). Right now many roads have no sidewalks or bike lanes so it's very unsafe.

one month ago

A way to connect the Baxter village side of 77 to the Kingsley side, and a sidewalk that connects to the multi use trail would benefit the community greatly and allow safe crossing of 77 on bicycles/via walking.

one month ago

More sidewalks around town would be helpful

one month ago

I would like it to connect each side of town

one month ago

<https://publicinput.com/Reporting/ReportPreview/17425>

75/100

12/2/22, 9:33 AM

Town of Fort Mill, SC - Report Creation

I am unsure if the town leaders have any idea how many people run, walk, hike and mountain bike on the current trails. We will ALWAYS welcome more trails and many of us would gladly volunteer to help maintain. People are the most important thing currently, NOT more housing.

one month ago

Very unsafe bike lanes

one month ago

We need a trail along the Catawba River asap.

one month ago

na

one month ago

No comment

one month ago

We have lived here for 20 years and see the growth. While nice to open to tourism, it is nice to recognize those paying the bonds? I'm not sure the way that could be done .. but it would be nice

one month ago

I would love to see a wide sidewalk/bike path along the entire fort mill bypass. You could possibly build a path along the area where new piping is currently being installed? Many fort mill residents would benefit from this.

one month ago

Stop with the building of communities, the roads aren't in shape for the amount of housing and development. We miss Fort Mill.

one month ago

I moved from Fayetteville AR where we had one of the best trail systems in the country (for real, look it up - the Razorback Greenway) and I miss the ability to move easily through town JUST using the trails. My kids miss biking to school and frankly I miss them having a safe path by which to get there.

one month ago

Need a safe connection from Kingsley to fort mill hospital. Need a safe way to cross 77 on foot at 160

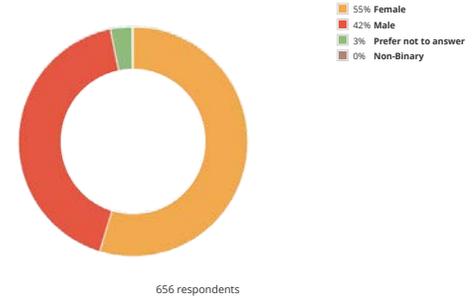
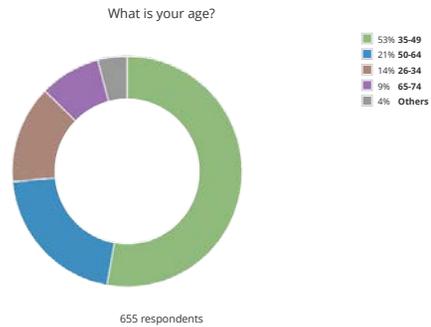
one month ago

Many of the sites I visit are too far apart to utilize trails/paths for transportation purposes, but as a runner, I would appreciate more paths/sidewalks where appropriate.

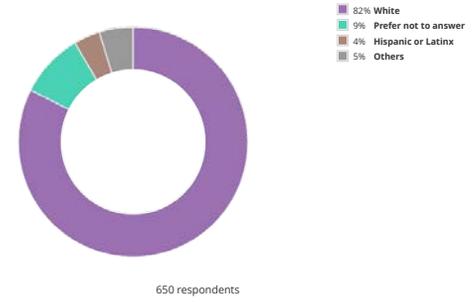
one month ago

<https://publicinput.com/Reporting/ReportPreview/17425>

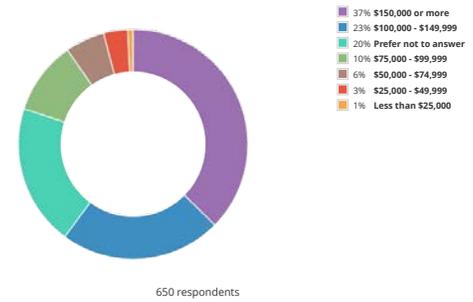
76/100



What is your race/ethnicity?

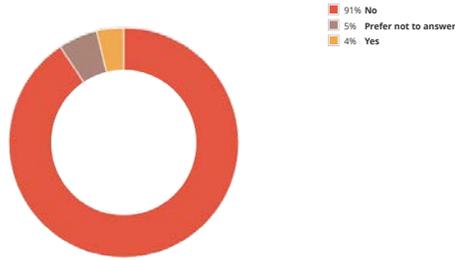


What is your annual household income?



12/2/22, 9:33 AM

Town of Fort Mill, SC - Report Creation  
Do you have a disability?



655 respondents

15. How did you hear about the survey? Please select all that apply.

59%	Social Media (Facebook, Twitter, Instagram, etc.)	585 ✓
17%	Word of Mouth (from friends, family, co-workers)	170 ✓
17%	Email	168 ✓
8%	Other (please specify)	84 ✓
5%	Town Website	49 ✓
3%	Newspaper	26 ✓

997 Respondents

Are you familiar with the existing trails in Fort Mill (i.e., Nation Ford Greenway, Anne Springs Close Greenway, Founders Trail, etc.)? Existing trails and greenways in the study area are displayed on the map below. Draw lines on the map where you currently like to walk, bike, and/or hike.

No data to display...

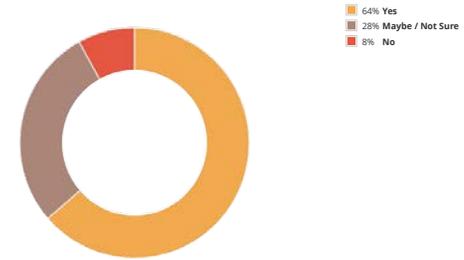
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99/100

12/2/22, 9:33 AM

Town of Fort Mill, SC - Report Creation

12. Trail networks are often funded through bonds. Bonds are debt issued by local governments that are often used to finance large capital projects. If the opportunity presents itself, would you pay additional taxes to support a bond to expand the development of Fort Mill's trail network?



1,023 respondents

Loading more report objects...

<https://publicinput.com/Reporting/ReportPreview/17425>

100/100

# Public Workshop



PUBLIC WORKSHOP

## PLAN OVERVIEW

The goal of the *Fort Mill Trail Master Plan* is to identify existing and potential trails in the Town and find ways to connect them to transportation corridors, parks, schools, and other places of interest within the Town. Ultimately, the plan will help establish a long-range vision for trails within the community.

We look forward to receiving more community input from this project. For more information, please visit the Town of Fort Mill’s website at [www.fortmillsc.gov/TrailMasterPlan](http://www.fortmillsc.gov/TrailMasterPlan).

## VISION + GOALS

The Steering Committee for the *Fort Mill Trail Master Plan* formed the vision and goals for the plan through a visioning exercise held during the first Steering Committee Meeting. The goals of the *Fort Mill Trail Master Plan* focus on themes of accessibility, connectivity, safety, project feasibility, and community. These themes served as a guide throughout the planning process. The goals in this plan reflect the proposed outcomes and recommendations of the plan. The community vision for the plan is provided below and the goals are described in the graphic to the right.

### Community Vision

“The Town of Fort Mill will be safe and accessible for community members of all ages and abilities to bike, walk, and hike along a comprehensive trail network that connects key destinations such as neighborhoods, parks, schools, commercial centers, and neighboring communities.”

**ACCESSIBILITY + CONNECTIVITY**  
Ensure that Fort Mill’s trail network expands recreational access for residents and visitors and connects to key destinations within Town limits and neighboring jurisdictions.

**SAFETY**  
Address safety needs of users of all ages and abilities in the development of Fort Mill’s trail network and propose safety improvements at critical intersections and access points.

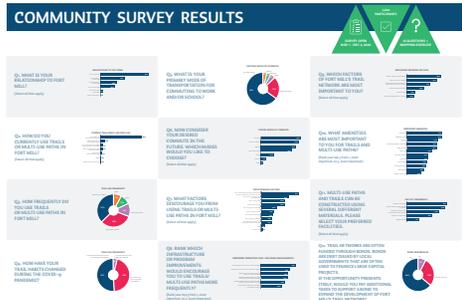
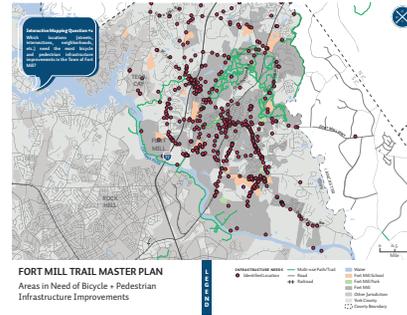
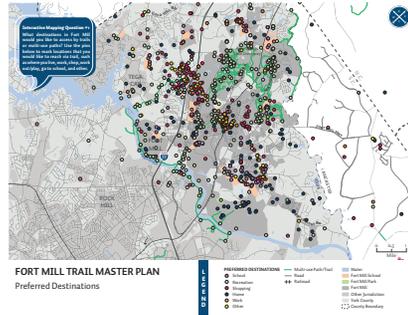
**FEASIBILITY**  
Prioritize the development of a comprehensive trail network that can be implemented and maintained with Town resources.

**COMMUNITY**  
Foster engagement and collaborate with community groups and residents to promote future development, use, and maintenance of Fort Mill’s trail network.

**Community Goals**  
Founders Trail, Fort Mill

Please flip the page to view the draft trail network.





### PROGRAMS + POLICIES

RECOMMENDED PROGRAMS	RECOMMENDED POLICIES
<ul style="list-style-type: none"> <li>Establish dedicated funding in the Capital Improvements Plan (CIP) for trails and multi-use paths</li> <li>Establish a specific maintenance policy and operations plan</li> <li>Reduce Town-wide speed limit to 30 mph</li> <li>Create standard guidelines for trail design within the Town's policies and guidance documents</li> <li>Developers should dedicate right-of-way and contribution for trails within new developments and redevelopment projects</li> <li>Use native plants in landscaping along trails or multi-use paths</li> <li>Adopt outdoor lighting ordinances or codes</li> <li>Adopt a Complete Streets Ordinance and/or Program</li> <li>Encourage construction of open-space subdivision design within the Town</li> <li>Minimize the number of on-street crossings and promote trail connections at existing grade-secs</li> <li>Encourage land and easement acquisition along trail corridors to ensure permeability with trails</li> <li>Support all development in tandem with the implementation of riparian buffer installation and mitigation projects</li> <li>Establish a trail classification system and accompanying trail use policies</li> <li>Establish trail walkability standards</li> </ul>	<ul style="list-style-type: none"> <li><b>Encouragement Programs</b> <ul style="list-style-type: none"> <li>Trial Network Map and/or Mobile App</li> <li>Walking Challenge</li> <li>Bicycle Parking</li> <li>Bicycle Repair Stations</li> <li>BlockWalk to School Day Events</li> <li>BlockWalk to Work Day Events</li> <li>Theme-Biking Walking events</li> <li>Prosecco in Town (Bicycle Walk with a Dog)</li> <li>Trial Scavenger Hunt/GeoCaching</li> <li>Greenway Trial Ambassadors</li> <li>Block-Friendly Community Organization</li> <li>Walk-Friendly Community Organization</li> <li>Trial Town Program</li> </ul> </li> <li><b>Enforcement Programs</b> <ul style="list-style-type: none"> <li>Trial Walk Program</li> <li>South Carolina Code of Law Concerning Bicycles</li> <li>South Carolina Code of Law Concerning Pedestrians</li> <li>South Carolina Target Zero Project</li> </ul> </li> <li><b>Project Based Programs</b> <ul style="list-style-type: none"> <li>Pop-up Demonstration Trail Programs</li> <li>Pilot the Payment Program</li> <li>Trial Month Program</li> <li>Adopt-a-Trial Segment</li> </ul> </li> <li><b>Educational Programs</b> <ul style="list-style-type: none"> <li>Trial Court Program</li> <li>Trial User Surveys</li> <li>Block and Walk Audits</li> <li>Adopt-a-Trial Segment</li> </ul> </li> </ul>

## APPENDIX D: FUNDING RESOURCES

The project team identified potential funding sources, compiled criteria and requirements, and related the anticipated schedule of funding to the target projects. Costs of the projects will be compared with funding needs, so that long-term programming for local matching funds can be accomplished. The team explored various funding options from public and private sources to determine availability and requirements for grants that could potentially fund trail projects throughout the Town of Fort Mill in the future.

### FEDERAL FUNDING

Communities in South Carolina have partnered with Federal agencies to improve crossings and build multi-use paths, greenways, and trails. Federal funding is primarily distributed to municipalities through state agencies and Metropolitan Planning Organizations (MPO), as well as through discretionary grant programs.

The Fixing America's Surface Transportation (FAST) Act authorizes transportation funding for highway, transit, rail, bicycle, pedestrian projects, safety programs, and infrastructure. FAST Act funding is administered by the Federal Highways Administration (FHWA). FHWA distributes funding to SCDOT and directly to MPOs through the Locally Administered Projects Program (LAPP). To obtain Federal funding, communities must submit their candidate projects to their respective MPO or RPO to then be entered into the SCDOT's transportation investment strategies. This formula ranks projects and identifies those to be funded in the State Transportation Improvement Program (STIP). These funds require a 20% match from the municipality. Federal transportation funds for bicycle and pedestrian projects are primarily distributed through five programs: Transportation Alternatives (TA), Congestion Mitigation & Air Quality (CMAQ), the Recreational Trails Program, (RTP), the Highway Safety Improvement Program (HSIP), and the Carbon Reduction Program (CRP).

Please reference the following link for information related to potential eligibility for bicycle and pedestrian activities and projects under the USDOT's surface transportation funding programs:

[https://www.fhwa.dot.gov/environment/bicycle\\_pedestrian/funding/funding\\_opportunities.pdf?u=092922](https://www.fhwa.dot.gov/environment/bicycle_pedestrian/funding/funding_opportunities.pdf?u=092922)

Additional federal funding sources for bicycle and pedestrian planning, infrastructure, and programs are administered through the Department of Housing and Urban Development (HUD) with the Community Development Block Grant (CDBG) Program. Several discretionary grant programs are administered by the US Department of Transportation, National Park Service, and the National Endowment for the Arts. Several federal programs were added under the Bipartisan Infrastructure Law (BIL) and the Inflation Reduction Act (IRA) that can be used to help fund active transportation projects.

### STATE & MPO ADMINISTERED FUNDING

#### TRANSPORTATION ALTERNATIVES

Transportation Alternatives (TA) provides federal funds for community-based projects that expand travel choices and enhance the transportation experience by integrating modes and improving the cultural, historic, and environmental aspects of our transportation infrastructure. In South Carolina, TA funds are administered by SCDOT, and eligible projects include pedestrian facilities, bicycle facilities, and streetscaping projects.

Funds are available only on a reimbursement basis. Only after a project has been approved by the State Department of Transportation or Metropolitan Planning Organization (MPO) and the FHWA division office, can costs become eligible for reimbursement. Costs must be incurred after FHWA division office project approval, or they are not eligible for reimbursement.

The divisions for the population-based suballocation are:

- ▶ \$2,999,400 Million - In urbanized areas of the State with an urbanized area population of over 200,000, also known as a Transportation Management Area;

- ▶ \$1,834,589 Million - In areas of the State other than urban areas with a population greater than 5,000; and
- ▶ \$2,600,164 Million - In areas of the state with a population less than 5,000.

The target federal allocation for the Rock Hill-Fort Mill Area Transportation Study (RFATS) MPO is \$110,833. The Federal government will pay for up to 80% of eligible project costs for a TAP project. A local match is required to pay for 20% or more of the remaining project costs. TA funds can be used to fund trail maintenance efforts.

<https://www.scdot.org/projects/community-transportation-alternatives.aspx>

SCDOT created a document guiding local governments interested in pursuing TA funding. The Transportation Alternatives Program Guide provides an overview of the program, its requirements, and the steps needed to compete for funding. The document is available below:

[https://www.scdot.org/projects/pdf/TAP\\_Guidance.pdf](https://www.scdot.org/projects/pdf/TAP_Guidance.pdf)

### CONGESTION MITIGATION & AIR QUALITY

Funding for projects that demonstrate reductions in ozone and particulate matter pollutants. Funding is used for projects within EPA designated non-attainment or maintenance areas, as well as incident response services. In South Carolina, the State Highway Emergency Program (SHEP) is the incident response services provided in Beaufort, Charleston, Columbia, Florence, Grand Strand/Myrtle Beach, Rock Hill, and Greenville/Spartanburg urban areas. Incident responders make minor repairs to disabled vehicles, assist with traffic control and incident management, and provide first aid until emergency medical service arrives. CMAQ funds typically require a 20% match.

Each project funded under the CMAQ program must meet three basic criteria:

- ▶ It must be a transportation project.
- ▶ It must generate an emissions reduction.
- ▶ It must be in (or benefit) a nonattainment or maintenance area.

The following pedestrian and bicycle projects are eligible to receive funding:

- ▶ Building shared-use paths, bicycle racks, and other facilities intended to reduce automobile trips, and that are not exclusively for recreational use;
- ▶ Outreach activities that promote safe bicycle use; and
- ▶ Establishing and funding state-level pedestrian/bicycle coordinator positions.

The FAST Act establishes a formula that is used to determine how much money each state receives through the CMAQ program. States may transfer up to 50% of these funds to other programs. Each state allocates these funds to MPOs within its jurisdiction, who are responsible for suballocating these funds to different projects.

<https://www.fhwa.dot.gov/environment/air-quality/cmaq/>

### HIGHWAY SAFETY IMPROVEMENT PROGRAM

The Highway Safety Improvement Program (HSIP) is a core Federal-aid program with the purpose of achieving a significant reduction in fatalities and serious injuries on all public roads. Projects are evaluated using crash data and the cost-effectiveness of proposed improvements. South Carolina's HSIP 2020 Annual Report states that funding for non-motorized users was \$5 million. Additional information can be reviewed at <https://safety.fhwa.dot.gov/hsip/reports/pdf/2020/sc.pdf>

<https://highways.dot.gov/safety/hsip>

### RECREATIONAL TRAILS PROGRAM

The Recreational Trails Program (RTP) provides funds to state agencies to develop and maintain recreational trails and trail-related facilities for both non-motorized and motorized recreational trail uses. RTP is an assistance program of the Department of Transportation's Federal Highway Administration (FHWA). Municipalities must provide project funds upfront and are reimbursed upon completion of deliverables.

The South Carolina Department of Parks, Recreation and Tourism (SCPRT) administers the Recreational Trails Program (RTP) under the approval of the Federal Highway Administration (FHWA). RTP, a federally funded program, receives its funding from a portion of federal gas taxes paid on fuel used in nonhighway recreational vehicles. The grant program requires recipients to pay 100 percent of the total cost before being reimbursed for 80 percent of the eligible costs. Additional facts related to the program are listed below.

- ▶ Funds can be spent on both Motorized and Nonmotorized recreational trail projects — to construct new recreational trails, improve/maintain existing trails, develop/enhance trailhead or trailside facilities and acquire trail corridors.
- ▶ Eligible applicants include local, state, and federal governmental agencies, qualified private organizations, and registered nonprofits.
- ▶ Individual trail grants can range from a minimum of \$10,000 (\$12,500 would be the total project cost) to \$100,000 maximum (\$125,000—total project cost), except for Motorized projects.
- ▶ Motorized trail projects are not limited to \$100,000 — these projects are eligible for the maximum amount of Motorized funding available.

[https://www.fhwa.dot.gov/environment/recreational\\_trails/](https://www.fhwa.dot.gov/environment/recreational_trails/)

<https://www.scpert.com/recreation/recreation-grant-programs/recreational-trails-program>

### COMMUNITY DEVELOPMENT BLOCK GRANT

The Community Development Block Grant (CDBG) Program provides annual grants on a formula basis to states, cities, and counties to develop viable urban communities by providing decent housing, suitable living environments, and expanding economic opportunities for low- and moderate-income persons. The program is authorized under Title 1 of the Housing and Community Development Act of 1974. CDGB funds are allocated at the federal level by HUD and at the state level by the Texas Department of Agriculture. All municipalities are eligible to receive State CDBG funds except for entitlement communities, which receive funds directly from HUD.

The following 11 municipalities are entitlement communities in South Carolina:

- ▶ City of Aiken
- ▶ City of Anderson
- ▶ City of Charleston
- ▶ City of Columbia
- ▶ City of Florence
- ▶ City of Greenville
- ▶ City of Rock Hill
- ▶ City of Spartanburg
- ▶ City of Sumter
- ▶ Town of Hilton Head Island

- ▶ Town of Summerville

In addition, the following six counties in South Carolina are eligible to receive State CDBG funds:

- ▶ Charleston County
- ▶ Greenville County
- ▶ Lexington County
- ▶ Richland County
- ▶ Spartanburg County
- ▶ Horry County

CDBG funds may be used for activities which include, but are not limited to:

- ▶ Acquisition of real property
- ▶ Relocation and demolition
- ▶ Rehabilitation of residential and non-residential structures
- ▶ Construction of public facilities and improvements, such as water and sewer facilities, streets, neighborhood centers, and the conversion of school buildings for eligible purposes
- ▶ Public services, within certain limits
- ▶ Activities relating to energy conservation and renewable energy resources
- ▶ Provision of assistance to profit-motivated businesses to carry out economic development and job creation/retention activities

[https://www.hud.gov/program-offices/comm\\_planning/communitydevelopment](https://www.hud.gov/program-offices/comm_planning/communitydevelopment)

[https://www.hud.gov/states/south\\_carolina/community/cdbg](https://www.hud.gov/states/south_carolina/community/cdbg)

#### STATE & COMMUNITY HIGHWAY SAFETY GRANT PROGRAM

The State and Community Highway Safety Grant Program (also referred to as Section 402) is a federal grant program that provides funding for education, enforcement, and research programs which are aimed at reducing traffic crashes, injuries, deaths, and property damage. Bicycle and pedestrian safety projects qualify for Section 402 funding; however, qualifying uses are restricted. Funding is contingent on the establishment of a state Highway Safety Plan that considers crash data as it relates to national traffic safety goals. Forty percent (40%) or more of funds allocated to a state must be used to address safety issues on a local rather than state level. Also, federal funds can cover as much as 80% of a project's total budget. Each year, South Carolina receives approximately \$4 million in Section 402 funding. The grant funding cycle is typically from October 1 - September 30.

<https://safety.fhwa.dot.gov/legislationandpolicy/policy/section402/>

<https://scdps.sc.gov/ohsjp/hs-grants>

#### CARBON REDUCTION PROGRAM

The Carbon Reduction Program (CRP) was established in 2021 with the passage of the Infrastructure Investment and Jobs Act (IIJA) to fund projects designed to reduce transportation emissions. Project types eligible for funding include on- and off-road facilities for pedestrians and bicyclists, as well as public transportation, congestion pricing, and electrification projects. The CRP also requires each state to consult with its MPOs on the development of a carbon reduction strategy which must be updated at least every four years and identify projects and strategies to support the reduction of transportation emissions. South Carolina has been allocated \$21.6 million in the 2022 Fiscal Year to reduce carbon emissions.

[https://www.fhwa.dot.gov/bipartisan-infrastructure-law/crp\\_fact\\_sheet.cfm](https://www.fhwa.dot.gov/bipartisan-infrastructure-law/crp_fact_sheet.cfm)

## DISCRETIONARY GRANTS

### *REBUILDING AMERICAN INFRASTRUCTURE WITH SUSTAINABILITY AND EQUITY (RAISE)*

The 2021 Consolidated Appropriations Act appropriated \$1 billion to be awarded by the Department of Transportation (DOT) for National Infrastructure Investments, formerly known as Transportation Investment Generating Economic Recovery (TIGER) and Better Utilizing Investments to Leverage Development (BUILD) Grants and now as Rebuilding American Infrastructure with Sustainability and Equity (RAISE) Grants.

Congress has dedicated nearly \$9.9 billion for thirteen rounds of National Infrastructure Investments to fund projects that have a significant local or regional impact.

The eligibility requirements of RAISE allow project sponsors at the State and local levels to obtain funding for multi-modal, multi-jurisdictional projects that are more difficult to support through traditional DOT programs. RAISE can fund port and freight rail projects, for example, which play a critical role in our ability to move freight but have limited sources of Federal funds. RAISE can provide capital funding directly to any public entity, including municipalities, counties, port authorities, tribal governments, MPOs, or others in contrast to traditional Federal programs which provide funding to very specific groups of applicants (mostly State DOTs and transit agencies). This flexibility allows RAISE and our traditional partners at the State and local levels to work directly with a host of entities that own, operate, and maintain much of our transportation infrastructure, but otherwise cannot turn to the Federal government for support.

The projects must be awarded based solely on the selection criteria. The primary selection criteria are safety, environmental sustainability,

quality of life, economic competitiveness, and state of good repair. The secondary selection criteria include partnership and innovation. The Federal share of project costs may not exceed 80 percent for a project located in an urban area. The Secretary may increase the Federal share of costs above 80% for projects located in rural areas and for planning projects located in areas of persistent poverty.

#### Project Awards:

- ▶ Total Funding: \$1 billion.
- ▶ Minimum Project Awards:
  - » Urban Projects: \$5 million.
  - » Rural Projects: \$1 million.
  - » Planning Grants: No project minimum required.
- ▶ Maximum Awards:
  - » Urban/Rural Projects: \$25 million.
  - » Per State: \$100 million
  - » Geographic Distribution: 50% of total funds (\$500 million) awarded to both urban and rural projects.
- ▶ RAISE 2022 Awards for South Carolina: \$12,845,300

<https://www.transportation.gov/RAISEgrants>

### *SAFE STREETS AND ROADS FOR ALL GRANT PROGRAM*

The Safe Streets and Roads for All Grant Program (SS4A) was established in 2021 under the Infrastructure Investment and Jobs Act (IIJA) as a discretionary grant. \$5 billion was appropriated for the 5-year period starting with fiscal year 2022. Regional, local, and tribal agencies are eligible to apply for funding to develop or update a comprehensive safety action plan and carry out planning and development of projects and strategies within the action plan. Two types of grants are available: Action Plan Grants and Implementation Grants.

Action Plan Grants may be used for activities such as:

- ▶ Leadership commitment and goal setting
- ▶ Safety analysis
- ▶ Engagement and collaboration
- ▶ Policy and process changes
- ▶ Strategy and project selections

Implementation Grants may be used for activities such as:

- ▶ Improving pedestrian crossings and closing sidewalk gaps
- ▶ Complete Street improvements
- ▶ Developing bicycle networks
- ▶ Creating safe routes to school

#### **RECONNECTING COMMUNITIES PILOT PROGRAM**

The BIL established the new Reconnecting Communities Pilot (RCP) discretionary grant program which is funded with \$1 billion over the next 5 years. It is the first-ever Federal program dedicated to reconnecting communities that were previously cut off from economic opportunities by transportation infrastructure. Funding supports planning grants and capital construction grants, as well as technical assistance, to restore community connectivity through the removal, retrofit, mitigation, or replacement of eligible transportation infrastructure facilities.

<https://www.transportation.gov/grants/reconnecting-communities>

#### **NEIGHBORHOOD ACCESS AND EQUITY GRANTS**

Neighborhood Access and Equity Grants is a new program included in

the Inflation Reduction Act. Approximately one-third of the funding can be used towards lower-income areas. This bill establishes discretionary grants for removing, replacing, or retrofitting highways and freeways to improve connectivity in communities and for planning and capacity building to increase community involvement in transportation planning and related activities. The bill prioritizes grants that fund projects in economically disadvantaged communities or that meet other criteria. Funding can be used for several different type of projects, including covering a highway, turning a highway into a boulevard, adding trails and bike lanes, installing sound barriers, providing better connections to transit, using green infrastructure to mitigate storm runoff, reducing urban heat island hot spots, installing safety features, and curbing air pollution. The FHA must award the grants to state, tribal, territorial, and local governments and metropolitan planning organizations.

<https://www.congress.gov/bill/117th-congress/house-bill/5267>

#### **ACTIVE TRANSPORTATION INFRASTRUCTURE INVESTMENT PROGRAM**

This new program includes \$200 million annually and is subject to appropriations. The program establishes competitive connectivity grants that strategically invest in projects that connect active transportation networks and trail networks, accelerating local and regional plans to create safe and convenient routes to everyday destinations. This program is not yet funded (currently working on appropriations); funding for the program should be appropriated at \$500 million per year.

[https://www.railstotrails.org/media/1173656/caats\\_atiip\\_2922.pdf](https://www.railstotrails.org/media/1173656/caats_atiip_2922.pdf)

#### **PROMOTING RESILIENT OPERATIONS FOR TRANSFORMATIVE, EFFICIENT, AND COST-SAVING TRANSPORTATION (PROTECT) FORMULA PROGRAM**

Funding is available to states over a period of five years to make transportation infrastructure more resilient to future weather events

and other natural disasters by focusing on resilience planning, making resilience improvements to existing transportation assets and evacuation routes, and addressing at-risk highway infrastructure. Eligible projects include highway and transit projects, bicycle and pedestrian facilities, and port facilities including those that help improve evacuations or disaster relief. States are encouraged to work with regional and local partner organizations to prioritize transportation and emergency response improvements, as well as address vulnerabilities.

<https://highways.dot.gov/newsroom/biden-administration-announces-new-protect-formula-program-73-billion-bipartisan>

#### FEDERAL LANDS ACCESS PROGRAM

The Federal Lands Access Program (FLAP) provides funds for projects to improve Federal Lands Access Transportation Facilities that provide access to, are adjacent to, or are located within federal lands. This can include public roads, bridges, paved trails, or transit systems that are owned and/or maintained by the state, county, town, township, tribal, municipal, or local government. Funds may be used for the costs of transportation planning, research, engineering, preventive maintenance, rehabilitation, restoration, construction, and reconstruction of transportation facilities located on or adjacent to, or that provide access to, federal lands. Applicable activities include parking areas; acquisition of scenic easements or historic sites; bicycle and pedestrian provisions; environmental mitigation; public safety; and roadside rest areas. Other eligible activities include the operation and maintenance of transit facilities, and any transportation project that is within, adjacent to, or provides access to federal land.

<https://highways.dot.gov/federal-lands/programs-access>

<https://highways.dot.gov/federal-lands/programs-access/sc>

#### FEDERAL LAND AND WATER CONSERVATION FUND

The Land and Water Conservation Fund (LWCF) was established by Congress in 1964 to fulfill a bipartisan commitment to safeguard natural areas, water resources and cultural heritage, and to provide recreation opportunities to all Americans. The LWCF program is divided into the "State Side" which provides grants to State and local governments for the acquisition and development of public outdoor recreation areas and facilities, and the "Federal Side" which is used to acquire lands, waters, and interests therein necessary to achieve the natural, cultural, wildlife, and recreation management objectives of federal land management agencies. State Side funds are distributed by the State and Local Assistance Programs Division of the National Parks Service (NPS). Funding is available as 50/50 matching grants to states and territories to plan, acquire, and develop public lands for outdoor recreation. Projects are selected by states and submitted to NPS for approval. To be eligible for LWCF assistance, every state must prepare and regularly update a Statewide Comprehensive Outdoor Recreation Plan (SCORP). Applicants can request a maximum grant of \$500,000. An applicant must match the grant with a minimum of 50%. Due to a federal share cap of \$500,000, a greater match is required for projects that exceed total costs of \$1 million. South Carolina has invested more than \$60 million of Land & Water Conservation Funds in approximately 1,171 land acquisition and recreation facilities projects since the program began in 1965.

<https://www.nps.gov/subjects/lwcf/stateside.htm>

<https://www.scprt.com/recreation/recreation-grant-programs/land-and-water-conservation-fund#:~:text=The%20Land%20and%20Water%20Conservation,outdoor%20recreation%20areas%20and%20facilities>

### **RIVERS, TRAILS, AND CONSERVATION ASSISTANCE PROGRAM**

The National Parks Service (NPS) Rivers, Trails, and Conservation Assistance Program (RTCA) supports community-led natural resource conservation and outdoor recreation projects across the nation. Although RTCA is not a traditional funding program, NPS staff provide planning, design, and technical expertise for trails and outdoor recreation projects. Depending on the scale of the project, RTCA can invest up to four years of planning and project development assistance. Eligible entities include community groups, non-profit organizations, tribes, and government agencies.

Technical Assistance Services:

- ▶ Define project vision and goals
- ▶ Set priorities and build consensus
- ▶ Inventory and map community resources
- ▶ Identify funding strategies
- ▶ Identify and analyze key issues and opportunities
- ▶ Design community outreach, participation, and partnerships plans
- ▶ Create project management and strategic action plans
- ▶ Develop concept plans for trails, parks, and natural areas

<https://www.nps.gov/orgs/rtca/index.htm>

### **NATIONAL ENDOWMENT FOR THE ARTS OUR TOWN PROGRAM**

Our Town is the National Endowment for the Arts' (NEA) creative placemaking grants program. Through project-based funding, the NEA supports projects that integrate arts, culture, and design activities into

efforts that strengthen communities by advancing local economic, physical, and/or social outcomes. These projects require a partnership between a local government entity and non-profit organization, one of which must be a cultural organization; and should engage in partnership with other sectors (such as agriculture and food, economic development, education and youth, environment and energy, health, housing, public safety, transportation, and workforce development). Cost share and matching grants range from \$25,000 to \$150,000, with a minimum cost share or match equal to the grant amount.

<https://www.arts.gov/grants/our-town>

### **COMMUNITY FACILITIES DIRECT LOAN AND GRANT PROGRAM**

The Community Facilities Direct Loan and Grant Program is administered in South Carolina by the US Department of Agriculture (USDA) Rural Development. This program provides affordable funding to develop essential community facilities in rural areas. An essential community facility is defined as a facility that provides an essential service to the local community for the orderly development of the community in a primarily rural area, and does not include private, commercial, or business undertakings. Rural communities with less than 20,000 residents are eligible to apply for loan or grant funding to purchase, construct, or maintain essential community facilities. Street improvements are eligible as public facilities. Projects must serve a rural area, and communities with a population under 5,500 or a median household below 80% of the state nonmetropolitan median household income are prioritized.

<https://www.rd.usda.gov/programs-services/community-facilities/community-facilities-direct-loan-grant-program/sc>

## **STATE FUNDING**

Public sector funding is often obtained from state agencies. These funds

are generally made available to local governments through grant-in-aid formulas. Generally, the easiest way to obtain state grant funding is to include projects in locally adopted plans before submitting a grant application. Several state funding opportunities are described below.

#### **STATE TRANSPORTATION IMPROVEMENT PROGRAM**

South Carolina's Statewide Transportation Improvement Program (STIP) is a six-year list of all projects or program areas receiving federal funding (including bridge replacements, safety, road resurfacing, interstate maintenance and upgrades, primary and secondary road system upgrades, transportation alternatives, congestion mitigation and air quality, and public transportation) through the funding period. The document is updated every three years and is revised on a continual basis to reflect new information. SCDOT, the Federal Highway Administration and Federal Transportation Administration approve the STIP.

<https://www.scdot.org/inside/planning-stip.aspx>

#### **SOUTH CAROLINA CONSERVATION BANK ACT**

The South Carolina Conservation Bank's mission is to improve the quality of life in South Carolina through the conservation of significant natural resource lands, wetlands, historical properties, archeological sites, and urban parks. Through June 30, 2020, the Conservation Bank has conserved 96,711 acres of isolated wetlands, 1,982 acres of Carolina Bays, and 405 miles of river/creek frontage.

The Act is funded by placing twenty-five cents out of each one dollar thirty-five cents of the Documentary Deed Stamp recording fee into a trust for the Conservation Bank to carry out the Act. Funding began in July 2004, and participating landowners may sell property outright or sell conservation easements and retain traditional use of the land. Funding of proposals is based on the amount of funding available, and the priority of the proposals as set by the Conservation Bank Board in accordance with the statutory criteria.

Funding from the South Carolina Conservation Bank can be used to accomplish the following:

- ▶ Protect significant natural resource areas and wildlife habitats
- ▶ Protect water quality
- ▶ Maintain the State's forest lands Protect farmlands, especially family farms
- ▶ Protect and enhance the State's natural beauty
- ▶ Protect and enhance significant historical and archaeological sites enhance public access for outdoor recreation and preserve traditional uses such as hunting, fishing, and other types of outdoor recreation

<http://sccbanc.sc.gov>

#### **SOUTH CAROLINA DEPARTMENT OF TRANSPORTATION (SAFETEA)**

SCDOT manages the implementation of all transportation programs and improvements throughout the state through the Transportation Improvement Program (STIP). The STIP is the six-year transportation improvement program for all projects or program areas receiving federal funding. The STIP covers all federally funded improvements with approved funding. SCDOT expects to begin these projects in the next six-year period. The report details the funding of each project and the work phase for each project in the appropriate year. The present STIP includes the fiscal years 2021 - 2027. It is produced and printed every two years as part of the SAFETEA federal requirement. The following funding has been defined within Safe, Accountable, Flexible, Efficient Transportation Equity Act: A Legacy for Users (SAFETEA-LU) for the SCDOT.

### SAFE ROUTES TO SCHOOL PROGRAM

SCDOT is responsible for disbursing funds to local programs in accordance with state policies and applicable federal laws and guidance. Most states have a coordinator responsible for administering the Safe Routes to School program or have shifted to having a coordinator of the Transportation Alternatives Program. South Carolina's Safe Routes to School contacts are provided below with a link containing additional information on the program.

Amy Blison | [blinsonal@scdot.org](mailto:blinsonal@scdot.org)

Ashley Johnson | [johnsonha@scdot.org](mailto:johnsonha@scdot.org)

<https://saferoutespartnership.org/safe-routes-school/srts-program/state-contacts>

### SOUTH CAROLINA DOT PEDESTRIAN & BICYCLE PROGRAM

SCDOT published the state's first Pedestrian and Bicycle Safety Action Plan (PBSAP) in 2022. The PBSAP is a result of extensive data analysis and review of best practices to develop a plan to reverse the increasing number of pedestrian and bicyclist deaths and serious injuries in the state. According to this plan, SCDOT allocates a portion of its annual Highway Safety Improvement Program (HSIP) federal funds to perform road safety audits (RSA) at locations identified to have a high density of pedestrian- and bicycle-involved crashes. Each year, ten to twenty locations are identified and studied by a multi-disciplinary team to identify highway safety issues and to develop an implementation plan to improve the safety of these locations.

<https://www.scdot.org/projects/bikeped.aspx>

<https://www.scdot.org/projects/pdf/SC%20Pedestrian%20and%20Bicycle%20Safety%20Action%20Plan.pdf>

### "C" PROGRAM

SCDOT partners with its 46 counties in the C-Fund Program to fund local

transportation projects and improvements to state and county roads and city streets. Funds come from 2.66 cents per gallon of the state gasoline tax distributed to each of the 46 counties based on population, land area, and rural road mileage. Due to increases, the new cost totals 3.99 cents per gallon. This increase must be used exclusively for repairs, maintenance, and improvements to the state highway system.

Counties must spend at least 25 percent of their "C" funds on the state highway system for construction, improvements, and maintenance. Proceeds from the increase in cents per gallon must be used exclusively for repairs, maintenance, and improvements to the State Highway System. In fiscal year 2021-2022, the minimum state requirement is approximately 33%. The County Transportation Committee (CTC) is responsible for selecting and approving projects to be funded. A Donor Bonus allocation goes to counties which contribute more to the "C" Fund than they receive. SCDOT transfers the additional monies among the donor counties in this process.

Funding is apportioned to York County based on these criteria:

- ▶ 1/3 based on the ratio of land area in York County compared to the land area in the state
- ▶ 1/3 based on the ratio of York County population (as determined by the latest 10-year federal census) compared to the state's population
- ▶ 1/3 based on the ratio of rural road mileage in York County compared to total rural mileage in the state

<https://www.scdot.org/projects/c-program.aspx>

<https://www.yorkcountygov.com/256/C-Fund-Critical-Needs-Programs>

#### ***SOUTH CAROLINA PARKS AND RECREATION DEVELOPMENT FUND***

The Park and Recreation Development (PARD) Fund is a state-funded, noncompetitive, reimbursable grant program for eligible local governments or special purpose district entities which provide recreational opportunities within each county. PARD assists in the development of new public recreation facilities or with the enhancement/renovation of existing facilities. It is an 80-20 match program. Projects need the endorsement of a majority weighted vote factor of County Legislative Delegation members. The grant cycle is monthly, and grants are due on the 10th of each month. Eligible entities are notified of new allocation amounts each July.

Alesha Cushman, PARD Grants Coordinator | [acushman@scprt.com](mailto:acushman@scprt.com)

<https://www.scprt.com/recreation/recreation-grant-programs/park-and-recreation-development-fund>

#### ***SOUTH CAROLINA NATIONAL HERITAGE CORRIDOR GRANT PROGRAM***

The South Carolina National Heritage Corridor (SCNHC) is a congressionally designated National Heritage Area of the National Park Service. The SCNHC preserves and promotes the natural, historic, and cultural assets of a 17-county area of South Carolina. Since 1996, the SCNHC has awarded over \$15 million in grant funds to various tourism projects across South Carolina ranging from new museum displays to hiking trails.

One example of a project that received funding is the Stumphouse Mountain Multi-use Trail System located in Oconee County, South Carolina. In partnership with the Palmetto Conservation Foundation, the SCNHC provided partnership funds for trail construction and technical assistance for trail marketing, promotion, and visitor services. This phase of the project will create 12.9 miles of trail.

### **LOCAL FUNDING**

Local governments can create independent, local funding sources to

be used to match federal and state grants for the development of trails and multi-use paths within their respective jurisdictions. Several local funding sources available to Fort Mill are detailed below.

#### ***YORK COUNTY CRITICAL NEEDS PROGRAM***

The Critical Needs Program was established by the York County Council to address the improvement and preservation of county-maintained, paved roads. This program is funded through the General Fund.

<https://www.yorkcountygov.com/256/C-Fund-Critical-Needs-Programs>

#### ***BONDS***

Multi-use paths and trails are also frequently included in municipal transportation bond packages. Successful bond campaigns require a well-defined plan with specific projects supported by the community. Bond campaigns should be well organized with a community's public affairs department and thoroughly coordinated across all internal departments. Public outreach during the campaign is essential to educate residents about the benefits of infrastructure investment and to understand which projects garner the highest community support.

While the Town of Fort Mill has not used a bond to fund trails, there are examples of communities that are trying to use bonds as a tool to fund recreation amenities within their communities. For example, the City of Mount Holly in North Carolina recently initiated a bond referendum for parks and greenways on their November 2021 ballot. Funds raised from the bond would help the community build seven additional miles of trail and expand a park in downtown Mount Holly.

<https://storymaps.arcgis.com/stories/375cb347caf247359f2d5ef216dbfb17>

#### ***DEVELOPER BUILT TRAILS/IN-LIEU FEES***

One convenient method of funding trails is to pass on the cost to

developers. The York Forward Comprehensive Plan (2016), recommends that the County should adopt design incentive provisions to allow developers to donate a fee in-lieu instead-of the required open space dedication for residential developments not located along proposed greenways and dedicated open space. In addition, the Town of Fort Mill 2040 Comprehensive Plan states that the community would like for the Town to require developers to provide/build trail, and at a minimum build natural surface trails.

Also, several approved development agreements in the Town of Fort Mill require developers of residential subdivisions to provide public access to existing greenways and trails, donate land for buffers to support greenway networks, and requires them to work with appropriate environmental organizations.

### **IMPACT FEES**

Impact fees represent financial payments made to a local government by a developer to fund certain off-site capital improvements needed to accommodate future growth. Many communities impose impact fees for transportation, parks and recreation, and open space facility needs. In the state of South Carolina, municipal and county governments may adopt impact fees pursuant to the South Carolina Development Impact Fee Act (S.C. Code of Laws, Sec. 6-1-910 et seq).

In the Town of Fort Mill funds from development impact fee trust accounts can only be expended for the public facilities and system improvements identified as eligible for impact fee funding in the Town of Fort Mill Capital Improvements Plan. No funds can be used for administrative or operating costs associated with imposing any of the development impact fees. Eligible components of a public facility may include, but are not limited to, the following: design and construction; land acquisition; right-of-way acquisition; construction of new facilities; the purchase of new equipment with an individual purchase price of \$100,000 or more; principal payments, interest, and other financing charges. Other municipalities may charge open space and

parks and recreation development fees to be used for the acquisition of park land and the provision of facilities, including athletic fields, parks, playgrounds, courts, recreation centers, shelters, stadiums, arenas, swimming pools, lighting, trail construction, and bike paths.

### **YORK COUNTY'S PENNIES FOR PROGRESS PROGRAM**

York County's Capital Projects Sales and Use Tax Program is called Pennies for Progress. The programs were initiated by York County to provide residents with a safer and more efficient roadway system. The projects were chosen by a Sales Tax Commission that represent the residents of York County, and then were approved by York County voters. York County was the first county in South Carolina to pass this type of sales tax to improve the road system. The tax will expire in 7 years or less and non-residents who use York County roads help fund the improvements. Project improvements help ensure faster response times by emergency personnel. It is important to note that funding may only be used to improve roadways in the County.

<https://www.penniesforprogress.net/27/About-Us>

### **CAPITAL IMPROVEMENT PROGRAM**

A Capital Improvement Program (CIP) is one element in a municipality's long-term planning process. It is a bridge between the municipality's Comprehensive Plan and short-term planning for infrastructure and operations. A CIP analyzes major facility and equipment needs, establishes priorities, estimates fiscal resources, and schedules the development of funded projects.

Several projects identified in the Town of Fort Mill Capital Improvements Plan include project justifications that state trails and open space will need to be acquired and/or constructed to maintain the current level of recreation service. To maintain current level of recreation service, additional parks and park facilities, amenities, and trails and open space will need to be acquired and/or constructed. Impact fees for park and recreation were originally based on consumption (i.e., per capita

replacement value), so impact fee funds may now be used toward the purchase of any eligible facility, land, or equipment. The projects listed in the plan include the Banks Athletic Park, the gymnasium on Tom Hall Street, a future park (location to be determined), and the amphitheater at Walter Y. Elisha Park.

<https://www.fortmillsc.gov/DocumentCenter/View/176/Town-of-Fort-Mill-Capital-Improvements-Plan-PDF>

### **MUNICIPAL SERVICE DISTRICTS**

Municipal Service Districts (MSD), also called Municipal Management Districts or Municipal Improvement Districts (MID), provide an equitable method for funding special improvements to public right-of-way areas because property owners share in the cost through an assessment or tax.

According to Fort Mill’s zoning ordinance, “a MID allows for the financing of publicly-owned infrastructure serving persons and property located therein, with the cost thereof borne solely by the owners of property located within the boundaries of the MID. Thus, a MID will encourage an increase in the value of taxable property within the town without the cost of public improvements being charged against properties located within the town but outside of the boundaries of a MID. The Town is authorized by the Act to finance publicly-owned improvements within a MID through the issuance of bonds secured by assessments imposed on properties within a MID, and, further, because the interest on such bonds may, in many circumstances, be exempt from both state and federal income taxation, the use of a MID can also lower the cost of development to the developer of properties located within a MID.”

[https://library.municode.com/sc/fort\\_mill/codes/zoning?nodeId=Z00R\\_ARTIREDI\\_S2oMIMUIMDI](https://library.municode.com/sc/fort_mill/codes/zoning?nodeId=Z00R_ARTIREDI_S2oMIMUIMDI)

### **PUBLIC/PRIVATE PARTNERSHIPS**

Leveraging strong public-private partnerships is an innovative way

to achieve funding strategies for multi-use paths and trail projects. Partnerships with the private sector can help bring needed investment to public infrastructure.

The Prisma Health Swamp Rabbit Trail is an example of a successful public-private partnership between Greenville County and the Prisma Health Hospital System. The hospital helped fund segments of the trail which opened in 2009. The trail is currently 22 miles long and connects schools, parks, and local business in Greenville County. The trail’s partners hope the facility will encourage people to become more physically active in the community.

<https://greenvillerec.com/swamprabbit/>

### **LOCAL TRAIL SPONSORS / ADOPT-A-TRAIL**

Individuals and businesses can contribute donations towards trail sponsorship opportunities. Smaller trail amenities such as signs, benches, lighting can be customized for recognition with names or business logos to show local support. Clean up efforts, labor, equipment, services, or supply donations can supplement monetary donations. Donor recognition through plaques and/or small ceremonies may help highlight new amenities and bring more attention to the trail.

### **VOLUNTEER WORK**

Volunteer work is a free and community-building activity that can help support maintenance efforts for a trail. For example, the Carolina Thread Trail was created by volunteers who support activities such as community engagement efforts, trail construction and maintenance, habitat restoration, special events, Trail Master Workshops, and serve as Thread Trail Ambassadors for outdoor programs. Other examples of volunteer opportunities for trails include invasive plant removal, litter cleanups, and moving rocks along trails.

<https://www.carolinathreadtrail.org/how-you-can-help/volunteer/volunteer-event-registration/>

### AMERICAN HIKING SOCIETY'S NATIONAL TRAILS FUND GRANTS

The American Hiking Society's National Trails Fund is the only privately funded, national grants program dedicated to building and protecting hiking trails. This Fund has supported grassroot organizations acquire funding and resources to build and maintain trails across the country. So far, the American Hiking Society has funded 226 trail projects through 170 organizations by awarding over \$718,000 in National Trails Fund grants.

<https://americanhiking.org/National-Trails-Fund/>

### DUKE ENERGY FOUNDATION

The Duke Energy Foundation provides philanthropy-focused funding and support for communities interested in vibrant economies; climate resiliency; and justice, equity, and inclusion. The Foundation supports over \$30 million in charitable grants each year. In 2021, employees and retirees from Duke Energy volunteered more than 70,000 hours within nonprofit organizations. The following project topic areas may be considered for grants:

- ▶ Vibrant Economies:
  - » Workforce development training for jobs vital to the energy economy, with a focus on underrepresented populations
  - » Community revitalization and local economic development efforts
  - » Economic recovery for customers and communities facing unprecedented challenges
  - » Energy and engineering education for K-12 students and teachers
- ▶ Climate Resiliency:
  - » Environmental projects supporting land conservation; clean water; and biodiversity of plant and animal species
  - » Environmental resiliency projects that prepare communities for and mitigate against the effects of climate change

- » Natural disaster preparedness and response
- ▶ Justice, Equity, and Inclusion:
  - » Thriving natural environments, including access to green space, in historically underserved and environmental justice communities\*
  - » Programs supporting the “just transition” for communities transitioning to cleaner energy generation\*\*
  - » Diversity, equity, and inclusion initiatives supporting upward mobility

South Carolina issues a request for proposal (RFP) for grant programs annually that focus on timely issues and areas impacting the state. Local Impact Grants receive up to \$20,000 in funding and focus on programs aligned with vibrant economies, climate resiliency and justice, equity, and inclusion. This grant category is open year-round for applicants to apply to. Qualifications and restrictions are included in the second link below.

<https://www.duke-energy.com/community/duke-energy-foundation>

<https://www.duke-energy.com/community/duke-energy-foundation/south-carolina>

## PRIVATE FUNDING

### SOUTH CAROLINA LAND TRUSTS & CONSERVATION ORGANIZATIONS

Land trusts in South Carolina partner with landowners and local communities to permanently protect natural resources with agricultural, cultural, recreational, ecological, and scenic value across the state. There are several organizations in Fort Mill and throughout South Carolina that contribute to conservation efforts in the state. Descriptions are provided with some of the key organizations in York County. See below for a list of the organizations.

- ▶ The Nation Ford Land Trust is dedicated to the preservation of open spaces, natural beauty, and the scenic heritage of the

York County, South Carolina area. This land trust owns or holds conservation easements on a total of 15,092 acres in the region.

- ▶ Anne Springs Close Greenway is a nature preserve that is located in the center of Fort Mill. The preserve's 2,100 acres serve as a natural buffer from urban development and provides a wide-open space for countless activities, including hiking trails, camping, fishing, and more.
- ▶ In 1998, the York County Council created the York County Forever Commission to serve as the county's land trust organization. York County Forever aims to promote and protect significant natural, cultural, historic, and environmental resources.
- ▶ The Carolina Thread Trail is a regional network of trails, blueways, and conservation corridors that will ultimately link more than 2.3 million citizens in North and South Carolina. The Thread Trail currently runs through the following 15 counties: Anson, Cabarrus, Catawba, Cleveland, Gaston, Iredell, Lincoln, Mecklenburg, Rowan, Stanly and Union counties within North Carolina and Cherokee, Chester, Lancaster, and York counties in South Carolina.
- ▶ Katawba Valley Land Trust
- ▶ Palmetto Conservation Foundation
- ▶ South Carolina Land Trust Network
- ▶ National Register of Historic Places
- ▶ South Carolina Department of Archives & History
- ▶ South Carolina Department of Natural Resources
- ▶ South Carolina Conservation Bank

- ▶ The Trust for Public Land
- ▶ Upstate Forever
- ▶ York County Soil & Water Conservation District
- ▶ The Conservation Fund
- ▶ The Conservation Alliance

In addition to the groups listed above, the South Carolina Land Trust Network represents land trusts throughout the state by:

- ▶ Presenting a united front in support of land conservation,
- ▶ Organizing opportunities to network and collaborate,
- ▶ Providing access to resources and information; and
- ▶ Promoting land conservation through statewide marketing initiatives.

#### **FOUNDATION FOR THE CAROLINAS**

The Foundation for the Carolinas (FTFC) was established in 1958 and it supports personal and corporate philanthropy through a range of innovative fund and giving options to improve the quality of life for its surrounding communities. This organization also supports nonprofit sustainability through endowment management, customized solutions, and grantmaking. The geographic focus areas for this group include Cabarrus, Cleveland, Davidson, Iredell, Lincoln, Mecklenburg, Richmond, Rowan, Stanly and Union Counties, in North Carolina; Cherokee, Lancaster, and York Counties in South Carolina. FTFC manages grant programs in the region where nonprofits can apply for grants. The grant programs vary in size, application process, and funding priorities. Through FTFC's website, webpage visitors can make donations to support the Carolina Thread Trail and the York County Community Foundation Endowment Fund.

<https://www.ftfc.org/>

### **BLUE CROSS BLUE SHIELD OF SOUTH CAROLINA FOUNDATION**

The Blue Cross Blue Shield of South Carolina Foundation funds a range of programs from targeted, mini grants to multi-year partnerships. Their grantmaking supports initiatives that focus on early childhood, healthy communities, healthy food, and oral health. The Foundation does not operate regular grant cycles. Instead, the Foundation invites applications based on specific strategic objectives or announces broader opportunities to apply for funding on a periodic basis.

<https://www.bcbsscfoundation.org/health-priority-grants/apply>

### **PEOPLEFORBIKES COMMUNITY GRANTS**

The PeopleForBikes Community Grant Program supports bicycle infrastructure projects and targeted advocacy initiatives that make it easier and safer for people of all ages and abilities to ride. PeopleForBikes accepts grant applications from non-profit organizations with a focus on bicycling, active transportation, or community development; city or county agencies or departments, and state or federal agencies working locally. PeopleForBikes focuses most grant funds on bicycle infrastructure projects, such as bike paths, lanes, trails and bridges, mountain bike facilities, bike parks and pump tracks, BMX facilities, and end-of-trip facilities such as bike racks, bike parking, bike repair stations and bike storage. Advocacy projects funded through the program include open street events and campaigns to increase investment in bicycle infrastructure. PeopleForBikes accepts requests for funding up to \$10,000. PeopleForBikes does not require a specific percentage match, but they will not consider requests in which the grant funding would amount to 50% or more of the project budget.

<https://www.peopleforbikes.org/grant-guidelines>

### **NATIONAL ASSOCIATION OF REALTORS - SMART GROWTH & PLACEMAKING GRANTS**

The National Association of Realtors (NAR) funds placemaking and smart growth grants to make communities better places to live by transforming unused or underutilized sites into welcoming destinations accessible to everyone in a community.

**Smart Growth Grants:** Smart Growth Grants fund efforts to engage in local land-use, growth, and transportation policy issues with other stakeholders and elected officials. Eligible projects include Better Block events, placemaking visioning processes, charettes, pop-up workshops, project mock-ups, developer open houses, public open houses, utility roundtables, Main Street analysis, walkable community workshops/audits, assistance with updating land use ordinances and codes and community plans, and hosting conferences and webinars. Applications can only be submitted by a state or local REALTOR® association, and grants provide up to \$5,000 per award.

**Placemaking Grants:** Placemaking Grants fund the creation of new, outdoor public spaces and destinations in a community. Funds can be used for amenities such as street furniture, paint, signage, materials, landscaping, murals, site preparation, and artist fees. Applications can only be submitted by a state or local REALTOR® association, and grants provide up to \$5,000 per award.

<https://www.nar.realtor/about-nar/awards-and-grants>

### **AARP COMMUNITY CHALLENGE GRANT**

The AARP Community Challenge provides small grants to fund quick-action projects that can help communities become more livable for people of all ages. Applications are accepted for projects to improve public spaces, housing, transportation, civic engagement, coronavirus recovery, diversity, and inclusion, and more. Project types include those that provide permanent physical improvements in the community, temporary demonstrations that lead to long-term change, and innovative programming or services. The program is open to 501(C)(3),

501(C)(4) and 501(c)(6) nonprofits and government entities. Grants can range from several hundred dollars for smaller, short-term activities to several thousand or tens of thousands of dollars for larger projects.

<https://www.aarp.org/livable-communities/community-challenge/>

